



Updated:

12/17/18 @ 3:30pm

Added Neighborhood Map, Non-Residential Use Table, & Land Use Map for Item 5 – Rezoning 1900 Wakarusa

12/17/18 @ 1:30pm

Added communications for the following items:

Item 1 - Comprehensive Plan 2040

Items 2A-2C - North Lawrence Riverfront

12/14/18 @ 4:45pm

Added the following items:

Draft November 2018 Planning Commission Minutes

Updated 2019 Mid-Month Calendar

Communications for Item 1 - Comprehensive Plan 2040

Communications for Items 2A-2C - North Lawrence Riverfront

12/13/18 @ 4:00pm

Added the following:

Communications for Item 1 - Comprehensive Plan 2040

Item 2C - Preliminary Development Plan for North Lawrence Riverfront

12/11/18 @ 5:30pm

The following items will be added when available:

Item 2C - Preliminary Development Plan for North Lawrence Riverfront

Draft November 2018 Planning Commission Minutes

**LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION
CITY HALL, 6 EAST 6TH STREET, CITY COMMISSION MEETING ROOM
AGENDA FOR PUBLIC & NON-PUBLIC HEARING ITEMS
MONDAY & WEDNESDAY, DECEMBER 17 & 19, 2018 6:30PM - 10:30PM**

GENERAL BUSINESS:

PLANNING COMMISSION ACTION SUMMARY

Receive and amend or approve the action summary (minutes) from the Planning Commission meeting of November 12 & 14, 2018.

COMMITTEE REPORTS

Receive reports from any committees that met over the past month.

COMMUNICATIONS

- a) Receive written communications from the public.
- b) Receive written communications from staff, Planning Commissioners, or other commissioners.
- c) Receive written action of any waiver requests/determinations made by the City Engineer.
- d) Disclosure of ex parte communications.
- e) Declaration of abstentions from specific agenda items by commissioners.

f) General public comment.

AGENDA ITEMS MAY BE TAKEN OUT OF ORDER AT THE COMMISSION'S DISCRETION
REGULAR AGENDA (DECEMBER 17, 2018) MEETING

PUBLIC HEARING ITEM:

ITEM NO. 1 COMPREHENSIVE PLAN 2040

Continue discussion regarding the Comprehensive Plan 2040 for unincorporated Douglas County and the city of Lawrence. Topic discussion will include an introduction and Growth & Development.

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

Recess until 6:30pm on December 19, 2018

BEGIN PUBLIC HEARING (DECEMBER 19, 2018):

COMMUNICATIONS

- a) Receive written communications from staff, Planning Commissioners, or other commissioners.
- b) Disclosure of ex parte communications.
- c) Declaration of abstentions from specific agenda items by commissioners.
- d) General public comment.

AGENDA ITEMS MAY BE TAKEN OUT OF ORDER AT THE COMMISSION'S DISCRETION **REGULAR AGENDA (DECEMBER 19, 2018) MEETING**

PUBLIC HEARING ITEM:

ITEM NO. 2A REZONING 16.116 ACRES FROM OS, CS, IG TO CD-PD; 311, 317, 401, 409, 415, 501, 505 N 2ND ST (SLD)

Z-18-00505: Consider a request to rezone approximately 16.116 acres from OS (Open Space) District, CS (Strip Commercial) District, & IG (General Industrial) District to CD-PD (Downtown Commercial with Planned Development Overlay) District, and to affirm the findings for publication of PCR-1-1-12 and CPA-11-8-11 expanding the identified boundaries of Downtown Lawrence, located at 311, 317, 401, 409, 415, 501, & 505 N 2nd St. Submitted by Paul Werner Architects on behalf of Abfield Investments LLC, City of Lawrence, Douglas County Kaw Drainage District, D&D Rentals of Lawrence LLC, Exchange Holdings LLC, HDD of Lawrence LLC, Kaw River Estates LLC, Patience LLC, Loosehead Investments LLC, and Riverfront Properties of Lawrence LLC, property owners of record.

NON-PUBLIC HEARING ITEM:

ITEM NO. 2B PRELIMINARY PLAT FOR NORTH LAWRENCE RIVERFRONT ADDITION; 311, 317, 401, 409, 415, 501, 505 N 2ND ST (SLD)

PP-18-00504: Consider a one-lot Preliminary Plat for North Lawrence Riverfront Addition, located at 311, 317, 401, 409, 415, 501, & 505 N 2nd St for mixed use development including residential and commercial uses. Variances related to block length and right-of-way dedication for N. 2nd Street as a principal arterial. Submitted by Paul Werner Architects on behalf of Abfield Investments LLC, City of Lawrence, Douglas County Kaw Drainage District, D&D Rentals of Lawrence LLC, Exchange Holdings LLC, HDD of Lawrence LLC, Kaw River Estates LLC, Patience LLC, Loosehead Investments LLC, and Riverfront Properties of Lawrence LLC, property owners of record.

RESUME PUBLIC HEARING:

ITEM NO. 2C PRELIMINARY DEVELOPMENT PLAN FOR NORTH LAWRENCE RIVERFRONT; 311, 317, 401, 409, 415, 501, 505 N 2ND ST (SLD)

PDP-18-00506: Consider a Preliminary Development Plan for North Lawrence Riverfront, located at 311, 317, 401, 409, 415, 501, & 505 N 2nd St. The project includes multiple phases and mixed residential and commercial development. Submitted by Paul Werner Architects on behalf of Abfield Investments LLC, City of Lawrence, Douglas County Kaw Drainage District, D&D Rentals of Lawrence LLC, Exchange Holdings LLC, HDD of Lawrence LLC, Kaw River Estates LLC, Patience LLC, Loosehead Investments LLC, and Riverfront Properties of Lawrence LLC, property owners of record.

ITEM NO. 3 REZONING .18 ACRES FROM PCD TO CS; 3235 OUSDAHL (MKM)

Z-18-00508: Consider a request to rezone approximately 0.18 acres from PCD (Planned Commercial Development) District to CS (Strip Commercial) District, located at 3235 Ousdahl. Submitted by Paul Werner Architects, for Fraternal Investors LLC, property owner of record.

ITEM NO. 4 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; CONDITIONAL ZONING (SLD)

TA-18-00430: Consider a Text Amendment to the City of Lawrence Land Development Code, to define and clarify the use of conditional zoning. *Initiated by Planning Commission on 8/22/18.*

ITEM NO. 5 REZONING 2.7 ACRES FROM IBP TO IL; 1900 WAKARUSA DR (KEW)

Z-18-00495: Consider rezoning approximately 2.7 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, located at 1900 Wakarusa Dr. Submitted by Home Sweet Home on behalf of Larry McElwain, property owner of record.

ITEM NO. 6 CONDITIONAL USE PERMIT; NW OF N 1200 RD & E 1150 RD (KEW)

CUP-18-00501: Consider a Conditional Use Permit for a rugby/soccer complex, located northwest of the intersection of N 1200 Rd & E 1150 Rd. Submitted by Paul Werner Architects, for Westwick LC, property owner of record.

ITEM NO. 7 SPECIAL USE PERMIT; 1000 W 2ND ST (SLD)

SUP-18-00518: Consider a Special Use Permit/Institutional Development Plan for the development of residential housing to include a 12 bedroom *Group Home* and 10 one-bedroom apartments, located at 1000 W. 2nd Street. Submitted by TreanorHL, for Douglas County, property owner of record.

****DEFERRED****

~~ITEM NO. 8 SPECIAL USE PERMIT; 1040 MASSACHUSETTS ST, 1041 NEW HAMPSHIRE ST & 1000 NEW HAMPSHIRE BLOCK 1 (BJP)~~

~~SUP-18-00502:~~ Consider a Special Use Permit for ground floor dwelling units, The Hub at Lawrence, located at 1040 Massachusetts St, 1041 New Hampshire St, and 1000 New Hampshire Block 1. Submitted by Core Lawrence Massachusetts LLC on behalf of Allen Press Inc and Allen Realty Inc, property owners of record.

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

ADJOURN

CALENDAR

November 2018						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
				1	2	3
4	5	6	7	8	9	10
11	12	13	14	15	16	17
18	19	20	21	22	23	24
25	26	27	28	29	30	

December 2018						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
						1
2	3	4	5	6	7	8
9	10	11	12	13	14	15
16	17	18	19	20	21	22
23	24	25	26	27	28	29
30	31					

January 2019						
Sun	Mon	Tue	Wed	Thu	Fri	Sat
		1	2	3	4	5
6	7	8	9	10	11	12
13	14	15	16	17	18	19
20	21	22	23	24	25	26
27	28	29	30	31		

PCCM Meeting:

(Generally 2nd Wednesday of each month, 7:30am-9:00am)

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<http://www.lawrenceks.org/subscriptions>

2019
LAWRENCE-DOUGLAS COUNTY METROPOLITAN PLANNING COMMISSION
MID-MONTH & REGULAR MEETING DATES

Mid-Month Meetings, Wednesdays 7:30 – 9:00 AM	Mid-Month Topics		Planning Commission Meetings 6:30 PM, Monday and Wednesday	
Jan 9	STAR Rating/LEED for Cities & Communities		--	Jan 23
Feb 13			Feb 25	Feb 27
Mar 13			Mar 25	Mar 27
Apr 10			Apr 22	Apr 24
May 8			May 20	May 22
Jun 12			Jun 24	Jun 26
Jul 10			Jul 22	Jul 24
Aug 14			Aug 26	Aug 28
Sep 11			Sep 23	Sep 25
Oct 9			Oct 21	Oct 23
Nov 6			Nov 18	Nov 20
Dec 4			Dec 16	Dec 18
	Suggested topics for future meetings: Affordable Housing New County Zoning Codes Water Resources		Retail Market Bus Tour – Development Patterns Native Prairie Visit/Tour Soils	
Meeting Locations	The Planning Commission meetings are held in the City Commission meeting room on the 1 st floor of City Hall, 6 th & Massachusetts Streets, unless otherwise noticed.			
Planning & Development Services Lawrence-Douglas County Planning Division 785-832-3150 www.lawrenceks.org/pds				

Revised 12/14/18

2018 PLANNING COMMISSION ATTENDANCE

	Jan 24 2018	Feb 21 2018	Mar 28 2018	April 23 2018	April 25 2018	May 23 2018	June 27 2018	July 25 2018	August 22 2018	Sept 26 2018	Oct 24 2018	Nov 12 2018	Nov 14 2018
Butler	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	-	Yes	Yes
Carttar							Yes	Yes	Yes	Yes	-	Yes	Yes
Carpenter	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	-	Yes	Yes
Culver	Yes	Yes	Yes	Yes	Yes	Yes							
Kelly	No	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	-	Yes	Yes
Paden	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	-	Yes	Yes
Sands	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	-	Yes	Yes
Sinclair	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	-	Yes	Yes
Struckhoff	Yes	Yes	Yes	No*	Yes	Yes	Yes	Yes	Yes	Yes	-	Yes	Yes
Weaver	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	-	Yes	Yes
Wiley	Yes	Yes	Yes	No*	Yes	Yes	Yes	No	Yes	Yes	-	Yes	Yes
			*APA Conf										

2018 MID-MONTH ATTENDANCE

	Jan 10 2018	Feb 7 2018	Mar 14 2018	April 11 2018	May 9 2018	June 13 2018	July 11 2018	Aug 8 2018	Sept 12 2018	Oct 19 2018	Nov 7 2018	Dec 5 2018
Butler	Yes	Yes	Yes	-	No	Yes	-	Yes	Yes	Yes	-	-
Carttar						No	-	Yes	Yes	Yes	-	-
Carpenter	Yes	Yes	Yes	-	Yes	Yes	-	Yes	Yes	Yes	-	-
Culver	Yes	Yes	Yes	-	Yes							
Kelly	Yes	Yes	Yes	-	Yes	Yes	-	Yes	Yes	Yes	-	-
Paden	Yes	Yes	Yes	-	Yes	No	-	Yes	Yes	Yes	-	-
Sands	Yes	Yes	Yes	-	No	Yes	-	No	Yes	Yes	-	-
Sinclair	Yes	No	Yes	-	Yes	No	-	Yes	Yes	Yes	-	-
Struckhoff	Yes	Yes	Yes	-	Yes	Yes	-	Yes	Yes	Yes	-	-
Weaver	Yes	Yes	No	-	Yes	Yes	-	Yes	Yes	Yes	-	-
Wiley	Yes	Yes	Yes	-	Yes	Yes	-	Yes	Yes	Yes	-	-



PLANNING COMMISSION MEETING
November 12 & 14, 2018
Meeting Action Summary

November 12, 2018 – 6:30 p.m.

Commissioners present: Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, Willey

Staff present: McCullough, Crick, Ewert, A. Miller

GENERAL BUSINESS
PLANNING COMMISSION ACTION SUMMARY

Receive and amend or approve the action summary (minutes) from the Planning Commission meeting of September 26, 2018.

Motioned by Commissioner Sands, seconded by Commissioner Struckhoff, to approve the September 26, 2018 Planning Commission action summary minutes.

Motion carried 10-0. Commissioners Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, and Willey voted in favor.

COMMITTEE REPORTS

Receive reports from any committees that met over the past month.

Commissioner Paden said the Downtown Master Plan Steering Committee met and identified the most pressing issues for downtown. She said they discussed the balance of progress and keeping the feel of downtown. She said they discussed transportation, retaining businesses, and several other things.

EX PARTE / ABSTENTIONS / DEFERRAL REQUEST

- Ex parte:
Commissioner Willey said she spoke with the following groups about rural issues: Clint Hornberger the Willow Springs Township Clerk, Willow Springs Fire Department, American Farmland Trust, Helen Schnose with the Douglas County Sustainability Office, Tonya Voigt with the Douglas County Zoning & Codes. She said she spoke with the following groups about environmental issues: Frank Norman with Kansas Alliance for Wetlands and Streams and Upper Wakarusa WRAPS, Kim Bellemere with the Grassland Heritage Foundation, Jennifer Delisle and Kelly Kindscher with the Kansas Biological Survey, Pennie von Achen who was a former Planning Commissioner, Doug Holcomb with the Water Action Committee, Dawn Buehler with Friends of the Kaw.
- No abstentions.

GENERAL PUBLIC COMMENT

No general public comment.

DRAFT

PC Minutes 11/12/18

ITEM NO. 1 COMPREHENSIVE PLAN 2040

Discussion regarding the Comprehensive Plan 2040 for unincorporated Douglas County and the city of Lawrence.

STAFF PRESENTATION

Scott McCullough and Jeff Crick presented the item.

PUBLIC COMMENT

Thad Holcombe, Lawrence Ecology Teams United in Solidarity, supported amending Plan 2040. He felt Chapter 16-Environment of Horizon 2020 should be included in Plan 2040. He referenced his letter that was included in the agenda packet.

Pennie von Achen, 1346 E 2350 Rd, Eudora, commented on Chapter 6-Natural Resources in Plan 2040. She wanted clarity, substance, utility, and direction in the comprehensive plan. She expressed concern that Chapter 16-Environment from Horizon 2020 was condensed from 30 pages to 4 ½ pages in Plan 2040. She felt the clarity, direction, substance, and utility, had been lost in doing so. She felt specific environmental language should be incorporated in Plan 2040.

Hugh Carter, Lawrence Chamber of Commerce, felt there was still work to be done on the plan. He asked Planning Commission to take their time and allow time for feedback.

Teresa Wilke, 3221 Yellowstone Drive, read the overview from Chapter 16, Horizon 2020. She felt an overarching statement should be added to Plan 2040 recognizing the importance of sustaining the environment and protecting people from future effects of waste and pollution. She felt language should be added to Plan 2040 regarding STAR certification, fracking, and ways to protect the environment. She felt Plan 2040 was too general.

Cathy Dwigans, Heritage Conservation Council, said they wanted the opportunity to be able to participate in the implementation of the plan to preserve natural and cultural heritage of the county.

COMMISSION DISCUSSION

Commissioner Kelly said the introduction was a crucial part of the comprehensive plan document. He said the current Development Code looked different than it did when Horizon 2020 was written. He said it was not fair to look at Horizon 2020 without understanding all the other things happening such as the Transportation 2040 documents, which did not exist when Horizon 2020 was created. He said Chapter 16 was not in the first plan. He encouraged everyone to look at the appendix and the action plan. He agreed with public comments about them not being specific enough or measurable enough. He said some were not as measurable as he would like. He said it provided a way moving forward to possibly have better measurables and better specifics and that the plan could be amended based on that. He said he was not sure there would ever be enough community input. He said the steering committee went chapter by chapter after receiving the draft plan. He said the committee received a lot of feedback from the community during that stage. He said there was some fatigue on the steering committee to hand it over to the next body to keep it going.

Wiley said during the first pass of looking at the document she wanted to look at is as a whole and gauge community interest and concerns to gauge the work plan from there. She said the overarching concern with environmental groups was the lack of detail and if not included in the comprehensive

Complete audio & video from this meeting can be found online:

<https://lawrenceks.org/boards/lawrence-douglas-county-metropolitan-planning-commission/>

plan, then where would it be and who would be responsible for it. She felt it would provide a level of comfort to the community to know nothing would be lost from the old plan. She said in terms of recommendations, to bring in large sections of Chapter 16 into 6, was not appropriate in trying to make a full readable document.

McCullough encouraged them to look at the appendix. He said the appendix was meant to lay out future action items that would be prioritized and provide a benchmark on how successful the plan was over time. He said the Natural Resources category had the largest list of policies. He said it starts outlying how to go from policy, goal, and aspiration in the Comprehensive Plan to the Code, which is where it was most valuable. He said staff and the steering committee tried to sift out the topics and policies in Horizon 2020 that weren't directly helpful to Planning Commission from a land use perspective.

Commissioner Sands said the Comprehensive Plan was the originating document for all other plans so it was probably not appropriate to have a finite level of specificity. He said the highest document shouldn't have specifics. He said he would like to see a few sentences to explain that concept.

McCullough said Planning Commission's role was to identify issues in the plan. He said, for example, Planning Commission could go back to Chapter 16 of Horizon 2020 and look at some of the policies to incorporate in the plan. He said Planning Commission could also ask for more information on how much public input there was. He said staff was looking for issues Planning Commission would need to spend time with and educated on. He said maybe the discussion should turn to if Planning Commission was comfortable with what came out of the steering committee and if Planning Commission saw conflicts that need to be looked at.

Commissioner Sands said he liked the document overall. He said it was written better than similar examples he found. He said it was a good originating document to direct other plans.

Commissioner Kelly said it may be beneficial to receive public input in looking at each individual chapter.

Sands said he was fine with going through the document a few chapters at a time.

Commissioner Struckhoff said the steering committee went through the document chapter by chapter.

McCullough said Planning Commission has to work with this plan after it's approved and they need to know it inside and out. He said it would take around three months to get through if they take a few chapters at a time.

Commissioner Willey said she hoped they could identify areas of concern by Planning Commission. She felt doing the bulk of the work now could eliminate the document from being kicked back by City Commission and County Commission. She said a concern she had was that the document did not mention home business.

Commissioner Kelly felt they should talk about the second sentence of the introduction and talk about what is policy and what isn't policy. He said the plan did not feel like a policy document but rather a vision document. He felt a policy should be very specific and the plan was not.

Commissioner Sands said one of his concerns was if the plan would be a legal document and the consequences of not following it. He said policies usually have an enforcement arm. He wondered how it would be enforced using words such as 'encouraged' or 'support.'

Commissioner Butler said under the authority section it talked about the Comprehensive Plan as implemented by the Land Development Code.

McCullough said the Development Code states that a project has to meet the Comprehensive Plan or the plan needs to be amended. He said when talking about a binding document for development purposes it needs to meet the policies or sector plan being developed in.

Commissioner Willey asked what documents would cover environmental specifics.

McCullough said the Climate Protection Plan had not been developed yet. He said the Sustainability Division was working through the STAR program. He said the Development Code and joint Subdivision Regulations would carry out the floodplain and sensitive land areas. He said agriculture had the highest protection of all and was exempt from regulations by State Statute. He said Planning Commission could talk about how to make agriculture work better but they did not have a lot of authority over it. He said an aspirational statement could be added.

Commissioner Carttar wondered about the process for a developer or land owner who makes a proposal that conforms to the existing Code but conflicts with elements of the new plan.

McCullough said the Code is what staff uses for development proposals because that was the law. He said once Plan 2040 was approved it would be implemented in the Code. He said if there are conflicts identified then a development may meet the Code but conflict with Plan 2040 policy until the Code can be changed to align those policies.

Commissioner Carttar expressed concern about discussions in the coming years that could involve a development allowed by Code but in conflict of the Comprehensive Plan.

McCullough said he did not believe there would be that many issues. He said there may be conflicts in the sector plans and what the Comprehensive Plan says since the sector plans were built on the Horizon 2020 policy. He said some of the sector plans may need to be tweaked to align better with Plan 2040.

Crick said the sector plans would be a stepping stone to make some things come down to that level.

Commissioner Struckhoff expressed concern about specificity of the language in the new plan. He was worried about removing language that provided guidance, institutional memory, and intent of previous commissions. He said he took comfort in the fact that the language could be found in other documents.

Commissioner Kelly said drilling down all the specific items would take many discussions and many hard decisions. He said he was not sure they would be able to get there to approve it. He said he wanted measurable specifics but they wouldn't have time to get to all of them and by the time they did there would be new ones they never thought of. He said the Comprehensive Plan was a flexible document.

Commissioner Carttar felt it would be helpful to know how the old plan was different from the new plan. He wanted to be able to map the document to the old one.

Butler said she understood the need for more information but that was a huge task for staff. She did not feel specific details was the way to go when they want the public to participate in the development process. She said specificity would add thousands of pages to the document. She felt having specifics in other documents was more appropriate.

McCullough said staff would not be able to compare and contrast by December. He suggested the topic of environment be part of the third meeting. He said taking the document in smaller bites may be easier. He suggested Planning Commission discuss growth management and vision in December.

Commissioner Carpenter said the timeframe for Horizon 2020 was expansive. He did not want the carryover between the two plans to drop out or disappear that are of value to the community as expressed in Horizon 2020.

Commissioner Struckhoff said he wasn't proposing adding specific language into the document. He was hoping the language would reside elsewhere. He said he was comfortable with the more visionary approach of Plan 2040.

Commissioner Willey said Planning Commission would table the discussion until the December 17 meeting.

Commissioner Weaver said he liked the general nature of the plan. He said he did not like all of the individual plans at the back. He said he would prefer a more consistent format.

McCullough said that may be difficult.

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

Motioned by Commissioner Carpenter, seconded by Commissioner Struckhoff, to recess until 6:30pm on November 14, 2018.

Motion carried 10-0. Commissioners Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, and Willey voted in favor of the motion.

Recess at 8:58pm until 6:30pm on November 14, 2018

PC Minutes 11/14/18

Reconvene November 14, 2018 – 6:30 p.m.

Commissioners present: Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, Willey

Staff present: McCullough, Day, Ewert, Kobe, Larkin, M. Miller, Pepper

BEGIN PUBLIC HEARING (NOVEMBER 14, 2018):

EX PARTE / ABSTENTIONS / DEFERRAL REQUEST

- No ex parte.
- No Abstentions.

GENERAL PUBLIC COMMENT

No general public comment.

PC Minutes 11/14/18

ITEM NO. 2 PRELIMINARY PLAT FOR HERITAGE TRACTOR; 1110 E 23RD ST (MKM)

PP-18-00440: Consider a Preliminary Plat for Heritage Tractor, a one-lot commercial subdivision of approximately 3.58 acres located at 1110 E 23rd St. Submitted by Davidson Architecture & Engineering on behalf of Muth Properties LLC, property owner of record.

STAFF PRESENTATION

Mary Miller presented the item.

APPLICANT PRESENTATION

Tom Clemmons, Surveyor, agreed with staff report and was present for questions.

PUBLIC COMMENT

No public comment.

ACTION TAKEN

Motioned by Commissioner Kelly, seconded by Commissioner Carttar, to approve the Preliminary Plat, PP-18-00440, for Heritage Tractor Subdivision subject to the following condition:

1. Applicant shall provide a revised preliminary plat with the following change:
 - a. Show the waterline within the 15 foot wide utility easement being provided along the east side of the lot.

Unanimously approved 10-0. Commissioners Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, and Willey voted in favor of the motion.

PC Minutes 11/14/18

**ITEM NO. 3 TEXT AMENDMENT TO CITY OF LAWRENCE CODE; SYNTHETIC TURF
(BJP)**

TA-18-00466: Consider a Text Amendment to the City of Lawrence Code, Chapter 18 & 20, regarding the use of synthetic turf. *Initiated by City Commission on 10/16/18.*

STAFF PRESENTATION

Becky Pepper presented the item.

PUBLIC COMMENT

No public comment.

COMMISSION DISCUSSION

Commissioner Willey asked about the maximum square footage.

McCullough said it was at the discretion of the Planning Director. He said they were trying to avoid the wide scale use of it in place of living plant material. He said an applicant could appeal a denial decision from the Planning Director.

Commissioner Willey asked if synthetic turf was permeable.

McCullough said yes. He said it was appropriate in small scale projects.

Commissioner Willey wondered if it was possible to put a size description on 'small scale' or limit it to instances where the grass or other surface would not be appropriate.

McCullough said in practice the language had served well. He said when plants are required or necessary there should be live plants or landscape material used such as rock or mulch. He said the uses approved so far had not been for landscaping use but rather for a dog run or playground field area for children.

Commissioner Carttar asked about the appeal going to the Board of Zoning Appeals instead of Planning Commission.

McCullough said it was part of the site plan process. He said many standards go through the Board of Zoning Appeals before an administrative determination on a site plan so it would align with current practices.

Commissioner Kelly said he would vote in favor of the text amendment.

ACTION TAKEN

Motioned by Commissioner Struckhoff, seconded by Commissioner Sinclair, to approve the proposed Text Amendment, TA-18-00466, to the City of Lawrence Code, Chapters 18 & 20, regarding the use of synthetic turf and forwarding to the Lawrence City Commission with a recommendation for approval.

Complete audio & video from this meeting can be found online:

<https://lawrenceks.org/boards/lawrence-douglas-county-metropolitan-planning-commission/>

Approved 9-1, with Commissioner Sands voting in opposition. Commissioners Butler, Carpenter, Carttar, Kelly, Paden, Sinclair, Struckhoff, Weaver, and Willey voted in favor of the motion.

DRAFT

PC Minutes 11/14/18

ITEM NO. 4 CONDITIONAL USE PERMIT FOR TOP SOIL REMOVAL & LANDFILL; NE CORNER OF E 1600 RD & N 1250 RD (MKM)

CUP-18-00432: Consider a Conditional Use Permit for *Top Soil Removal* and *Landfill* (clean rubble) on approximately 44.88 acres located at the NE Corner of E 1600 Rd & N 1250 Rd. Submitted by RD Johnson Excavating Company LLC, property owner of record.

STAFF PRESENTATION

Mary Miller presented the item.

APPLICANT PRESENTATION

Jason Dahl, RD Johnson, said the Conditional Use Permit was important to continue their business.

PUBLIC COMMENT

No public comment.

COMMISSION DISCUSSION

Commissioner Paden said the staff report included the statement "High quality soils are not identified for protection in the zoning regulations and are not considered one of the environmentally sensitive lands that requires protection when land divisions occur.....The property is within the City of Lawrence's Urban Growth Area and is planned to be annexed into the city and developed with urban land uses at some point." She inquired about floodplain regulations.

Miller said the county had floodplain regulations. She said the county allowed minimal adverse. She said the Zoning Regulations were being revised so there were no current plans to change the floodplain. She said the floodplain manager for the county would evaluate it. She said the county floodplain manager felt that keeping the elevation the same would not impact to floodplain.

McCullough said the city of Lawrence exceeds state and national model ordinances. He said they participate in the community rating system, which through different program elements, helps to get reduced insurance rates for those that need it in the city.

Commissioner Willey inquired about protections for prime agricultural soils, class one and two.

Miller said Horizon 2020 was adopted after the Zoning Regulations and that part was included in the industrial chapter after 2006. She said the Zoning Regulations had not been updated to reflect that. She said environmentally sensitive land protections were created but that high quality agricultural soils were not added at that time. She said they would probably be added with the revised regulations. She said there were no current protection standards or requirements for high quality soils.

McCullough said with the development of the Northeast Sector Plan discussions evolved into where class I and II soil type amounts were significant and contiguous in their value to the agricultural industry. He said the Northeast Sector Plan is the one sector plan that doesn't call for urban density. He said the other areas of significance were along the Wakarusa River area where it's not likely to urbanize because of the floodplain and floodway. He said periodically there are parcels with small amounts of class I and II soils that staff review for its merit but don't necessarily hold it to the same level as the Northeast Area Plan, for example.

Complete audio & video from this meeting can be found online:

<https://lawrenceks.org/boards/lawrence-douglas-county-metropolitan-planning-commission/>

Commissioner Willey said the Wakarusa Floodplain was another area of contiguous high value agriculturally significant class I and II soils. She said there was not equal value in the land before and after for agriculture from mining a high quality soil and replacing it with rubble.

Commissioner Carpenter asked how deep the top soil was.

Miller said staff uses the term top soil but that she doubted everything being removed would be top soil. She said it was the term used for a borrow pit in removing soil.

Carpenter said there was a big difference between soil and dirt. He inquired about possible future development.

Miller said development would be limited due to the floodway and floodplain.

McCullough said they wouldn't necessarily be having this discussion if it weren't for the need for the borrow pit with the highway project.

Commissioner Sinclair asked what could be done with agricultural land that had been reclaimed.

Willey said it could be used for grazing land. She wondered how deep the extraction was.

Dahl said the topsoil was about 8 inches to 1 foot. He said they can save the top soil or use good sources of top soil to replace it. He said after the topsoil is gone they dig down about 12 feet. He said fescue or rye could be planted, or any other kind of agricultural vegetables. He said part of the property was being used to grow beans.

Dean Grob, Grob Engineering Services, said RD Johnson was responsible for the reclamation of the Ames construction site on 59 Highway. He said the Soil Conservation Service came up with maps when the city and county looked at class I and II soils. He said there was no delineated line for where crops grew better than others.

Commissioner Willey agreed there was no hard and fast line between soil types. She said there were 8 classifications for soil suitability types and class I and II were limited. She said it was the only protection there was for agricultural land and soils.

Commissioner Kelly asked the applicant what he saw for the space going forward if the Conditional Use Permit was not approved.

Dahl said a pond, which is what was there now. He said the material was used to build K-10.

Commissioner Carttar said the application was for two uses; to fill with clean rubble and continue to excavate. He wondered about the economic feasibility of half of the application, to fill, without the continued excavation.

Dahl said he needs the area to be able to get rid of clean rubble. He said the borrow source of material was good and he wants to be able to utilize the site. He said it would be detrimental if he was not able to open up the rest of the property.

Commissioner Sinclair said they were talking about 7 acres that were bounded.

Commissioner Butler asked how much area had been used and what was left.

Dahl said the area total was 44 acres.

Grob said there were areas not part of this site. He said there were about 35 acres in the Conditional Use Permit. He said the pond was probably 22-25 acres and 10 acres to borrow.

Commissioner Willey said the fill has been ongoing but wondered if the borrowing was current as well.

Grob said there was borrowing from the existing pond and some borrowing in the south area.

Commissioner Sands inquired about the estimated end date.

Grob said it was a 10 year Conditional Use Permit with 5 year renewal. He said it would depend on construction so it was hard to predict.

Commissioner Willey asked if RD Johnson owned any other active borrow pits at this time.

Dahl said no.

Commissioner Kelly inquired about the permit issued originally for the borrow pit.

Miller said there was no limit to the amount they could borrow.

Commissioner Sands said he was generally comfortable with this.

ACTION TAKEN

Motioned by Commissioner Sands, seconded by Commissioner Struckhoff, to approve the Conditional Use Permit, CUP-18-00432, subject to the following conditions:

1. The plan shall be revised with the following changes:
 - a. Deadlines for the reclamation phases listed: May 31, 2019 for the 1st phase and November 30, 2019 for the 2nd phase and the area in each phase noted.
 - b. Note regarding the burning of trees and brush and the area shown for this activity removed from the plan.
2. The following conditions apply to the use. Note these on the plan:
 - a. Only materials that are included in the KDHE definition of Clean Rubble may be disposed of in the landfill. These materials are: *"...the following types of construction and demolition waste: concrete and concrete products including reinforcing steel, asphalt pavement, brick, rock and uncontaminated soil as defined in rules and regulations adopted by the secretary."* Materials which do not meet this definition, including trees and brush, shall not be brought to the site.
 - b. The current drainage pattern with drainage flowing through a culvert under N. 1250 Road, northward across the subject property and under K-10 Highway, shall be maintained.

Complete audio & video from this meeting can be found online:

<https://lawrenceks.org/boards/lawrence-douglas-county-metropolitan-planning-commission/>

- c. When filled, the borrow pit shall be graded to no more than 6 inches of the final elevation shown on the reclamation plan. When grading is complete, 6 inches of top soil shall be applied and the property shall be seeded. Reclamation is considered complete when vegetation has been established.
 - d. The Conditional Use Permit is subject to a 5 year administrative review and will expire 10 years after the date of the Commission's approval, unless an extension request, submitted before the deadline, is approved by the Board of County Commissioners.
3. A reclamation / closure plan shall be submitted to the Zoning and Codes Office by December 31st of each year, beginning in 2019. The plan shall show the following:
- a. The portion that has been reclaimed (graded and final seeded and vegetation has been established) and reclamation has been approved by the Zoning and Codes Office; the portion that has been filled and graded (and seeded, if seeding has occurred but vegetation hasn't been established); the portion that is open and soil is being removed from, and the portion that has not yet been disturbed.
 - b. The area of each section shall be noted on the plan.
 - c. The plan shall note the estimated timeframe for the completion of reclamation of any phases where borrowing is complete.
 - d. As reclamation occurs, the plan shall include the surveyed elevation points of the areas that have been final graded and seeded to insure that the finished elevation is less than or equal to that which existing prior to borrowing, as shown on the approved CUP plan.

Commissioner Sinclair said he doesn't love the idea of borrowing the soil but it was just 7 acres left to borrow from and Planning Commission was left to balance the value of the soils with the need for development. He said it did not feel good to let class I and II soils go but that it was a good balance.

Commissioner Carttar said he agreed with Commissioner Sinclair. He said the applicant mentioned provisions could be made to save the topsoil. He asked the applicant to explain how that would work.

Dahl said the topsoil would be stripped and an area would be created to stage that material. He said there was a current pile of topsoil saved. He said the topsoil could be saved and replaced after filling. He said it was standard procedure to use the topsoil to backfill.

Commissioner Willey said they were balancing the need for resource for building development and agricultural soils. She said she would vote against the motion because she wanted the Board of County Commission to have the benefit of the Planning Commission discussion. She said soil is an active ecosystem and when it is scrapped up and put it in a pile it is no longer the same thing.

Commissioner Kelly said reclamation would happen on the borrow pit and there was value in it not being continued as a barren borrow pit.

Motion carried 7-3, with Commissioners Carpenter, Paden, and Willey voting in opposition. Commissioners Butler, Carttar, Kelly, Sands, Sinclair, Struckhoff, and Weaver voted in favor of the motion.

PC Minutes 11/14/18

**ITEM NO. 5A COMPREHENSIVE PLAN AMENDMENT TO H2020, CHAPTER 6,
COMMERCIAL LAND USE (SLD)**

CPA-18-00365: Consider a Comprehensive Plan Amendment to Horizon 2020, Chapter 6, Commercial Land Use, and to Chapter 14 Specific Plans, to amend the Southeast Area Plan to include the southeast corner of the intersection of E. 23rd Street and O'Connell Road related to development located at 2110, 2120 & 2130 Exchange Ct. Submitted by CFS Engineers, for Eastside Acquisitions LLC, property owner of record.

**ITEM NO. 5B REZONING 4.31 ACRES FROM CO TO CC200; 2110, 2120, 2130
EXCHANGE CT (SLD)**

Z-18-00364: Consider a request to rezone approximately 4.31 acres from CO (Office Commercial) District to CC200 (Community Commercial) District excluding and prohibiting specific uses within the CC200 District, located at 2110, 2120 & 2130 Exchange Ct. Submitted by CFS Engineers, for Eastside Acquisitions LLC, property owner of record.

STAFF PRESENTATION

Sandra Day presented items 5A-5B together.

APPLICANT PRESENTATION

Bill Newsome said the ownership group purchased the property in 2003 and that there had not been one viable interest in the property. He said the current zoning was not of interest to the market. He felt the CC200 zoning was consistent with the commercial land use plan but staff did not agree with that. He said he held a neighborhood meeting and zero land owners attended. He said one of the allowed uses in CC200 is a hotel/motel use and is a viable use for the tract. He said he was disappointed with the staff recommendation and respectfully disagreed. He asked Planning Commission to approve the comprehensive plan amendment and the staff recommendation zoning. He said as the process for the new comprehensive plan takes course he would want a hotel/motel use on the site to be part of the approved uses. He said if he gets a contingent contract on the site the CN2 zoning would provide more marketing sizzle. He said if he receives a contract he would come back to request a zoning change. He felt the site was the perfect location for a hotel/motel use. He said a hotel was not necessarily a destination use. He said it would keep dollars in Lawrence.

PUBLIC COMMENT

No public comment.

COMMISSION DISCUSSION

Commissioner Kelly said they were between Horizon 2020 and Plan 2040. He asked staff for a recommendation with Plan 2040.

McCullough said that was what steered some of the staff analysis. He said there were clear differences in land use patterns on each section of the node. He said the southwest corner was developed with residential uses. He said Plan 2040 was aspiring to integrate some neighborhood commercial into residential areas. He said there was a need for commercial use to serve the area. He said staff was supportive of expanding the commercial node but wanting to be respectful of the residential area.

Complete audio & video from this meeting can be found online:

<https://lawrenceks.org/boards/lawrence-douglas-county-metropolitan-planning-commission/>

Commissioner Sands said the applicant was asking for a more intense zoning with restrictions. He wondered how it was different than conditional zoning.

McCullough said if Planning Commission felt the neighborhood commercial zoning was appropriate in this context then it provided more process and analysis to look at some of the uses in the CN2 zoning district through the Special Use Permit process than just striking certain uses from the CC200 zoning. He said the applicant felt the hotel use would be viable. He said the hotel use was not permitted in the CN2 zoning district but would be permitted in CC200. He said it boiled down to what was appropriate at the intersection that would be both commercial minded and residential minded. He said there was plenty of CC200 not developed in the area.

Commissioner Sands said the applicant intended to ask for rezoning for a hotel/motel use. He wondered if that was an appropriate use to border multi or single-family residential with no transition. He asked about the approximate density of the RM15 area.

Day said it would probably hit 14-15 units per acre. She showed the concept plan on the overhead. She said the landscaping would be the only buffer.

Commissioner Willey asked about the proposed building height for the multi-dwelling project.

Newsome said two-story.

Commissioner Willey said the existing CC200 zoning to the east of E 1600 Rd/O'Connell Road and south across 25th Terrace was single family with no transition.

Day said some of the uses in CC200 were fairly intensive for the site.

McCullough said the current CC200 was not built out so there were alternatives.

Commissioner Carttar said the rezoning was contingent on the comprehensive plan amendment. He asked staff to clarify the contingency.

McCullough said staff operated on the concept of nodal development for commercial uses versus stripping it out. He said part of the analysis was whether the Southeast Area Plan should be expanded to include this corner. He said the Southeast Area Plan, as proposed by staff, would expand it to a CC200 node with limitations. He said many sector and nodal plans call out the different quadrants that serve different needs.

Commissioner Carttar asked if this was an artifact of the fact that this was essentially undeveloped land when the plan was developed.

McCullough said O'Connell Road was a natural ending point to the plan because much of it had been developed residential on the west side.

Day said there were other factors. She said the city had adequate services up to O'Connell Road but jumping that road was a significant planning concern that had to incorporate how services would be extended east. She said the boundary had to be somewhere. She said that was before there was focus on nodal design concepts that have been embraced over the last 10 years. She said the plan

did not look at the node because it was focusing on what was happening on the north side of 23rd Street.

Commissioner Willey said it seemed a CN2 zoning designation was safe. She said she was not convinced that the uses allowed in CC200 would be inappropriate since it was industrial on the north side of the node and already CC200 on the east side of the node. She said there was already close proximity between CC200 and single-family in the area plan. She said it did not bother her to make that transition from RM15 to CC200 in that area.

Commissioner Carttar agreed with Commissioner Willey. He said this was a major thoroughfare and seemed appropriate.

Commissioner Kelly said if that's not the place for commercial where is the commercial neighborhood district for that area.

Commissioner Carpenter said this type of intersection does suggest a neighborhood commercial. He said it would eliminate conditional zoning. He said multi-family housing was not a buffer to single-family anymore because the new Plan 2040 would incorporate all types of housing in the same neighborhood. He said he was leaning toward the staff recommendation. He felt it met the goals of what they were discussing.

Commissioner Sinclair asked if Planning Commission approved the CC200 zoning with restrictions would another zoning request be needed in the future to undo the restrictions.

Day said yes.

Commissioner Willey asked Mr. Newsome if there were other uses allowed under CC200 but not allowed under CN2 zoning that concerned him.

Newsome said he did not have a hotel project in his pocket. He said there were other uses in CC200 that he generally liked, such as farm machinery (ex: John Deere dealership). He said the only reason he focused on a hotel/motel use was because it would be more probable. He said he met with staff and tried to take the uses off the table that were unrealistic. He said between the three lots it was about 4.5 acres and there were some uses in CC200 that could not be on the southwest corner due to the size. He said there was about ½ acre of unusable space due to the regulators on site. He said the southeast corner had sat with the current zoning for some time with no development. He said the hotel across the street was doing well.

Commissioner Carttar said Horizon 2020 identified a node for neighborhood commercial south of the Douglas County jail.

Day said that was part of the Southeast Area Plan.

Commissioner Sinclair said it looked like a lot of the uses that would be beneficial for neighborhood commercial were present in the CC200 zoning district also. He said there was the potential for a few other uses they would not want to see.

McCullough said that was true.

Commissioner Sinclair said he did not have a problem with a hotel but liked the idea of having services for the neighborhood. He asked if the owner of the abutting multi-family development was aware.

Newsome said the owner was the same party.

McCullough said one of the approaches is to look at context of uses and if tasked with developing a nodal plan first, how would you designate each corner of the intersection within its context if there was no zoning request. He said staff's approach was to give weight to the residential component of everything that exists west of O'Connell Road.

Commissioner Kelly said they needed to give neighborhood commercial a chance. He said they were looking forward to spaces where neighborhoods have access to services. He said a hotel was a great use but that it or heavy equipment sales was not something the residents next door would use. He said if they were expecting people to age in place there needed to be services. He said there was opportunity for larger commercial development across O'Connell Road.

Commissioner Butler said the land owner had desires for his property. She said a hotel did not give her heartburn. She said she would support the comprehensive plan amendment and rezoning.

Commissioner Willey agreed with Commissioner Kelly, that they should give the neighborhood commercial a chance.

ACTION TAKEN on 5A

Motioned by Commissioner Carttar, seconded by Commissioner Carpenter, to approve a Comprehensive Plan Amendment, CPA-18-00365, to *Horizon 2020*, Chapter 14 Specific Plans, to amend the Southeast Area Plan to include the southwest corner of the intersection of E. 23rd Street and O'Connell Road related to property located at 2110, 2120, & 2130 Exchange Court, and forwarding the recommendation to the Lawrence City Commission.

Commissioner Struckhoff said in general it was an appropriate location for a hotel but that he agreed with Commissioner Kelly's comment about giving neighborhood commercial a chance. Said he would support the motion.

Motion carried 8-2, with Commissioners Butler and Sinclair voting in opposition.
Commissioners Carpenter, Carttar, Kelly, Paden, Sands, Struckhoff, Weaver, and Willey voted in favor of the motion.

Motioned by Commissioner Sands, seconded by Commissioner Struckhoff, to approve and authorize the Chair to sign Planning Commission Resolution PCR-18-00552.

Motion carried 8-2, with Commissioners Butler and Sinclair voting in opposition.
Commissioners Carpenter, Carttar, Kelly, Paden, Sands, Struckhoff, Weaver, and Willey voted in favor of the motion.

ACTION TAKEN on 5B

Commissioner Sands said he would staff the staff recommendation of CN2.

Motioned by Commissioner Sands, seconded by Commissioner Paden, to approve rezoning, Z-18-00364, approximately 4.31 acres, from CO (Office Commercial) District to an applicable commercial district based on the lesser change table set out in Section 20-1303(c), recommending approval of CN2 (Neighborhood Commercial Center) District and forwarding the recommendation to the City Commission with a recommendation for approval to CN2.

Motion carried 8-2, with Commissioners Butler and Sinclair voting in opposition.
Commissioners Carpenter, Carttar, Kelly, Paden, Sands, Struckhoff, Weaver, and Willey voted in favor of the motion.

DRAFT

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**ITEM NO. 6 REZONING 1.6 ACRES FROM IL TO IBP; 1314 RESEARCH PARK DR
(SLD)**

Z-18-00481: Consider rezoning approximately 1.6 acres from IL (Limited Industrial) District to IBP (Industrial/Business Park) District, located at 1314 Research Park Drive. *Initiated by City Commission on 11/6/18.*

STAFF PRESENTATION

Sandra Day presented the item.

PUBLIC COMMENT

No public comment.

COMMISSION DISCUSSION

Commissioner Sands asked if the Design Guidelines still applied.

Day said yes.

Commissioner Willey asked if the City made this request.

Day said yes.

ACTION TAKEN

Motioned by Commissioner Sands, seconded by Commissioner Carpenter, to approve the request to rezone, Z-18-00481, approximately 1.685 acres from IL (Limited Industrial) District with use restrictions, to IBP (Industrial/Business Park) District, and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

Unanimously approved 10-0. Commissioners Butler, Carpenter, Carttar, Kelly, Paden, Sands, Sinclair, Struckhoff, Weaver, and Willey voted in favor of the motion.

PC Minutes 11/14/18

ITEM NO. 7 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; CONDITIONAL ZONING (SLD)

TA-18-00430: Consider a Text Amendment to the City of Lawrence Land Development Code, to define and clarify the use of conditional zoning. *Initiated by Planning Commission on 8/22/18.*

Item 7 was deferred prior to the meeting.

DRAFT

PC Minutes 11/14/18

MISCELLANEOUS NEW OR OLD BUSINESS

Consideration of any other business to come before the Commission.

MISC NO. 1 2018 PLANNING COMMISSION MEETING CALENDAR

Receive revised 2018 Planning Commission meeting dates and submittal calendar.

MISC NO. 2 2019 PLANNING COMMISSION MEETING CALENDAR

Review and consider adopting the 2019 Planning Commission meeting dates and submittal calendar.

Motioned by Commissioner Willey, seconded by Commissioner Carttar, to approve the amended 2018 calendar and new 2019 calendar.

Motion carried 9-1, with Commissioner Kelly voting in opposition.

MISC NO. 3 RESOLUTION REGARDING ETHICS POLICY

Receive Resolution No. 7269, adopted by City Commission on 11/6/18. The resolution is an updated ethics policy which applies to City employees, elected officials, and appointed officials, including advisory board members.

Randy Larkin discussed the resolution regarding the City's updated ethics policy.

Commissioner Sinclair inquired about solicitation of gifts. He asked if the policy would prohibit soliciting donations for charitable causes.

Larkin said as long as it was not done in the name of the City.

ADJOURN 9:19pm

Planning Commission

Key Links



City of Lawrence
Douglas County
PLANNING & DEVELOPMENT SERVICES



Plans & Documents

- [Horizon 2020](#)
- [Sector/Area Plans](#)
- [Transportation 2040](#)
- [2015 Retail Market Study](#)

Development Regulations

- [Community Design Manual](#)
- [County Zoning Regulations](#)
- [City Land Development Code](#)
- [Subdivision Regulations](#)

Online Mapping

- [City of Lawrence Interactive GIS Map](#)
- [Douglas County Property Viewer](#)
- [Submittals to the Planning Office](#)

Planning Commission

- [Bylaws](#)
- [Mid-Months & Special Meetings](#)
- [Minutes](#)
- [Planning Commission Schedule/Deadlines](#)



PLAN 2040

A Comprehensive Plan for Unincorporated Douglas County & The City of Lawrence



City of Lawrence
Douglas County
PLANNING & DEVELOPMENT SERVICES



[Steering Committee Draft Plan \(August 2018\)](#)



[Interactive Map Gallery](#)



[Issue Action Report](#)



[Comprehensive Plan Update Guide](#)



[Planning Commission Orientation Staff Presentation Video](#)



[Horizon 2020 Steering Committee Website](#)

From: JoAnn F <sepiaspirit@hotmail.com>
Sent: Sunday, November 4, 2018 5:02 PM
To: City Hall email <CityHallemail@lawrenceks.org>
Subject: Please IMPROVE Horizon 2040

To the Planning Commission:

I have some concerns about the new Horizon 2040 plan.

Before we bought land to build a home in rural Douglas County, I met with staff in the planning department to see what might be planned in terms of development that could impact the area we wanted to buy and build on and we counted on clear, specific and detailed plans laid out in Horizon 2020 to inform the biggest investment most of us ever make.

Several years later, I came to further appreciate just how important these long term planning documents are when Lawrence developers sought to island annex and then zone heavy industrial, a parcel of land near my home in rural Douglas county. The developers sought to do this, even though it was actually outside the planned area of growth as outlined in H2020 and they very nearly got away with this.

Had they been successful THEY ALONE would have altered the direction of growth in the county in a way that would have diverted resources from things that made much more long term sense for the county as a whole, while costing taxpayers a lot-- not to mention this would have been extremely unjust to those who already built homes and would have seen their value plummet.

Given the new Kansas Law that seeks to dis-empower residents trying to protect their property value and quality of life from large animal confinement operations setting up next door, (The "tyson Law") which also makes it easier for such operations to unfairly use egregious amounts of limited resources like water, while forcing taxpayers to subsidize extra infrastructure and services that become necessary as a result, while contaminating and dirtying the air, water and soil, it is even more important than ever before, that Horizon 2040 contain extremely detailed, specific, and forward thinking measures to help our community have the best chance of having a livable, just place for those who have already moved here...or those considering moving here -- who want a safe healthy home and assurances that the largest investment they may ever make, won't take a huge economic hit, because a CAFO suddenly came to town.

The 2040 plan appears to have only a fraction of the details spelled out in H2020...when it is more important than ever, that the environmental section be EXPANDED.

Please make sure H2040 has extremely detailed goals articulated that will protect streams, forests, air quality, prevent noise, air and light pollution in rural residential neighborhoods. Encourage conservation of all natural resources, encourage home gardening and local organic plant-farming and use of native plant species for restoration and landscaping, reduce erosion -- and whatever other things help to make places livable and loved by those who actually live there and raise families.

Also --- even if some details as I request may be located in other sections, I urge you to ALSO have these things spelled out in the environment section as that will be the most user friendly and accessible to citizens seeking to understand the goals and scope.

Thank you,
JoAnn Farb

November 5, 2018

Memorandum

To: Lawrence/Douglas County Metropolitan Planning Commission
From: Douglas County Heritage Conservation Council
Cathy Dwigans (Chair), Lindsay Crick (Vice-Chair), Michael Delaney, Shelley Hickman-Clark, Kimberly Mahanna-Bellemere, Julia Manglitz, Douglas McKean
Re: **Plan 2040: A Comprehensive Plan for Unincorporated Douglas County and the City of Lawrence**

The members of the Douglas County Heritage Conservation Council have reviewed the August 2018 Draft of Plan 2040, particularly Chapter 6, Natural Resources, and Chapter 7, Community Resources.

The Heritage Conservation Council (HCC) was established by the Douglas County Board of Commissioners in 2011 (Resolution No. 11-19), with authority and responsibilities set forth in the Douglas County Heritage Conservation Plan (HR-13-11-4). The purposes of the Heritage Conservation Plan are to:

- (A) Ensure the conservation of the County's natural and cultural resources.
- (B) Identify, conserve and promote the County's natural resources, prehistoric, historic and cultural heritage through ongoing surveys and studies of natural and cultural heritage resources.
- (C) Implement the strategies and goals contained in Chapter 11 of Horizon 2020 for the protection, development and utilization of historic resources.
- (D) Foster civic pride and promote tourism, particularly as related to the natural resources, pre-settlement history, settlement history, and the themes encompassed in Freedom's Frontier National Heritage area.
- (E) Work in concert with the State Historic Preservation Officer and observe the State Preservation Act, contained at K.S.A. 75-2701 et seq., as amended.
- (F) Support education programs to increase public awareness of and support for the County's historic environment.

As part of the Heritage Conservation Plan, the HCC is responsible for complying with all requirements of the State Historic Preservation Officer to maintain its status as a Certified Local Government.

To further the purposes of the Heritage Conservation Plan, we believe the Douglas County Heritage Conservation Council should be included as an advisory board for Chapter 6, Natural Resources, and Chapter 7, Community Resources, A. Historic Resources and B. Parks, Recreation and Open Space. The HCC also should be notified of other activities governed by Plan 2040 and affecting natural and cultural resources in unincorporated Douglas County.

Thank you for your consideration of these changes.

Cc: Jan Shupert-Arick, Heritage Coordinator, Douglas County

From: Thad Holcombe <tjholcombe@gmail.com>
Sent: Tuesday, November 6, 2018 4:23 PM
To: City Hall email <CityHallemail@lawrenceks.org>
Subject: Submission of Comments regarding 2040 Comprehensive Plan

To: Planning Commission

From: Thad Holcombe

Moderator for LETUS (Lawrence Ecology Teams United in Solidarity - an interfaith network of eight "green" teams representing Muslim, Jewish, Catholic, Protestant faith communities) Lawrence faith communities

I will be elaborating my request for amendments when presenting at either November or December dates established for public comment. I do want to express appreciation for the time, effort, and expertise demonstrated in the 2040 Comprehensive Plan. I will be asking that the Commission consider re-visiting the 2020 Comprehensive Plan, particularly the Environment section. My remarks will focus on rationale for the Commission to consider replacing the 2040 chapter on Environment and section on land, with an amended version of the 2020 Overview on the Environment and Land Resource and Management.

Briefly, my reasons for such an amendment are as follows:

- + The context for a Comprehensive Plan would be strengthened by affirming priority being given to the natural environment as having integral value and not "resource".
- + The Environment Overview is especially relevant given our present need to address the consequences of climate change.
- + The Environment Overview, as amended, provides a more substantial basis for making decisions that effect the land, water and air. Granted, it does ask the reader to spend more time and effort, but the importance of understanding why land, air and water have intrinsic value and not simple "commodities" may begin re-consideration of the way predominant culture has neglected such an affirmation.
- + My request is based on science as well as philosophical and theological rationale.
- + Moving the Environment Section to be in first part of Comprehensive Plan sends a message that the environment is important.

HORIZON 2020 16-1 ENVIRONMENT

CHAPTER SIXTEEN - ENVIRONMENT

"The care of the earth is the most ancient and most worthy, and after all our most pleasing responsibility. To cherish what remains of it and to foster its renewal is our only hope."
- Wendell Berry -

OVERVIEW

Douglas County has a rich and valuable heritage that is characterized by a variety of environmental and natural conditions. Prairies, rivers, forests, wetlands, agricultural soils, and other natural features provide scenic beauty, recreation, natural habitats, flood protection, and opportunities for interpretation, appreciation, and education.

Protecting and enhancing Douglas County's environment, including its built environment, is the focus of this chapter. A livable community must first of all give the protection of the natural environment as first priority. If not, there is the risk of the land, water and air becoming simply "resources" and treated as commodities to be traded. The built environment that is developed is therefore secondary to protecting and enhancing the natural conditions described. The recommendations are intended to foster a healthy environment that contributes to a livable community for all species of life. This concept provides a more comprehensive understanding of sustainability that was stated as the goal

of Horizon 2020 (see Chapter 1): "We will strive to ensure the sustainability of our physical environment, both natural and built, the health of our economy and the efficient and effective functioning of our community."

Environmental quality and the built environment can be at odds if the physical environment is given priority in planning. Appropriate use of natural "resources" with this understanding can mitigate unnecessary negative effect on natural environment, thus practicing sustainability in practical way that encourages a quality of life for all, including. Douglas County residents. Protection and preservation of natural environment is an important component in planning for growth and development in Douglas County since all development activities create some level of impact on the air, water and land mentioned in this chapter. While land development is important to economic vitality, land, water and air and climate protection are increasingly recognized as equally important to the health and vitality of the community and must be protected for future generations. The responsible way to achieve the mutual goals of environmental protection and planned growth is to develop in a sustainable manner, one that is capable of being continued with minimal long term effects on the environment.

There are already a few programs in place that aim to achieve some of the sustainability goals of this chapter, such as the county-wide ECO₂ program which is a tool used to promote the dual goal of open space preservation and economic development. The program uses a concept of net equity that states an amount equal to a portion of the investment of public funds for industrial development be used for open space preservation. In addition, the City of Lawrence adopted a Land Development Code in 2006 which addresses some recommendations of this chapter, including standards for impervious surface coverage, open space requirements, and landscaping. The City and Douglas County also have recently revised the subdivision regulations which include provisions for land divisions which contain environmentally sensitive features. The City of Lawrence also has multiple efforts currently underway with similar goals as presented in this chapter, including work by the Sustainability Advisory Board, the Mayor's Climate Protection Task Force, and the Peak Oil Task Force. These advisory boards review issues and make recommendations to the Lawrence City Commission. Douglas County has recently established a ? (incomplete sentence in PDF)

My focus is on 16-11 in Horizon 2020 and can be compared to 2040 draft.....Horizon 2020 is a more adequate and informative chapter than in the 2040 draft. Serious consideration of adopting this chapter instead of proposed one in 2040 is being asked.

HORIZON 2020 16-11 ENVIRONMENT

LAND MANAGEMENT

"We shall never achieve harmony with land, anymore than we shall achieve absolute justice or liberty for people in there higher aspirations. The important thing is not to achieve, but to strive."
- Aldo Leopold -

This section discusses Douglas County's various land features, which consist of rural woodlands and urban forests, native prairies, and agricultural soils. These provide wildlife habitats, viewsheds, and open spaces, as well as, serving as 'Green Infrastructure', as they provide benefits to the natural and built environments. Like many other parts of the country, land within Douglas County is being impacted by development pressures and agricultural practices. Benefits of preserving and managing diversity of land include growth management, flood control, improved water quality, protection of wildlife habitat, and economic advantages to the community, such as a lower cost to the community for development.

Summary of Issues:

1) Open space network. The creation of an open space network or green infrastructure system minimizes the fragmentation of natural areas and benefits the community by protecting natural habitats, providing appropriate stormwater management, providing open-air recreation areas and promoting sustainable development practices. Open space networks can include:

Topography: Developing on steep slopes can be costly and permanently alters the natural slope of the land which may have detrimental effects on other natural features, stormwater runoff and habitats.

Rural Woodlands and Urban Forests: The trees in rural woodlands and urban forests provide many valuable benefits ranging from:

- Ecological (improving air and water quality),
- Biological (providing wildlife habitat),
- Physical (serving as 'green infrastructure' by providing shade and screening),
- Social (providing areas of scenic beauty and areas for recreation), and
- Cultural (establishing and maintaining the character of the area).

Native Prairies: The tallgrass prairie has an intrinsic value as an endangered ecosystem which is a feature of our national heritage. The prairies provide recreational and educational opportunities, as well as providing habitats for wildlife and plant species. In addition, native prairies play a valuable role in controlling sedimentation, aiding groundwater recharge, and absorbing stormwater runoff.

Endangered Species and Wildlife Habitats: The protection of critical habitats is a principal means of protecting rare and endangered species and also serves to protect other species that use the same habitat. Because development has resulted in fragmentation of wildlife habitats, corridors connecting them should be maintained and protected. The Kansas Wildlife Conservation Plan² includes protection measures for rare and endangered species and is geared toward practices and policies that would help keep common species from being endangered.

² <http://www.kdwp.state.ks.us/news/Other-Services/Wildlife-Conservation-Plan>

HORIZON 2020 16-12 ENVIRONMENT

2) Agricultural soils. High Quality Agricultural Land is recognized as having exceptional quality and fertility, and in Douglas County is generally described as having Capability Class (non-irrigated) I and II soils as defined by the National Resources Conservation Service. This High Quality Agricultural Land is a finite resource that is important to the regional economy. This land requires less intervention to produce high yields of crops with high nutrition and should be protected, preferably for food production.

Goals and Policies:

Goal 2: Properly manage all..... soils, woodlands, native prairies, wildlife habitats, viewsheds and open spaces, to maintain the functions they provide, ensure their sustainability, and improve the environmental quality of the City of Lawrence and unincorporated Douglas County.

Policy 2.1 Development should maintain the natural benefits of existing topography. Development on steep slopes (above 15%) should be done in a manner that encourages the use of the existing topography with minimal grading to minimize adverse effects.

Policy 2.2 Preserve and sustain woodlands within Douglas County.

a. The City and County shall partner with other agencies and institutions to inventory and map woodlands within the county. The inventory and map should identify the different types of woodlands ('high quality natural areas', woodlands which form, or could form, corridors or greenways and riparian woodlands) and provide a ranking system in priority order for protection.

a.1 Develop regulations and incentives that provide different levels of protection for the different types of woodlands.

a.2 Encourage environmentally sensitive site design practices which minimize the unnecessary physical and visual impacts upon the surrounding landscape caused by removal of woodlands.

a.3 Develop regulations and incentives for the protection, maintenance, and improvement of riparian woodlands which include an ordinance defining the stream setbacks and the activity which may occur in the riparian area.

a.4 Develop public outreach and educational programs to increase public awareness concerning the importance of woodlands.

HORIZON 2020 16-13 ENVIRONMENT

b. Protect and increase the urban forest in Lawrence.

b.1 The City shall conduct an inventory of the Urban Forest.

b.2 Adopt an Urban Forestry Master Plan and associated policies, programs, and incentives for the preservation and enhancement of Lawrence's urban forest on both public and private property, through development and zoning codes, emphasizing the use of trees appropriate to the climate of this region.

b.3 Adopt standards for tree care activities and the regulation of tree maintenance contractors that will prevent the serious damage that inappropriate pruning practices cause to Lawrence's trees. Partner with utility agencies regarding appropriate tree location and pruning practices.

b.4 Establish educational programs to foster public/community awareness of, support for, and contribution to Lawrence's urban forestry initiatives, which are directed at establishing the maximum urban tree canopy, maintaining it in a healthy condition and promoting its conservation.

Policy 2.3 Preserve and protect native prairie.

a. Partner with the Kansas Biological Survey, other agencies, and individuals to inventory and map the remaining native prairie remnants within Douglas County.

b. Develop regulations, planning guidelines, management techniques, and incentives for preserving native prairies. The native prairie should be preserved and used as parks and/or open space either through purchase or the use of conservation easements.

Policy 2.4 Preserve and protect natural habitats.

a. Identify and map areas of 'critical habitat', key habitats, and wildlife corridors, including areas that could link together to increase connectivity throughout the City and County.

b. Develop incentives to encourage on-site and off-site habitat connections and/or enhancement of natural areas as part of development projects.

c. Develop regulations that permit only low-impact development with environmentally sensitive design in areas of 'critical habitat'.

d. Increase awareness of the species and loss of habitat through educational and outreach programs.

e. Treat areas identified as key habitats as high priority areas for preservation and protection in the development of regulations, protection standards, and incentives.

HORIZON 2020 16-14 ENVIRONMENT

- f. Develop regulations and incentive programs for the protection and maintenance of wildlife corridors and key habitat areas.
- g. Regulate the placement of roads, trails and utilities with development or infrastructure projects to minimize creation of fragmented natural areas.
- h. Develop a program to encourage and incentivize the connectivity of natural areas whether they are on a particular development site or off-site.
- i. Develop a combination of educational programs, incentives, and development standards that recognize and promote sound management practices by private land owners to maintain the health of natural habitats on private property.

Policy 2.5 Along with community members in Douglas County, identify and define important features that contribute to viewsheds, as well as establish possible protections for viewsheds. At such time, further policies relating to viewsheds may need to be addressed.

Policy 2.6 Preserve existing open space and create new open space areas to preserve and expand a sustainable green infrastructure system.

- a. To maximize the advantages to the community that the natural and built environments provide, open space preservation shall remain a goal especially as it relates to protecting and preserving natural features discussed in the comprehensive plan. This should be done through:
 - a.1 Maintaining and enhancing existing open space.
 - a.2 Creating new designated open space areas.
 - a.3 Creating a large interconnected network of open space.
- b. Incorporate open space evaluation into long range plans to determine in advance of development proposals what areas are suitable for development and what areas would serve better as open space.
- c. The acquisition and continued maintenance of open space that is publicly accessible shall be strongly encouraged.
- d. Promote and encourage eco-tourism to sustain open space and natural areas.

HORIZON 2020 16-15 ENVIRONMENT

Policy 2.7 Encourage the protection of High Quality Agricultural Land in Douglas County for current and future agricultural use.

- a. The protection of High Quality Agricultural Land shall be used as a key assumption in the sector planning process.
- b. Establish tools to protect High Quality Agricultural Land for farming and make its protection economically feasible for the land owner, such as an agricultural easement program, development incentives that encourage the protection of this resource, public/private partnerships, or other funding mechanisms.
- c. Maintain an inventory of High Quality Agricultural Land in Douglas County and track the amount lost to urbanization.
- d. Encourage and develop policies that support agri- and eco-tourism, as well as sustainable local/regional food supply.

From: Pat Miller <urvilemiller@gmail.com>
Sent: Monday, November 5, 2018 7:54 PM
To: City Hall email <CityHallemail@lawrenceks.org>
Subject: Comments on Plan 2040

Lawrence/Douglas County Planning Office:

I am writing this letter to comment on Chapter 6, Section 3 of Plan 2040: A Comprehensive Plan for Unincorporated Douglas County & Lawrence Kansas. This section addresses air quality.

With the exception of subsection 3.5 "Continue conducting the Lawrence-Douglas County Sustainability Office community-wide greenhouse gas inventory every 5 years." the subsections of this section do not state specific actions for meeting the goals.

I would like to see the actions recommended in Horizon 2020, Chapter 16 pages 18-20 that pertain to the appropriate subsections of Section 3 added to them. Policy 3.1 a-g could be added to subsection 3.1, Policy 3.5 a-f to subsection 3.2, Policy 3.3 a-b and Policy 3.4 to subsection 3.3 and Policy 3.6 to subsection 3.4.

I hope you will consider these changes.

Pat Miller
255 N. Michigan St. Apt 25
Lawrence, KS

Sara L. Taliaferro

2145 New Hampshire Street, Lawrence, KS 66046 | 785-842-9754 | saratally.st@gmail.com

November 5, 2018

Comprehensive Plan Committee Members

c/o Jeff Crick, Planner

Lawrence-Douglas County Planning Commission

6 East Sixth Street, Lawrence KS 66044

Dear Comprehensive Plan Committee Members:

The need for water affects every aspect of life in Kansas, from the irrigation that supports agriculture to the drinking water that people use daily in their homes and places of work. The future of the state is tied up in sustainability of safe and accessible water sources, and the Governor's *50-Year Vision For The Future of Water in Kansas* reflects a state-level acknowledgement of the vital role of water in our lives. But even this recognition of a great need for sustainable and safe water sources has only yielded a long-term policy that is mostly voluntary. If we are to reach goals of sustainable water use within Douglas County, it is paramount that we craft a clear, structured, and well-defined planning vision that allows us to be good stewards locally, even if the plan is to serve as a guideline rather than a policy.

The current version of Plan 2040 has reduced thirty-one goals and actions down to eleven general recommendations or suggestions. **None of the suggestions have measurable, quantifiable outcomes, which are of critical importance in defining local policy going forward.** Without measurable, actionable goals, any deliberation over a disputed project will not be informed by strong and specific plan language to help resolve differences. Horizon 2020 has often been cited or referred to by policy makers, planners, developers, and community members when discussing proposed projects, and in instances where language was weak or clear policy definition was lacking, we as a community struggled and got bogged down in dispute. Therefore, we must make every effort to ensure we add more definition rather than trend toward more generalities in our guidelines as we plan for the future of our community.

Here follows some examples:

Horizon 2020, Policy 1.2 aims to "[p]reserve and protect natural surface watercourses". Examples of specific goals to support this policy are Policy 1.2a, "[d]evelop stream setback regulations for both the City of Lawrence and Douglas County to establish stream corridors which provide a buffer that stabilizes stream banks, reduces erosion, preserves riparian areas, mitigates flood hazards, and ensures water quality." Also, Policy 1.2d, "[e]ncourage continued alignment with the Kansa Water Plan, which lists the following measures:...", and then lists specific actions for landscape plants, chemical and water use on lawns, and vegetative filters and distances of livestock operations from watercourses.

Horizon 2020, Policy 1.3 states the intention to "Improve and maintain water quality, particularly sources of public drinking water, through watershed protection measures". It follows with such important and specific goals as Policy 1.3a: "The City and County shall participate in applicable Watershed Restoration and Protection Strategy (WRAPS) programs, focusing on the protection of the Upper Wakarusa and Lower Kansas Watersheds." And, Policy 1.3b, "The City and County shall identify and map priority wetlands, surface water buffer areas, and riparian areas within each watershed." Policy 1.3c is a specific action, namely that "[t]he City of Lawrence should continue participation in the Community Rating System (CRS) program and increase their level of participation in order to achieve a greater discount to citizens on their flood insurance rates. Douglas County should investigate participating in the program as well."

Horizon 2020, Policy 1.7 tasks the community to “[d]evelop stormwater management policies and programs in a manner that ensures water quality and properly controls runoff.” Policy 1.7c makes a specific regulatory reference, recommending that, “[a]s part of the City of Lawrence’s overall stormwater management strategy, maintain regulations and policies that are consistent with the provisions and goals of the Clean Water Act, including its National Pollutant Discharge Elimination System (NPDES) Program, and other federal, state, and local requirements for water quality and environmental preservation.”

These are very specific goals and actions that support clearly defined and crucial policy pieces. Any pertinent regulatory documents or policies are cited.

Compare this to Plan 2040’s Chapter 6 on Natural Resources. Goal 1 states: “Manage all water resources to protect natural habitats, mitigate hazards, and ensure water quality”. More specific policies governing all aspects of water stewardship outlined in Horizon 2020 have been condensed under this more all-encompassing and more general heading. The list of goals that follow lack any specificity or definition for any of the terms listed and give no clear guidelines on how these goals might be accomplished or measured.

Consider, for example, Goal 1.1, which states that, presumably, the Lawrence-Douglas County Planning Commission will “[e]valuate development proposals for their impacts on critical water sources providing drinking water for Lawrence and Douglas County.” What are the evaluation criteria? Are they already in existence, and are they updated regularly? What, if any, regulations exist to govern this? Where would we find them? Could they be referenced here, or could links be provided?

Or consider Goal 1.7, “[i]dentify, preserve, and protect wetlands”. First, did the City and County identify and map wetlands, surface water buffer areas, and riparian areas as stated in Policy 1.3b of Horizon 2020? If so, this should be referenced. And, clearly, if some assessment already exists, then we have clearer action items that should be referenced.

While I understand the desire to create a stand-alone plan that is easy to read, I feel that it is imperative a new comprehensive plan acknowledges the context of its existence on a continuum that includes the document and works that came before it. What goals, if any, remain undone from Horizon 2020? What accomplishments and resources were created under Horizon 2020’s tenure? These should be referenced and documented.

Especially when people’s livelihoods are so tied up in water use, instituting a comprehensive plan that is overly general or with guidelines that are obscure or vague will encourage the tendency of people making decisions with short-term personal benefits. As benefits accrue for the individual, costs are incurred by the environment and by the community as a whole. Therefore, it is critical that any replacement for Horizon 2020 be detailed and clear enough to chart the way forward. I heartily encourage you to take these next weeks to do a constructively critical “walk” around this plan and carefully consider and incorporate the recommendations and concerns of the public rather than simply assure us that our concerns are already addressed. We have an opportunity to create a living document that will guide us well and thoughtfully. Thank you for your stewardship of this process, for the sake of our community.

Most sincerely,

Sara L. Taliaferro

Karen Willey, Chair
Lawrence-Douglas County Planning Commission
Lawrence City Hall
6 East 6th St.
Lawrence KS 66044

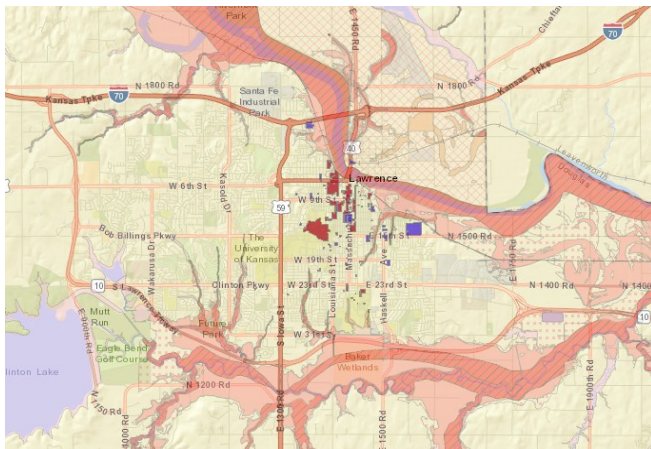
12 November 2018

re: Horizon 2040 update, combined 2017 and 2018 comments

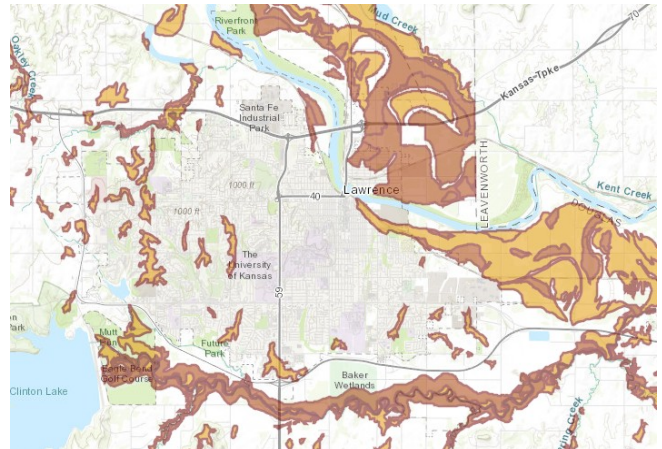
Chair Willey and Commission:

I would like to reiterate our concern that the previously unvetted or reviewed “3-tier growth map” is an inappropriate depiction of the Lawrence future urban growth area. While a 3-tier methodology is an interesting construct, the problem with it is how it places the second tier at a level of disproportionate importance. The first tier is a given; the third tier is nothing more than the current UGA. But the second tier is specifically targeted for urban expansion to take place.

This becomes problematic in areas of 100 year flood plain and the Capability I and II prime soils. Let me point out the shift in prevailing attitudes and actions in recent years regarding flood plain and prime soils protections. First of all, consider how the maps below show how the “peninsula” of Lawrence is constrained on three sides – north, east, and south – by flood plain and prime soils.



FEMA Regulatory Flood Plain & Floodway



Douglas County Capability I and II Soils

It is admirable, in two notable instances, how Lawrence and Douglas County officials have disapproved major urban developments in these sensitive lands.

- North of the Kansas River, the 145 acre Airport Industrial Park proposal met with opposition by citizens concerned about flooding, wetlands, and loss of prime soils. The plan was not approved. From it came the Prime Agricultural Soils Map and a Northeast Sector Plan protecting these natural assets.
- And just east of Lawrence, the case of the 94 acre expansion of the East Hills Business Park into the 100 year floodplain and prime soils illustrates even more dramatically the change of official attitudes to protect flood plains and soils. Initially approved in 2000, officials de-annexed and downzoned the 94 acres in 2014, committing Lawrence to growth out of the flood plain.

I hope you fully grasp how these decisions have changed fundamental values from that of bottom lands being readily developable to that of protecting them as ecological services and economic assets. Because the “3-tier growth map” flies in the face of this new-found wisdom where it earmarks about 215 acres of the Wakarusa floodplain, wetlands, and prime soils as “Tier 2 – planned and expected to urbanize”, south from K-10 Hwy all the way to the Wakarusa River (Plan 2040 draft, Chapter 2 goals, page 18).

The current 2017 edition of Horizon 2020 categorically prohibits expansion of the south Iowa commercial area south of K-10 Hwy – “K-10 provides a physical barrier and edge to the commercial corridor that has developed. Additional retail commercial uses shall not occur south of the highway, except for the possible location of an Auto-Related Commercial Center” (page 6-15).

The 215 acres of Tier 2 opens the floodgates south of K-10 Hwy for “planned and expected urbanization”, completely reversing the Horizon 2020 protections, as well as recent historical precedent that would protect 100 year flood plain and prime soils on the south edge of Lawrence.

The Planning Director has demurred on the threat of the 3-Tier Growth Map, claiming that Tier 2 indicates only “a potential for growth”, that flood plains are “protected by Federal Flood Plain Regulations” as well as “sensitive lands are protected through the site planning process”, and that “the Southern Development Plan (SDP) has land use protections built into it” (27 June 2018 Planning Comm). Not true.

- Tier 2 as “planned and expected to urbanize” says far more than a mere “potential for growth”.
- As Flood Plain Administrator, the Planning Director himself can administratively grant a fill permit under the Federal Flood Plain Regulations.
- As Planning Director, again he himself can administratively issue a site plan, with or without any protections of sensitive lands.
- The Southern Development Plan proved to be no protection at all when the 2015 Planning Commission amended the SDP and H2020, granting regional commercial zoning south of K-10 Hwy.

Please reject the 3-Tier Growth Map as a part of Plan 2040. It has not been authorized in public hearing by either the Planning Commission, the City Commission, nor the County Commission. Just because the 3-Tier Growth Map was accepted as unnecessarily detailed growth projections in Transportation 2040 does not mean that action validates this map for use in Plan 2040. It’s role in Plan 2040 to target areas for “planned and expected urbanization” has far reaching implications that need full vetting and review on it’s own merits (or lack thereof).

Please set aside the 3-Tier Growth Map for a later discussion, as several Commissioners requested at the 27 June 2018 Planning Commission meeting.

Thank you,

Michael Almon

Comprehensive Plan Committee
C/O Jeff Crick, Planner
Lawrence Douglas County Planning Commission
6 East 6th St.
Lawrence KS 66044

23 October 2017

re: H2020 Update Draft, comments

Committee Members:

I do not find this draft document to be a plan as much as a vision statement and promotional piece. While it has the typical plan elements of vision and goals, it lacks the effective means to accomplish those goals. Unlike the existing Horizon 2020 which, for the most part, contains clear and detailed policies and protections to promote equitable development and avoid damaging consequences, this 2017 draft is heavy on generalities, which can readily be nuanced in ways that give a free hand to to do almost anything that land speculators choose to do.

For one thing, the draft writers chose to "streamline" this draft, ostensibly to make it more accessible than the existing "cluttered" Comprehensive Plan, by gutting the content by 75%. The new draft is a "manageable" 116 pages, while the existing Plan is 321 pages. On the face of it, that is a 64% cut to the Plan. But a good 33% of those 116 pages consist of lovely "vision" photos, which instill in the reader a sense of purpose that is little reflected in the actual depleted text. That fragment of remaining text equals a 75.7% reduction from the existing Horizon 2020 Plan.

The word "shall", which means that a stated provision of the Plan must be adhered to, appears only 24 times in the H2020 draft, whereas in the current H2020, the word "shall" requires compliance a total of 377 times. This one aspect of the new draft might be the single most significant change. The vast majority of goals and action items in the already greatly diminished draft Plan are essentially optional. Without enumerating all the ways the draft is toothless, I will give one example.

In the current H2020 Plan, Chapter Six: Commercial Land Use, page 6-15 refers to the existing commercial area of "South Iowa St., 23rd St. to the South Lawrence Trafficway". The wording clearly states:

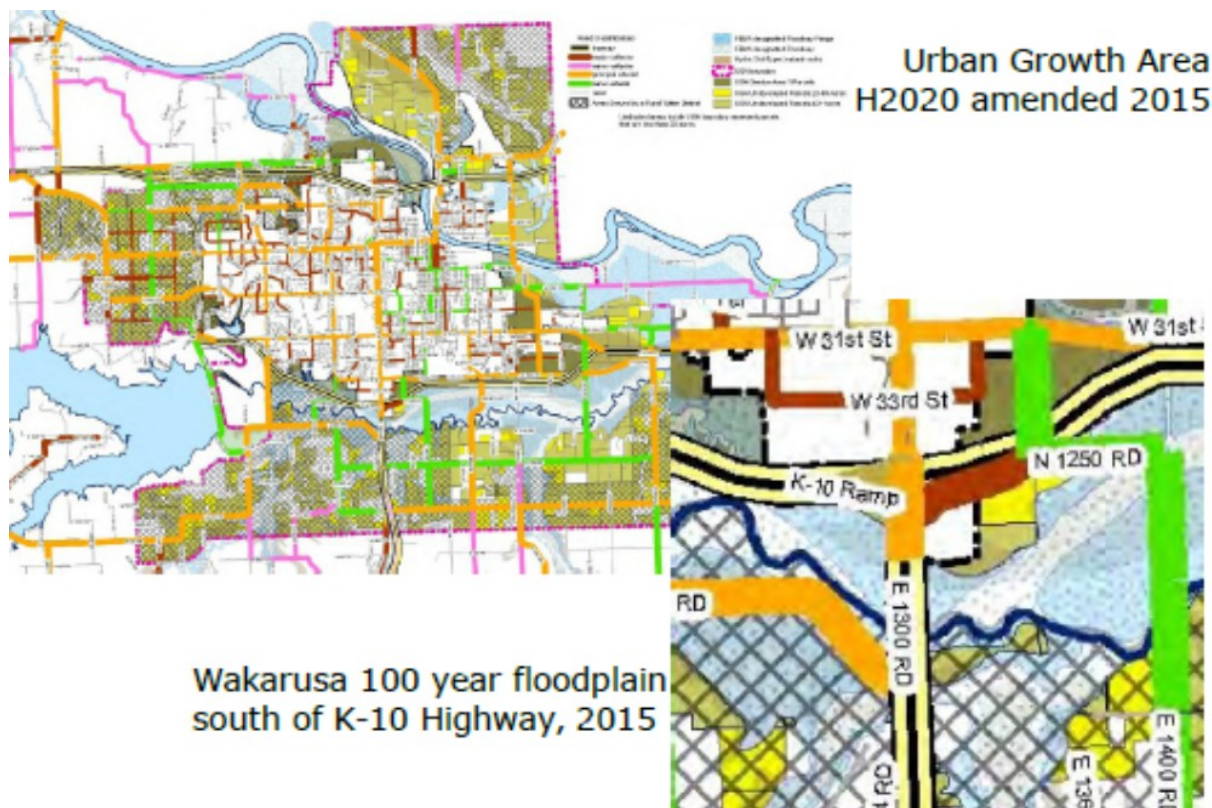
K-10 provides a physical barrier and edge to the commercial corridor that has developed. Additional retail commercial uses shall not occur south of the highway, except for the possible location of an Auto-Related Commercial Center. Two of the four corners of the intersection have existing auto-related uses. Located at the northwest corner is a hotel and an automobile dealership is located on the northeast corner. Because of access to two major highways (K-10 and US-59) the area south of K-10 could be a location for an Auto-Related Commercial Center.

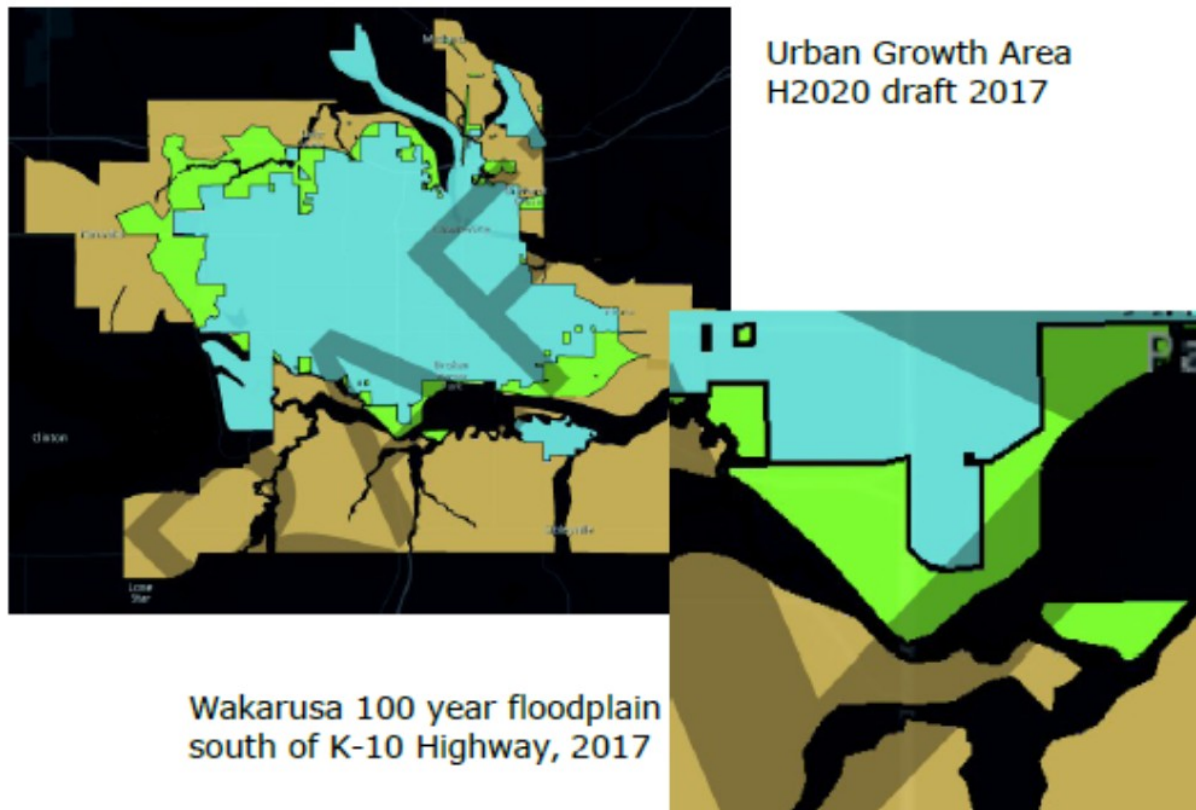
Both corners are an appropriate location for an Auto-Related Commercial Center, provided that the floodplain issues for the southwest corner can be addressed.

The KTen Crossing Regional Commercial Center proposes to build 2.7 million square foot of retail south of the South Lawrence Trafficway (K-10 Highway). Because the proposal is in direct violation of the above restriction to such a use south of K-10, they unabashedly want to delete the entire paragraph from the Comprehensive Plan.

Although Policy 1.6 of the current H2020 Plan calls for limiting new development from encroaching into the regulatory floodplain, and says that floodplains and riparian ways are a constraint to urban development, the key phrase prohibiting the KTen Crossing is "commercial uses shall not occur south of the highway". This development has been proposed four times since 2014, has been litigated in court, and has not succeeded in having the "south of K-10 barrier" deleted from H2020. The newly proposed H2020 draft does delete that phrase, and conveniently will open the floodgates for excessive intrusion into the 100-year floodplain and the Wakarusa Wetlands.

These implications of the new H2020 draft are not idle speculation, but are backed up by the new Urban Growth Area (UGA) map. The area along South Iowa St. south of K-10 Highway is targeted for urban expansion of Tier 2 growth all the way to the Wakarusa River. Below is a comparison of the current UGA map which mostly keeps growth out of the floodplain, and the new UGA map which calls for paving over hundreds of acres of wetlands.





2017 H2020 Draft Plan: Tier 2 of Urban Growth Area -

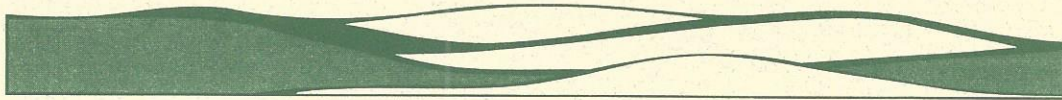


Regardless of any other elements in the draft H2020 that claim to manage and safeguard our community assets, such as protecting sensitive lands, maintaining floodplains, promoting central city density rather than sprawling into rural areas, or ensuring that any new proposal will not negatively impact the existing market, this map demonstrates how deleting that one phrase “commercial uses shall not occur south of the highway” changes the entire thrust of urban expansion.

Please remove all but 1% of the pretty pictures from the draft document, and reinstate the policies and protections that have been deleted from the existing version of Horizon 2020. Re-establish a liberal use of the admonition “shall” in the document so the Plan has some hope of accomplishing the lofty vision and goals that are well represented throughout.

thank you,

Michael Almon



GRASSLAND HERITAGE FOUNDATION

P.O. Box 394 • Shawnee Mission, Kansas 66201

Commission members developing PLAN 2040: A Comprehensive Plan for Unincorporated Douglas County & the City of Lawrence:

Our comment on the plan follows:

Grassland Heritage Foundation preserves prairies in eastern Kansas through education, stewardship, and land protection. We believe that prairies are fundamental elements of our ecological, cultural, and economic heritage. As unique reservoirs of biological diversity and providers of ecological services, prairies must be maintained for future generations.

We applaud the intentions of the Plan 2040 to better manage future development and ensure continued quality of life for the residents, which includes protecting the area's natural resources using concrete actions, as opposed to vague principals. We believe that Plan 2040 should include the following:

- measures to have the city and county identify high-quality prairies and other sensitive natural areas. Such information may be acquired from the Kansas Biological Survey, or other sources.
- measures to ensure that planning staff at both the city and county level are aware of available information identifying high-quality prairies and other sensitive natural areas and use that information to limit developments that would damage or destroy such areas

Board of Governors of Grassland Heritage Foundation

www.grasslandheritage.org

From: Delisle, Jennifer <jdelisle@ku.edu>
Sent: Thursday, November 08, 2018 10:33 AM
To: Jeff Crick <jcrick@lawrenceks.org>
Cc: Karen Willey <karenwilley1@gmail.com>
Subject: Comments on Comprehensive Plan

Hi Jeff;

I have attached our comments on Plan 2040.

I hope you will find these comments useful. Let me know if you have any questions about them.

Jennifer

Jennifer M. Delisle, Research Associate
Kansas Biological Survey
Takeru Higuchi Bldg.
2101 Constant Ave.
Lawrence, KS 66047
785-864-1538
jdelisle@ku.edu

Chapter	Goal	Action Item	Suggested changes	Comments about the suggested change
2A	1	1.5	Add 'and other sensitive lands' after '... agricultural lands'	
2A	1	1.6	Add 'and other sensitive lands' after '... high-quality agricultural soils'	
2B	1	1.5 (new)	add "Revise residential development regulations to better protect sensitive lands."	
6	1	Intro	Reword: 'Water plays a vital role in both our natural and built environments. Managing water resources ensures that water quality is maintained for drinking sources, for wildlife, and for recreational purposes. It also is vital to manage water quantity to limit and mitigate flooding throughout our community'	Bold type indicates two concepts to include in statement. Complete sentences provided as suggestion.
6	1	1.4	Reword: 'Develop stream corridor buffers to preserve riparian habitat, protect water quality, and reduce soil erosion.'	These are the usual benefits of stream buffers.
6	2	Intro	Add: 'and ecosystem services such as flood control, climate regulation, water purification, and pollination' after '... 'wildlife habitats and open space...'	
6	2	2.2	Remove the word 'sustain'	meaning is vague; does it mean 'manage'? If so, who is doing the management?
6	2	2.4	Change to 'Preserve native prairies through the development of regulations and incentives.'	uses same wording as 2.2.
6	2	2.5	Change to 'Establish corridors of wildlife habitat connecting parks and open spaces'	
6	Sensitive Lands sidebar		See definition below	
6		2.9	Possible wording: 'Address invasive species on municipal and county lands, with priority given to non-chemical methods'	
6		2.10	Add new item: 'Establish a Douglas County Open Space program to protect sensitive lands'	
6		4.1	Change 'harvesting' to 'extraction'	
6		4.2	Change first instance of the word 'extraction' to 'use'	
7B		Vision	Change to 'Create and maintain a variety of active and passive open spaces to protect sensitive lands and to provide options for residents of all ages to lead a healthy and active lifestyle'	
7B		2.1	Replace 'land dedications' with 'conservation easements'	Mimics definition of open Spaces in sidebar; refers to a defined term "sensitive lands"
7B		2.2	Change '...key natural and historic areas' to 'sensitive lands and key historic areas.'	meaning of 'land dedications' is vague.
7B		2.4	Add new item: 'Establish a Douglas County Open Space program to protect sensitive lands'	key natural areas' is not defined. Use 'sensitive lands' which is defined in the document.
7B		Intro	Change to 'Creating active and passive open spaces as the community grows is necessary..'	
7B	3	3	Change to 'Expand existing active and passive open space systems'	Mimics use of the defined term 'Open Spaces' rather than introducing the term 'parkland'.
7B	3	3.4	Reword: 'Locate active open spaces near community facilities..'	Remove 'Identify' because presumably these areas already are known. Provides continuity by using terms already defined. Park and recreation areas are included in the definition of Activ
7B	4	4.1	Define 'community' .	It is not desirable to locate passive open spaces such as nature preserves and scenic overlooks near community facilities.
7B	4	4.4	Change to 'Connect lands that provide continuity for floodplains and watercourses and as wildlife corridors'	Does 'community' refer only to Lawrence? Should it?
7B	5	5	Change to 'Preserve and restore...'	
7B	5	5.1	Change to 'Promote sensitive land protection through conservation easements and other voluntary mechanisms'	
7B	5	5.2	Change to 'Incorporate sensitive lands into development proposals as preserved features.'	
7B	5		What is a Steep Slope? Add 'or more' after 15 percent.	Conservation easements are not a program. provides continuity throughout the document by using the defined term 'sensitive lands'.

Sensitive Lands definition

Sensitive Lands are places that have unique environmental attributes worthy of retention or special care. They are critical to the maintenance of ecosystem services and healthy plant and wildlife populations. Protection of Sensitive Lands reduces vulnerability to natural hazards, and enhances the quality of places where people live, work, and play. These lands include:

- Floodways and floodplains
- High quality agricultural soils
- Steep slopes
- Wetlands and stream corridors
- Habitats for rare plants and animals
- Native prairies
- Urban forests and rural, high-quality, native woodlands

Dear Chairwoman Willey and Lawrence Douglas County Planning Commissioners,

I would like to commend Jeff Crick and the Plan 2040 Steering Committee for all their efforts and long hours spent on the Comprehensive Plan revision. However, I do have concerns.

My comments are restricted to Chapter 6, "Natural Resources", although I think some could be applied to other chapters as well.

My concerns stem not so much from what is IN Chapter 6, 2040, as from what is NOT in it. On the face of it, Ch. 6 may seem adequate. However, when compared to what is being lost from Horizon 2020, concerns may become more apparent.

My concerns fall into five categories:

1. **LACK OF SPECIFICITY**: Despite Plan 2040's own explanation in Ch.1, that "... action items "Are more *specific statements providing measureable strategies* " , the action items are often vague, broadly written and incomplete. As written, many are actually goals, with no specific means to achieve them. They rarely "provide measureable strategies". This lack of specificity and clarity will likely result in conflicts between various stakeholders, since they are open to interpretation.

2. **IMPORTANT POLICIES AND STRATEGIES OMITTED**: A number of important policies and action strategies from Horizon 2020 were omitted from 2040. The following are a few of many examples:

A. H2020 Policy 1.2d1: "Use native plants in yards and gardens: they need fewer chemicals and water". (Additionally we now understand that using native plants in landscaping is essential for the food web in a world of diminishing natural areas).

B. H2020 Policy 1.2d3: "Separate livestock operations from streams with a vegetated filter and adequate distance".

C. H2020 Policy 1.3b: "...identify and map priority wetlands, surface water buffer areas, and riparian areas within each watershed".

D. H2020 Policy 1.4b: "Develop a wetland policy which promotes protection, enhancement and restoration...".

E. H2020 Policy 1.7e: "Use nonstructural or natural approaches to storm water system design...rain barrels, rain gardens, bio-retention swales, pervious paving materials and limit use of impervious paving surfaces".

F. H2020 Policy 1.7g: "Encourage environmentally sensitive farming methods, such as terracing, buffering, the use of no-till farming practices, etc., near surface watercourses to reduce pollution, stabilize stream banks and prevent erosion".

G. H2020 Policy 2.4g: "Regulate placement of roads, trails and utilities...to minimize creation of fragmented natural areas".

3. **INFORMATION SCATTERED AND DIFFICULT TO ACCESS**: In a letter I submitted to the steering committee in October, 2017, I expressed my concerns that the draft plan removed "specific policies, strategies for implementation and recommendations for incentives, regulations and education....". The Planning Office responded that "Special Land Use Plans and incorporated policy plans were used to a higher degree, thus reducing the number of policies required in the main body of the Plan." I wholeheartedly agree that this information should be incorporated in other relevant plans, chapters and documents. But they should not be removed from this chapter. To do so makes it next to impossible to follow the thread from vision to goal to implementation strategy. I could not find any of the Ch 6 related information I sought in Special Land Use Plans. One of the primary goals of this Comp

Plan revision was that it be more accessible to the general public. However this manner of organizing, which scatters related information between chapters and different documents, makes it next to impossible to find the information one seeks. If one is looking for information on Natural Resources, they should be able to find it in the Natural Resources chapter. It should be included in the Natural Resources chapter *as well as* incorporated in other relevant chapters and plans.

4. OVERVIEW INFORMATION OMITTED: Horizon 2020 included an Overview at the beginning of each chapter giving background, context and rationale of that chapter. It also contained useful strategies. Some of this was highly valuable in understanding the chapter, but have been completely eliminated from 2040.

Strategies listed in the Overview included “Establish effective incentives and regulations that promote sustainable and efficient management of environmental resources”, and “Develop educational programs to foster community awareness.....”. The next page states “Code regulations shall be developed to achieve the policies discussed in this chapter”. I doubt many incentives or regulations or educational programs have been developed at this time, and yet, while occasionally mentioned in 2040, they are largely omitted. Have we abandoned those necessary goals?

5. NEED FOR ROBUST CLIMATE CHANGE PLAN: Within the past month, the U.N. International Panel on Climate Change was released. It revealed a grim prognosis for the planet, worse than previously thought. It concluded that if we don’t cut our greenhouse gasses emissions by 46% in the next 12 years the cost to humanity is enormous and irreversible. The effort needed to limit global warming requires aggressive action at all governmental levels. Action item 6.1 recommends adopting a climate change adaptation plan. This plan should include prevention as well as adaptation. And it should be initiated immediately!

I encourage you to compare Ch 16 “Environment” of Horizon 2020 with Ch 6 “Natural Resources”, of Plan 2040. Which is truly “Comprehensive”? If you learned that a large development was proposed to be built next door to your home, which would be most helpful to you? Which would be most helpful to you as a Planning Commissioner? Horizon 2020 isn’t perfect. Plan 2040 has much to commend. I’m not suggesting we keep H2020, nor that we disregard 2040, but I am suggesting that in an attempt to abbreviate the Plan we have gutted some important guidelines and strategies, while at the same time created a plan that is difficult for the general public to navigate. I fear that many substantial environmental protections have been lost.

The Environment chapter of H2020, went further than any other such document had ever gone to outline the need for environmental protection in Douglas County. It suggested necessary goals and the concrete means to achieve them. It was imperfect but it was an enormous step in “Consider(ing) the impact upon environmental and natural resources in planning and development efforts”.* Approval of that chapter was a contentious, hard fought battle. Let’s not lose what was so difficult to achieve.

Sincerely,
Pennie von Achen

Horizon 2020, Chapter 16, page 16-2.

From: Sandy Beverly <sbeverly@sunflower.com>
Sent: Wednesday, December 12, 2018 9:04 AM
To: City Hall email <CityHallemail@lawrenceks.org>
Subject: To Planning Commission re: Plan 2040

Dear Planning Commissioners:

I'm writing to express concerns about the Natural Resources chapter in Plan 2040. The new chapter lacks specific goals, implementation strategies, and recommendations for regulations, incentives, and education. It is very incomplete and will be ineffective. In short, it lacks specificity and "teeth." I stand with LETUS, whose representatives will speak at the Dec 17 meeting. I will be in the audience to show my solidarity with LETUS.

For contextual information, I urge you to read the following:

https://streets.mn/2018/11/07/everyones-2040-plan-sucks/?utm_source=Sightline%20Institute&utm_medium=web-email&utm_campaign=Sightline%20News%20Selections

<https://www.newyorker.com/magazine/2018/11/26/how-extreme-weather-is-shrinking-the-planet>

Thank you for your volunteer service to Lawrence and Douglas County.

Sincerely,

Sandy Beverly

From: Thad Holcombe <tjholcombe@gmail.com>

Sent: Friday, December 14, 2018 11:57 AM

To: City Hall email <CityHallemail@lawrenceks.org>

Subject: To Planning Commission - Comments one 2040 Comprehensive Plan for Dec. 17 Meeting

To Planning Commission Members:

I will be speaking during public comment at the Dec. 17 meeting. At that time, I hope to again elaborate on these comments and some of the concerns that LETUS (Lawrence Ecology Teams United in Sustainability) has regarding the 2040 Comprehensive Plan. I will repeat to some extent what was presented at the November meeting of the Planning Commission. Since that meeting, the information on how urgent it is to act regarding consequences of climate change has seemed to exponentially increased! Most likely you have all been sent the following essays and newspaper articles:

https://streets.mn/2018/11/07/everyones-2040-plan-sucks/?utm_source=Sightline%20Institute&utm_medium=web-email&utm_campaign=Sightline%20News%20Selections

<https://www.newyorker.com/magazine/2018/11/26/how-extreme-weather-is-shrinking-the-planet>

(This is a difficult read...I had to take a break to finish it. I would like to lift up a very small portion of it, but one that is very important -

"Humans share the planet with many other creatures, of course. We have already managed to kill off sixty percent of the world's wildlife since 1970 by destroying their habitats, and now higher temperatures are starting to take their toll." (pages50-51) This statement is certainly worth of consideration when planning for "growth".

I would also like to add the comments made by the Sustainability Advisory Board (These were sent to you previously.):

 **SAB Comments on Comp Plan.pdf**

LETUS would like to be in support of these comments. We particularly would like to emphasize the following suggestions made by SAB:

Adoption of a Climate Adaptation Plan

Additional goals for "Natural Resource" ("Environmental Stewardship) - Chapter 6:

- Make Lawrence 100% renewable energy city
- Promote deconstruction industry and more efficient recycling of building materials

Understand "role of sustainability in our community's development" along with inclusion of "sustainability". For a definition go to <https://lawrenceks.org/sustainability/about>.

We also agree that a major concern in growth are threats of unsustainable traffic congestion and affordable housing - these need to be addressed.

The rest of the SAB report is important as well, esp. comments on Chapter 7: Community Resources and Chapter 8: Implementation.

Certainly the task of the Planning Commission is occurring at a historic time, given the growing recognition of climate change consequences. We would like to continue supporting your endeavor.

Thad Holcombe
Moderator of LETUs

From: Tom Birt <birt.tom@gmail.com>
Sent: Sunday, December 16, 2018 10:21 PM
To: City Hall email <CityHallemail@lawrenceks.org>
Subject: P2040 + oil and gas in NE sector

see attached
or
December 15, 2018

Planning Commissioners,

Please consider that the Introduction and Growth chapters of P2040 should demonstrate our town's commitment to get us off fossil fuels and commit to 100% reliance upon renewable energy. Why? It's good for growth and good Press. Also, it demonstrates our commitment to being a resilient community in the face of Climate Chaos.

It would be bad for all of us (and bad Press) if the City of Lawrence and Douglas County ignored the danger posed by current and past activities of the oil and gas industry. For example, according to Kansas Geological Service maps, there are [oil wells](http://maps.kgs.ku.edu/oilgas/index.cfm) (<http://maps.kgs.ku.edu/oilgas/index.cfm>) within the boundary of Plan 2040's NE Sector. As you can see from the KGS map, these wells are ESE of the intersection N 1620 Rd and E 1600 Rd which is very close to the Kansas river. These wells are in the "Lawrence Oil Field" of the now infamous Squirrel formation. The KGS map shows four currently producing oil wells. Additionally, the map shows one "inactive" well (with no plugging date), a dry and abandoned well (with no plugging date) and a *gas* well on a lease named "Community" that was completed in 1945 (again, no plugging date).

In Douglas county SE of Lawrence the density of oil and gas wells increases. And if you look at the KGS map you'll see that this is an issue throughout east central Kansas. After all, the first oil well west of the Mississippi was [drilled one mile east of Paola in 1860](https://aoghs.org/petroleum-pioneers/kansas-mid-continent-oil-fields/) (<https://aoghs.org/petroleum-pioneers/kansas-mid-continent-oil-fields/>).

Finally, page 6 of P2040's introduction states: "If a [development] proposal does not comply with Comprehensive Plan requirements, then the applicant must pursue a plan amendment". Does this mean that a developer would only "pursue a plan amendment" if their plan didn't comply with a P2040 **requirement**?

Our town's Sustainability Advisory Board, the Climate Protection Task Force, and LETUS suggest a *shared* vision for Lawrence and Douglas County.

Thank you for your consideration,

Tom Birt
930 Missouri St.
Lawrence, Ks

Commissioners,

Page 6 of P2040's introduction states: "If a (development) proposal does not comply with Comprehensive Plan **requirements** (emphasis added), then the applicant must pursue a plan amendment".

Does this mean that a developer would only “pursue a plan amendment” if their plan didn’t comply with a P2040 **requirement**? This draft of P2040 requires very little but it does have a lot suggestive language.

According to the Kansas Geological Survey there are [oil wells](http://maps.kgs.ku.edu/oilgas/index.cfm) within the boundary of the NE Sector. there are 5 are oil

<http://maps.kgs.ku.edu/oilgas/index.cfm>

The Lawrence Oil Field within the Squirrel formation

Tufte Enterprises, LLC operates 1 “recompleted well”.

Circle E Investments operates 4 producing wells./

1 dry and abandoned well (Chas Wise lease) that was spudded in 1940.

http://www2.ljworld.com/weblogs/town_talk/2018/feb/02/t-o-the-surprise-of-neighbors-bulldozer-b/

get the town off fossil fuels by building local food, energy, and economic infrastructure toward this revolutionary notion of resilience and being able to bounce back from unexpected changes.
environmentally responsible manner" page 14

"Sensitive Lands are part of the natural environment that provide habitat for wildlife, endangered ecosystems, or presently unique settings that are rare in Douglas County. By protecting these designated spaces we can protect natural habitats, provide recreation areas, and help minimize development impacts in sensitive areas. These include: • Endangered Species Habitats • Floodway and Floodplain • High Quality Agricultural Soils • Native Prairies • Rural Woodlands and Urban Forests • Wetlands & Stream Corridors • Steep Slopes" page 67

2.6 Consider the complete natural system in identifying and preserving sensitive lands as individual developments occur to maintain continuity throughout the ecosystem. page 68

3.9 Protect environmentally sensitive lands as new and existing areas develop or redevelop.

1.4 Sites shall primarily be out of regulatory floodplains. 1.5 Sites shall contain minimal slopes. 1.6 Protect environmentally sensitive lands as new and existing areas develop or redevelop. Chap 2 page 31

Staying ahead of new trends and needs requires a proactive market response for new developments to a constantly changing environment. Chapter 2 page 32.

From page 24 of the Climate Protection Task Force Plan: This CPTF Climate Protection Report was modeled on the Climate Action Plan for Norman, OK, a city of similar size and demographics to Lawrence, KS. Norman Oklahoma is working towards having 100% of its energy come from renewable sources.

Chapter 8 Appendix
page 102 Implementation of Chapter 6 Natural Resource

D: Implementation

PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
02/27/2012

ITEM NO. 2A Z-18-00505 from IG (General Industrial), CS (Commercial Strip) and OS (Open Space) Districts to CD-PD (Downtown Commercial - PD Planned Development Overlay) District; 16.116 acres (SLD)

STAFF RECOMMENDATION: Staff recommends approval of the request to rezone approximately 16.116 acres, from IG (General Industrial), CS (Commercial Strip) and OS (Open Space) Districts to CD-PD (Downtown Commercial - Planned Development Overlay) District and to affirm the findings for publication of PCR-1-1-12 and CPA-11-8-11 expanding the identified boundaries of Downtown Lawrence, located at 311, 317, 401, 409, 415, 501, & 505 N 2nd St., based on the findings presented in the staff report and forwarding it to the City Commission with a recommendation for approval.

Reason for Request: *In lieu of producing a complicated set of design guidelines for the area, the applicant along with the planning director, agree that a planned development is more appropriate for this project. Rezoning the property is required for this process.*

KEY POINTS

- The request includes multiple parcels and multiple owners.
- Property owners included in request represent both public and private groups and individuals.
- The Planning Commission considered and approved a development request for CD (Downtown Commercial) District on February 27, 2012, subject to conditions.
- The City Commission approved the rezoning on May 1, 2012, subject to revised conditions including the submission and approval of design guidelines for the project.
- Guidelines were not approved and a zoning ordinance was not published. The CD zoning was not made effective.
- The related Comprehensive Plan Amendment was also approved but not published. (CPA-11-08-11). This application includes the reaffirmation of the previous approval for the amendment.
- The rezoning requests from 2012 have been consolidated and combined into a single request.
- The proposed development application includes a specific development plan. The 2011 application request only included a concept plan.
- The proposed PD (Planned Development Overlay) District designation is intended to replace the need for separately adopted design guidelines.

ASSOCIATED CASES/OTHER ACTION REQUIRED

- CPA-11-08-11; amendment to Chapter 6 of *Horizon 2020*.
- Publication of zoning ordinance.
- Subdivision approval including preliminary and final plats.
- Public improvement plan approval for infrastructure.
- Site plan and/or development plan approval as applicable.
- Local floodplain development permits as applicable.
- Historic review for portions of the property within the environs.

Previous zoning cases: Approved but not published;

- Z-12-29-11: 1.38 acres from IG (General Industrial) and CS (Commercial Strip) to CD (Downtown Commercial), located at 401 & 415 North 2nd Street.

- Z-12-30-11: 2.14 acres from IG (General Industrial) to CD (Downtown Commercial), located at 0 & 100 Lincoln Street and 151 & 100 Perry Street.
- Z-12-32-11: .83 acres from IG (General Industrial) and CS (Commercial Strip) to CD (Downtown Commercial), located at 409 & 501 North 2nd Street.
- Z-12-33-11: OS & CS TO CD; .34 acres from OS (Open Space) and CS (Commercial Strip) to CD (Downtown Commercial), located at 300, 311, & 317 North 2nd Street.
- Z-12-34-11: 1.61 acres from IG (General Industrial) to CD (Downtown Commercial), located at 139 Perry Street, 505 North 2nd Street & 141 Maple Street.
- Z-12-35-11: .55 acres from IG (General Industrial) to CD (Downtown Commercial), located at 133 Perry Street.
- Z-12-36-11: 1.38 acres from IG (General Industrial) to CD (Downtown Commercial), located at 600 North 1st Street, Block 3.

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Refer to Preliminary Development Plan.
- *Downstream Sanitary Sewer Analysis* – Refer to Preliminary Development Plan.
- *Drainage Study* – Refer to Preliminary Development Plan.
- *Retail Market Study* – Refer to CPA-11-08-11 for discussion of retail market study.

ATTACHMENTS

1. Ownership graphic
2. North Lawrence Drainage Study Build-Out Scenario Map

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

- None received

Project Summary:

Proposed request is for a mixed-use development seeking to capitalize on the recreational component of the Kansas River. The applicant is requesting CD zoning to accommodate development reflecting a similar and complementary development pattern to the downtown area with regard to building heights, setback, and mixed uses. This request is submitted concurrently with a preliminary plat and a preliminary development plan. The preliminary plat will consolidate the land area including vacating existing public right-of-way and establishing new easements and interior access. The preliminary development plan is submitted as a master plan for the entire development, including near-term and full build-out dependent upon the attainment of necessary infrastructure and access.

The river and levee along the west and the railroad and N. 2nd Street along the east define the property. The project includes multiple properties, property owners, and existing rights-of-way. Property owners include the City of Lawrence and the Douglas County Kaw Drainage District.

A development request was made in 2011 including multiple rezoning applications, a comprehensive plan amendment, and a concept plan. The rezoning and comprehensive plan were approved subject to conditions. The rezoning was specifically conditioned upon the submission and approval of applicable design guidelines. This application is being updated and submitted as a commercial zoning district with a PD overlay. The PD overlay will replace the design guidelines for this project. This review assumes the approval of the previous comprehensive plan amendment.

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response: *This area is already zoned commercial. We feel that by doing a Planned Development, the City has greater oversight in the process and will be able to review the proposed development in more detail.*

Horizon 2020 recognizes N. 2nd /N. 3rd Street as an existing commercial area within the community. *Horizon 2020* describes this area as:

The Comprehensive Plan recommends N. 2nd Street and N. 3rd Street play an enhanced role in the community as a commercial corridor, acting as an important entryway/gateway to Lawrence. This corridor is considered an *Existing Strip Commercial* area. The Comprehensive Plan identifies the intersection of the N. 3rd Street and I-70 as a possible location for an Auto-Related Commercial Center.

Marginal, obsolete, and underutilized sites and incompatible uses along this corridor should be redeveloped or reconstructed. For example, existing heavy industrial uses along the northern portion of the corridor should be relocated within the planning area and the sites redeveloped with compatible commercial, service, or retail uses. New development and redevelopment shall include improved parking, signage, and landscaping improvements that enhance the overall aesthetic and environmental conditions along the corridor. The city should encourage and work with landowners to undertake property improvement within the area. The city should consider special financing mechanisms, such as benefit districts or tax increment financing to assist in private and public improvement projects for the area.

Historically, the North Lawrence area including the N. 2nd and N. 3rd Street corridor has had repeated floodwater and stormwater problems. The Comprehensive Plan recommends that a comprehensive drainage study be completed as soon as possible and before any additional new development occurs along the N. 2nd Street and N. 3rd Street corridor.¹ The study shall be a joint project between the city and private property owners. The drainage study shall provide a plan for addressing existing flooding and stormwater problems.

Existing Horizon 2020 – Chapter 6 Commercial Land Use:

A key principal stated in *Horizon 2020* regarding the development and maintenance of commercial land use areas is:

- Support Downtown Lawrence as the Regional Retail/Commercial/Office/Cultural Center with associated residential uses through the careful analysis of the number, sale, and location of mixed-use commercial/retail developments in the community. Downtown Lawrence is the cultural and historical center for the community and shall be actively maintained through implementation of the adopted design guidelines that regulate the architectural and urban design character of this regional center.

The plan also supports policies that:

- Encourage infill development and/or redevelopment of existing commercial areas within an emphasis on Downtown Lawrence and existing commercial gateways. Sensitive to the form of site layout and design considerations shall be given to important architectural or historic elements in the review of development proposals.

These statements address the importance of Downtown to the community and the applicability of appropriate and sensitive infill and redevelopment projects. The development and application of design guidelines for the subject property can be required to assure community appropriate context of development.

¹ The North Lawrence Drainage study was completed in 2005.

Both the Downtown area and the N. 2nd and N. 3rd Street areas are identified in *Horizon 2020* as existing commercial areas. The N. 2nd and N. 3rd Street areas are classified as existing strip commercial development on page 6-14 of *Horizon 2020*. The plan recognizes the need to “upgrade” these commercial areas to remain viable in the marketplace, (pg. 6-12).

The plan supports the need and use of overlay districts, and development standards for redevelopment, based on a redevelopment plan. The design is subject to review and approval of a preliminary and final development plan and subject to compliance with historic design guidelines.

Proposed Horizon 2020 – Chapter 6 Commercial Land Use:

CPA-11-8-11 includes new language supporting the proposed area as an extension of the Downtown Area. This report assumes the approval and adoption of the revised language in *Horizon 2020* with regard to this area. The revised Downtown Commercial Center includes the North Mass Development as part of the historic commercial core of Lawrence, but as a secondary activity area along with and similar New Hampshire and Vermont Streets flanking Massachusetts Street today.

Development of the area is expected to include “mixed use, multi-story buildings” as a common building form. The plan recommends the maximum footprint for an individual store is limited to 50,000 gross square feet within this portion of the district. This limitation will be included as part of the development plan.

Staff Finding – The proposed rezoning assumes approval of the comprehensive plan amendment. The maximum footprint of an individual store shall not exceed 50,000 gross square feet. Additionally, specific design guidelines will be implemented through a combination of the applicable historic design requirements and the development plan. The proposed rezoning is consistent with the comprehensive plan with conditions.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

Zoning is summarized as if this application were one contiguous parcel of land.

Current Zoning and Land Use:	IG (General Industrial) District (11.21 acres), CS (Commercial Strip) District (1.23 acres) and OS (Open Space) District (7.92 acres) and FP (Floodplain Management Regulations Overlay District); Existing Development includes Mobile homes, Johnny's Tavern, KP&L tower, parking lots, residential and storage uses and commercial uses.
Surrounding Zoning and Land Use: To the West:	OS (Open space) District and FP (Floodplain Management Regulations Overlay District); Riverfront Park and the Kansas River
To the South:	OS (Open space) District River Front Park and River

To the East: East of Railroad	<p>IG (General Industrial) District east of bridge; city parking lot.</p> <p>GPI (General Public and Institutional) District east side of N. 2nd Street; <i>Union Pacific Depot</i> (City Visitor Center).</p> <p>IG (General Industrial) District and CS (Commercial Strip) east of railroad and FP (Floodplain Management Regulations Overlay District); Mixed commercial and industrial business between railroad and N. 2nd Street.</p>
To the North: North of Lyon Street	<p>OS (Open Space) and IG (General Industrial) District; <i>Riverfront Park</i> (Passive Recreation) and Union Pacific Railroad right-of-way.</p>

Staff Finding – The subject property is surrounded by commercial and industrial zoning along the N. 2nd corridor and confined by the river to the west and the railroad to the east. The project terminates at the north end at Lyon Street. City Park property is located to the north.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response: *This area is currently home to a long-time Lawrence bar/restaurant, as well as other retail establishments. It also houses a few residential structures. In addition to this, there is a the levee trail that is utilized by many citizens for walking and biking.*

This property is part of the North Lawrence area but technically outside of the described neighborhood boundary. This area is characterized by a mix of residential, commercial, and industrial uses. N. 2nd Street and N. 3rd Street are acknowledged in *Horizon 2020* as a primary gateway to the community.

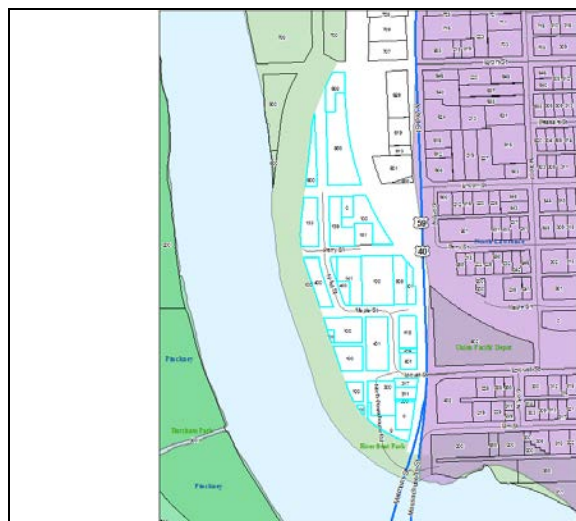


Figure 1: Portion of North Lawrence Neighborhood (purple)

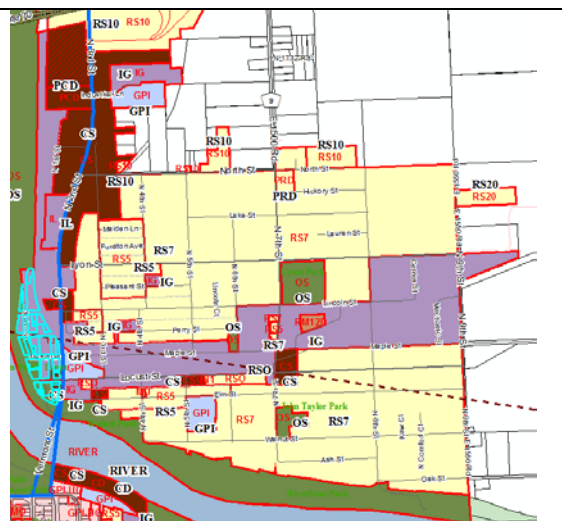


Figure 2: Neighborhood Zoning

This neighborhood includes a wide range of uses. Non-residential uses are located along N. 2nd and N. 3rd Streets and along the railroad corridor parallel to Locust Street. Residential uses are located interior to the neighborhood east of N. 2nd and N. 3rd Streets. The subject property is located

within the portion of the neighborhood that is commercial and industrial in nature. The subject property includes two small mobile home parks and a few detached residential units.

The bulk of the North Lawrence neighborhood is zoned for low-density residential zoning. The neighborhood includes two small parks and an elementary school. The neighborhood does not include any areas designated for multi-dwelling residential uses or mixed use development.

The neighborhood is bounded by the Kansas River and levee. This area provides recreation uses to the community and includes the Riverfront Park area and multi-use path along the levee. Riverfront Park includes a total of 994 acres along the Kansas River. The park began as a restoration and re-vegetation project according to City website information. Much of the park is forested up to the riverbank. The portion of the park nearest the Vermont Street/Massachusetts Street Bridge narrows with little tree growth within the park area between the riverbank and the top of the levee. This area provides direct viewing of the river from the recreation path.

Improvements to the Bowersock Dam included a canoe portage for additional access to the river. This portage is located at the south end of the proposed redevelopment area.

Single story buildings on smaller lots characterize the western portion of the North Lawrence neighborhood. The proposed change would facilitate an increased building height from 45 feet in the CS district and 75 feet in the IG district to 90 feet in the CD district. Physical elements addressing area height, bulk, and massing are being considered by the Historic Resources Commission given the proximity of the project to the Union Pacific Depot environs and as part of the preliminary and final development plans.

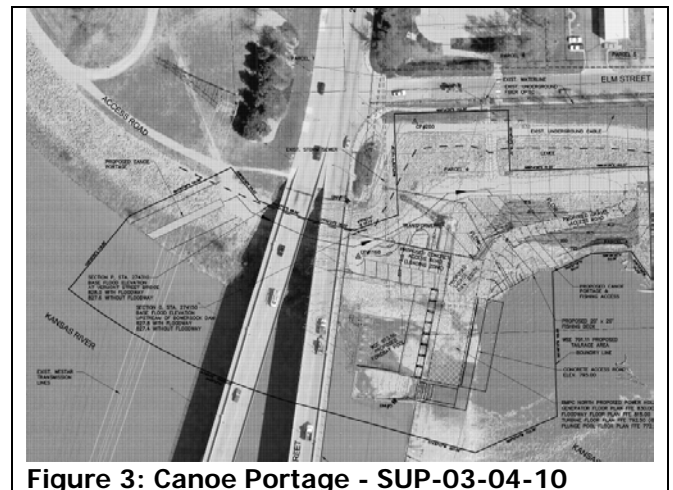


Figure 3: Canoe Portage - SUP-03-04-10

Staff Finding – The proposed request consolidates the base-zoning district and provides an extension of the downtown north of the river. The bulk of the existing neighborhood is zoned residentially with intensive commercial and industrial zoning located along the railroad right-of-way and along the N. 2nd/ N. 3rd Street corridor. The proposed change retains the ability to redevelop the property with mixed uses and expand multi-dwelling residential housing choices within the neighborhood.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

A neighborhood plan was completed in 1981 for this area. The plan has not been updated and has not been incorporated into *Horizon 2020*. In 2005, the City of Lawrence completed the North Lawrence Drainage Study. The subject property is located within the boundary of this study. The plan included a "Build-Out Scenario Map." The map was based on adopted land use policies for the projection of land uses and stormwater calculations.

This study included land use assumptions that included the subject property. Land uses within the boundary of the request were identified or projected as commercial and open space uses. The arrangement of these uses is non-specific within the Drainage Study. The study was not intended to convey a specific zoning designation.



Figure 4: Land Use Map - 2005 Drainage Study

The North Lawrence Drainage Study was divided into two main focus areas. The Internal System consists of the City operated ditches, pipes, and pumps within the existing City boundaries. The overall watershed analysis modeled the less developed drainage aspects of the North Lawrence Drainage Area.

Section I of the Executive Summary

A copy of the plan is available on line at: https://assets.lawrenceks.org/assets/public-works/N_Law_Drainage_Study_2005/N_Law_Drainage_Study_2005.htm

The 2011 staff report included only a concept plan. Since the original application, more details are available regarding the specific development intent. Amendments to the *Horizon 2020* specifically address this area as an extension of Downtown Lawrence. As stated previously, the proposed request is consistent with the land use recommendations included in the associated comprehensive plan amendment. The City is engaged in the development of an updated masterplan for the downtown area. The plan will include comments and narrative of the property included in this application.

Staff Finding – The review of this application assumes the approval of the amendments to the comprehensive plan to extend Downtown to this area. If approved, the request is consistent with planned redevelopment of this area.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response: *The proposed uses are not going to change with the rezoning of the property. The only change will be the process in which the property will be developed.*

The property is currently zoned IG, CS, and OS. The OS district represents the existing Riverfront Park property and other parcels owned by the City and Drainage District. A portion of the area zoned OS is included in the proposed development. The applicant has been working with the City to acquire excess land in this area to facilitate the development. The City agreed to the sale with conditions as discussed at the City Commission meeting on October 8, 2008. The approval will expire in spring of 2019 unless extended or amended.

Several residential uses were located within the IG portion of the property. Residential uses are not permitted uses in the IG Districts other than a mobile home park. The existing mobile home park use is being discontinued. Residents previously living in the area have relocated and any remaining residents will be relocated as the development progresses. The existing CS district includes *Eating and Drinking Establishments* and *Retail Sales uses*. These uses are permitted by right in the CS District and would be permitted in the CD District.

The Gas Light Tavern is, by Code, a *Bar or Lounge* use. This use is not permitted in the CD district. Approval of the request will make this use non-conforming. Non-conforming uses are permitted to remain with only limited changes permitted per Article 15 of the Land Development Code.

Assuming the development is approved and land transactions are completed the OS district would not be suitable for commercial uses. The CS district represents approximately 1.05 acres. A significant portion of this area is dedicated right-of-way. The parcels are generally small and difficult to redevelop in the current configuration. These parcels also represent the existing commercial uses within the boundary of the request. The remaining area is zoned IG. This is an intensive industrial district developed with residential uses in the form of mobile home parks, parking lots, and storage uses. The area is generally isolated by the levee and park property on the west and the railroad on the east. Streets in this area are substandard to the City's current design specifications and are not adequate to support intensive industrial activity.

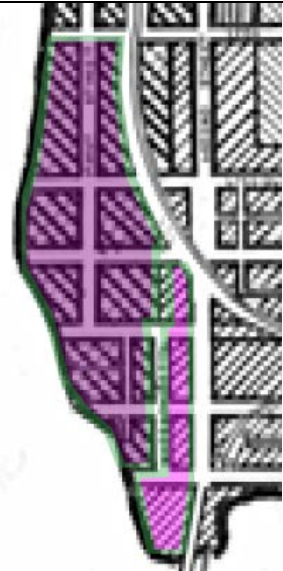
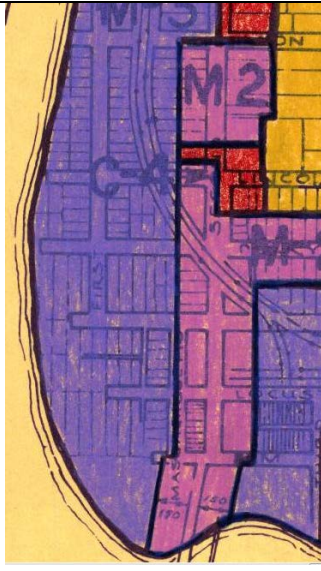


If approved, a mobile home park would not be a permitted use; however, multi-dwelling residential uses would be allowed. This change provides more residential options in the neighborhood than currently exist.

Staff Finding – The current OS and IG zoning districts do not accurately reflect the existing or proposed uses of this area. Staff concurs with the applicant's statements that the industrial zoning is not suitable for this area. Commercial and open space zoning is appropriate for this area. The rearrangement of the parcels and zoning district would facilitate redevelopment.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response: *The property is not vacant.*

The subject area includes both developed parcels and vacant land. The current zoning has been in place since the adoption of the Land Development Code in 2006. Zoning prior to that included M-2 (General Industrial) District, M-3 (Intensive Industrial) District, and C-4 (General Commercial) zoning. The following table shows the progressive zoning changes in the area.

1966	1977	1997	2005
			
Heavy Industrial Light Industrial	Intensive Industrial General Industrial	Intensive Industrial General Industrial General Commercial	Intensive Industrial General Industrial General Commercial

Staff Finding – The subject property includes both developed and vacant land. The zoning districts have remained generally consistent since 1966 with the infiltration of a small area for commercial uses at Locust Street and N. 2nd Street. This property is unique in its location and physical proximity to both the river and railroad. The area has a historically been underutilized. The small lot size and lack of public street improvements are contributing factors to this condition. The prevailing underutilization of the property, as zoned, implies the zoning may not be appropriate for the area. The proposed change would facilitate redevelopment of this area.

7. EXTENT TO WHICH APPROVING THE REZONING WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicant's Response: *We feel that rezoning the area to a Planned Development will give the neighboring properties more involvement in the project and the process. WE see this as a benefit to the surrounding property owners.*

This is a constrained development area given the location of the river and railroad tracks. Immediate properties would not be physically affected by the redevelopment of the site. Wider impacts related to adequate public facilities are assessed through the traffic study, assessment of the sanitary sewer and water capacity, and other basic municipal services. These elements are discussed in detail with the subdivision plat and the development plan staff reports.

As part of this rezoning request a retail market study was required. Staff reviewed the retail study and a discussion of the study is included in the staff report for CPA-11-8-11. A significant consideration of this analysis is the proposed development is intended as a regional attraction by virtue of the river relationship and pedestrian access to the downtown core. The proposed redevelopment is intended to complement the historic downtown area. Recent trends in commercial development are less speculative than in the past with one or more known tenants being identified prior to development. The development is expected to have a "regional draw that results in a more favorable" pull of non-local shoppers to the area, thus benefiting the overall community.

Establishment of the base zoning district is key to providing a framework for redevelopment of the site. Several parcels of land included in the redevelopment boundary are currently owned by the City. This project was discussed by the City Commission in October 2008 (*refer to on-line minutes for October 7, 2008*). The City Commission indicated support for selling the land for a redevelopment project, but retained the levee, levee rights-of-way, and necessary easements to access the levee, stormwater drainage, public utilities, and infrastructure. As development, applications are submitted these elements will be rigorously reviewed to assure continued protection of public interests. Applicable development agreements between the City and the developer are required.

Appropriate phasing, building size, height, and massing of development construction addressing infrastructure, traffic access and circulation, and occupancy will protect nearby properties and the community at large. The execution of design guidelines that address physical aspects of the development are coordinated through the Historic Design Review and through the development plan process as a measure to ensure compatibility with existing development in the area and the community.

Staff Finding – As a planned development, the proposed zoning will not detrimentally affect surrounding properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicant's Response: *We feel that a planned development offers a more transparent process for the public and the city as a whole. We feel that this is beneficial to the public. Their insight is appreciated and we look forward to improving the area for more public use.*

Evaluation of this criterion includes weighing the benefits to the public versus the benefit of the owners of the subject property. Benefits are measured based on anticipated impacts of the rezoning request on the public health, safety, and welfare.

At the heart of this request is a plan to reinvest in an area of the community that is underutilized. Redevelopment would provide an economic opportunity in the North Lawrence area that does not exist today and to protect and enhance existing historic community resources. This redevelopment as proposed provides a large-scale opportunity to utilize the Kansas River as an attraction-destination, as well as a linkage to downtown. These actions benefit the community with some risk as discussed in the retail market study. The current industrial zoning is not suitable for the existing uses and the proposed future redevelopment. This area, while part of the industrial inventory, lacks the necessary lot consolidation and highway access to be desirable for an employment-related use. With regard to industrial areas within the Union Pacific Railroad Corridor, *Horizon 2020* states:

"...Efforts to discourage non-residential traffic in other parts of the neighborhood are highly encouraged. It is also recommended that consolidation of industrial sites occur whenever possible to remove those residential and incompatible commercial uses located within predominantly industrial development land use patterns in a concentrated effort to minimize those impacts and conflicts between incompatible land uses. When the industrial usage of a particular property cases and is no longer practical, it is recommended those properties be converted to residential and/or neighborhood commercial uses." (pg 7-4)

This portion of the corridor is not developed with industrial uses. Denial of the request would hinder the redevelopment efforts for this area by limiting the mix of uses and density associated with the current zoning districts.

Staff Finding – The proposed request for the CD district allows for a mix of commercial and residential uses along with area, bulk, and height regulations accommodating a variety of development options. Denial of the request would not guarantee a gain to the public health, safety, and welfare but would hinder the planned redevelopment for this area. Approval of the request facilitates the redevelopment and allows for reinvestment in the area. Approval of the request with the requirement to develop appropriate design guidelines through the development plan process will protect the public interest and the community.

9. PROFESSIONAL STAFF RECOMMENDATION

When approached by the applicant and their desire to plan for a mixed-use development, staff and applicant discussed various zoning district options to accommodate the development proposal. Comparisons of the MU and CD Districts were discussed. The applicant determined the CD district provided the most flexibility to develop residential units since the CD district is not limited by code. The 2011 zoning applications for CD zoning were approved. This request is modified from the original by including the PD overlay. The PD overlay is proposed in response to unsuccessful attempts to develop applicable design guidelines for the project that was conceptual at that time.

The CD district does not include a maximum density, permits a maximum building height of 90 feet, and does not require off-street parking. The existing CD District is also subject to a specific set of design guidelines applicable only to the area described in those guidelines. They would not extend to this expanded CD District. The density and dimensional standards also allow development in the CD district with zero lot line setbacks. Appropriate transitions and incorporation of applicable easements and levee setback standards are implicit in development of the area.

For this project, a percentage of the development should require residential development as a mixed-use project.

- The current industrial zoning does not allow residential uses, which are integral to a mixed-use development.
- The CD district does not allow detached, attached, or duplex dwellings.
- The CD district allows Multi-Dwelling Structures, Non-Ground Floor Dwelling, and Work/Live Units.

Non-Residential land uses allowed in the CD district are similar to those allowed in the CS with some notable exceptions.

- *Bars and Lounges* in the CD district are required to derive from the sales of food for the consumption on the premises not less than 55% of all the gross receipts for a calendar year from sales of food and beverages on such premises.
- *Gas and fuel sales* is not allowed in the CD district, but is allowed in the CS district. Most of the *Vehicle Sales and Services* uses are not permitted in the CD district.
- *Fast Order Food Drive-in* uses are not permitted in the CD district.
- *Retails Sales, Large* is a use defined as "An establish engaged in retail sales, where the aggregate of retail uses within a building is 100,000 or more gross square feet of floor a that may or may not include ancillary uses within internal access from the principal building."

A large format “big box” retail use would not be permitted; however, retail buildings less than 100,000 square feet could be considered. These generally auto-centric uses are not appropriate in a CD district.

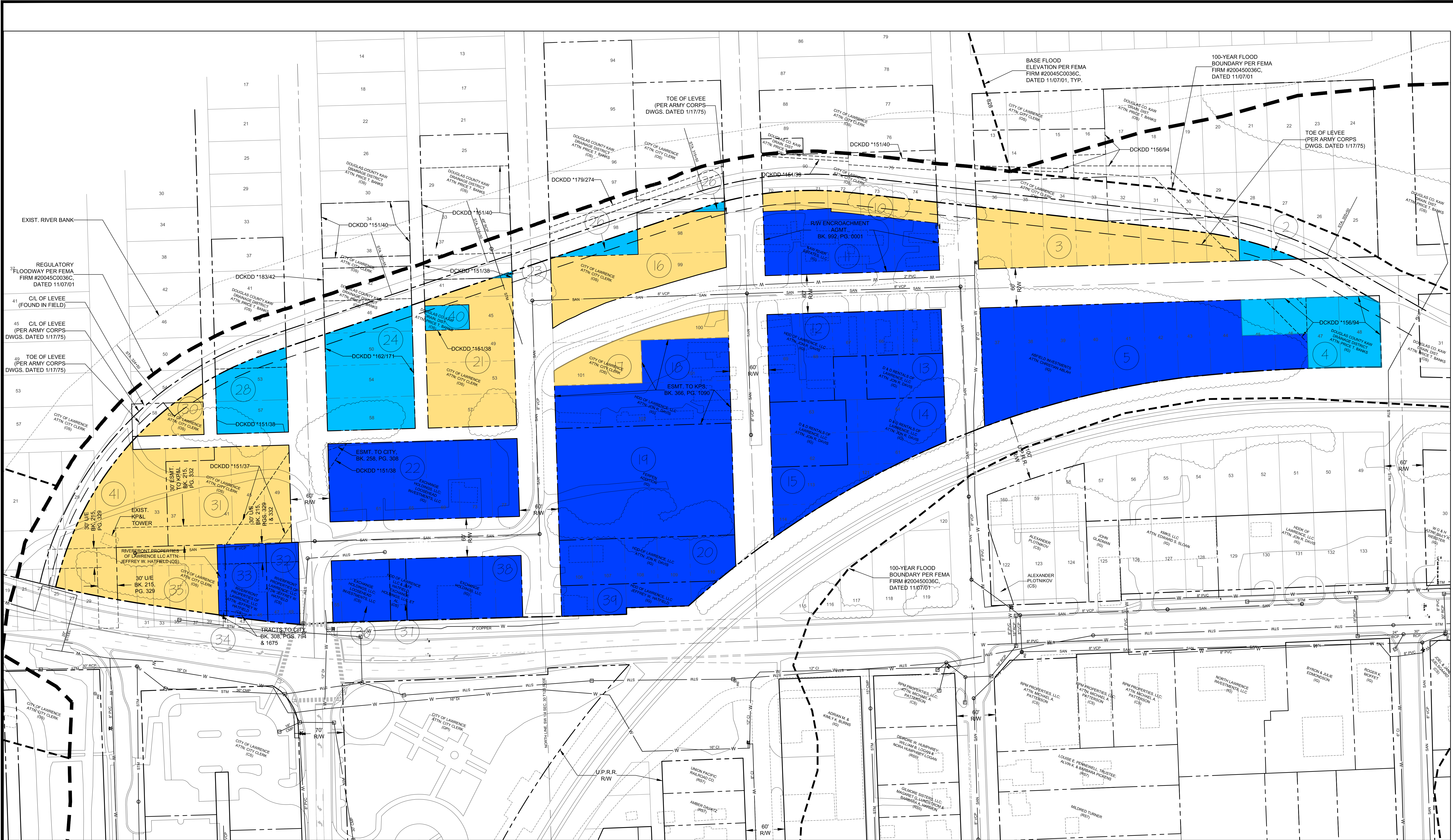
Section 20-901 (f) of the Land Development Code addresses off-street parking and specifically exempts uses in the CD district from the requirement to provide off-street parking. The exemption exists to maintain the urban form of downtown and because the city provides parking in this district. Development at the scale and intensity planned for the subject site will not function without appropriate off-street parking. The site is too far from the existing defined downtown to be served by city-provided parking, in staff’s opinion. For these reasons, staff recommends the subject area provide the appropriate parking; however, through the review of the design guidelines, the parking ratios of certain uses and the ability to use on-street parking could be considered. Parking is discussed in detail as part of the development plan staff report.

Special attention to landscaping, transition areas along the periphery of the redevelopment area, access, and treatment of the levee and railroad right-of-way are necessary to assure compatibility with the surrounding area and retention of the integrity of the neighborhood and the existing historic downtown core.

CONCLUSION

This request represents an opportunity to reinvest in an area of the community currently underutilized. With appropriate design guidelines and a balance of restrictions, this area can be complementary to the existing downtown and the surrounding neighborhood. Redevelopment of the area is subordinate to the overarching value of the levee and necessary limitations to assure the levee is protected. The preliminary development plan will establish the maximum building size and address design guidelines specific to the project to ensure expectations for development, adequate public infrastructure, and parking are provided in a manner complementary to the community.

As a planned development, conditions are not required for this project.

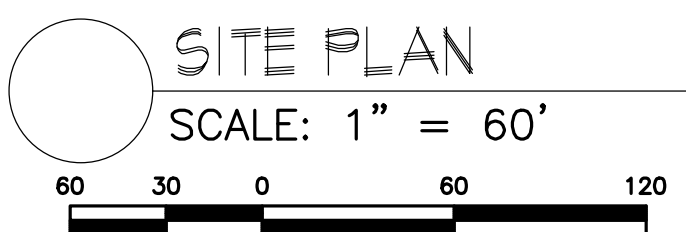


PROPERTY OWNERSHIP:

SYMBOL	OWNER NAME	ADDRESS	PROPERTY AREA	LEGAL DESCRIPTION	SYMBOL	OWNER NAME	ADDRESS	PROPERTY AREA	LEGAL DESCRIPTION
②	DOUGLAS COUNTY KAW DRAIN DIST.	600 N. 1ST ST, BLOCK 2	16,802.20 S.F.	NORTH LAWRENCE ADDITION NO. 2 AND INCLUDES THE E PORTION OF LOTS 21 AND 28 WHICH LIE E OF THE CENTERLINE OF THE LEVEE.	②	DOUGLAS COUNTY KAW DRAIN DIST.	100 MAPLE ST, BLOCK 3	35,80 S.F.	NORTH LAWRENCE MAPLE STREET LOTS 21, 33, 31, 4 THAT PORTION OF THE N 22' LOT 41 LYING EAST OF THE CL OF THE LEVEE.
③	CITY OF LAWRENCE	600 N. 1ST ST, BLOCK 4	26,823.51 S.F.	PART AND ALL LOTS 33 THRU 36 E OF CL OF LEVEE.	②	DOUGLAS COUNTY KAW DRAIN DIST.	100 LOUIST ST.	22,853.49 S.F.	NORTH LAWRENCE LOUIST STREET LOTS 50, 54, 4 50 AND THE PORTION OF LOT 46 LYING E OF CL OF LEVEE.
④	DOUGLAS COUNTY KAW DRAIN DIST.	600 N. 1ST ST, BLOCK 5	17,664.24 S.F.	NORTH LAWRENCE ADDITION NO. 2 H PART LOTS 45 & 46, ALL LOTS 47 & 48 E OF THE CL OF THE LEVEE.	②	DOUGLAS COUNTY KAW DRAIN DIST.	100 MAPLE ST, BLOCK 3	2,878.48 S.F.	NORTH LAWRENCE MAPLE STREET LOTS 51, 51 AND THE PORTION OF LOT 44 LYING E OF CL OF LEVEE.
⑤	CITY OF LAWRENCE	600 N. 1ST ST, BLOCK 3	40,271 S.F.	NORTH LAWRENCE ADDITION NO. 2 LOTS 31 THRU 46 LESS A PART OF LOTS 45 & 46.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑥	KAW RIVER ESTATES LLC	183 PERRY ST.	8,290.10 S.F.	NORTH LAWRENCE ADDITION NO. 2 BEG 10' W OF THE COR LOT 14, SLY 165.7', N 1/4 1/2' SLY 107', M 24' TO THE CL OF THE LEVEE, NORTH ALONG THE CL OF LEVEE TO POB.	②	DOUGLAS COUNTY KAW DRAIN DIST.	100 LOUIST ST, BLOCK 3	12,925.35 S.F.	NORTH LAWRENCE BRIDGE STREET LOT 51 & 51 AND THE PORTION OF LOT 44 LYING E OF CL OF LEVEE.
⑦	HDD OF LAWRENCE LLC	184 PERRY ST.	24,444.36 S.F.	NORTH LAWRENCE ADDITION NO. 2 BEG 5' COR LOT 14, M 70', SLY 165.7', N 1/4 1/2' SLY 107', M 24' TO THE CL OF THE LEVEE, NORTH ALONG THE CL OF LEVEE TO POB.	③	CITY OF LAWRENCE	100 ELM ST, BLOCK 1	4,418.48 S.F.	NORTH LAWRENCE ELM STREET BLOCK 1 LOT 54 & 50 LYING E OF CL OF LEVEE.
⑧	D&D RENTALS OF LAWRENCE LLC	0 LINCOLN ST.	17,465.14 S.F.	NORTH LAWRENCE ADDITION NO. 2 LOTS 65 & 66 LESS M 60' THEREOF.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑨	D&D RENTALS OF LAWRENCE LLC	100 LINCOLN ST.	21,302.35 S.F.	NORTH LAWRENCE ADDITION NO. 2 LOTS 61 & 64, LOTS 62 & 63 LESS 5 1/2' THEREOF, LOT 61 LESS M 60' THEREOF & LOTS 103 & 104 LESS RR.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑩	D&D RENTALS OF LAWRENCE LLC	181 PERRY ST.	11,100 S.F.	NORTH LAWRENCE ADDITION NO. 2 5 1/2' LOTS 62 & 63.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑪	CITY OF LAWRENCE	400 N. 1ST ST, BLOCK 3	23,802.21 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 48 LYING E OF THE CL OF LEVEE, LESS STRIP ALONG NLY SIDE & N 1/2 LOT 48.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑫	HDD OF LAWRENCE LLC	400 N. 1ST ST, BLOCK 1	11,497.26 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑬	HDD OF LAWRENCE LLC	400 N. 1ST ST, BLOCK 1	26,784.39 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑭	D&D RENTALS OF LAWRENCE LLC	100 PERRY ST.	46,465.62 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑮	HDD OF LAWRENCE LLC	505 NORTH 2ND ST.	18,571.44 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑯	CITY OF LAWRENCE	100 MAPLE ST, BLOCK 4	26,826.84 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.
⑰	EXCHANGE HOLDINGS LLC	401 NORTH 2ND STREET	34,855.46 S.F.	NORTH LAWRENCE ADDITION NO. 2 N 1/2 LOT 101 & 102.	③	CITY OF LAWRENCE	300 MASSACHUSETTS ST.	21,000 S.F.	NORTH LAWRENCE MASSACHUSETTS STREET BLOCK 1 LOTS 45 & 44, N 1/2 LOT 41, E 41' OF 5 1/2' LOT 41 & E 41' LOT 31, M 1/2 ADJ VAC N MASS ST, LESS TRACT 100 D&D CO DR, NORTH LAWRENCE BRIDGE STREET LOTS 45, 45 & N 1/2 LOT 42 & E 1/2 VACANT N MASS STREET ROK.

LEGEND - EXISTING ZONING

- DOUGLAS COUNTY KAW DRAIN DIST.
- CITY OF LAWRENCE
- ABFIELD INVESTMENTS
- D&D RENTALS
- EXCHANGE HOLDINGS, LLC
- JEFFREY HATFIELD
- HDD OF LAWRENCE, LLC
- RIVERFRONT PROPERTIES OF LAWRENCE, LLC
- KAW RIVER ESTATES, LLC



NORTH MASS REZONING
EXISTING OWNERSHIP

LAURENCE, KANSAS

123 W. 5TH STREET
SUITE 202
LAURENCE, KS 66044
OFFICE: 785.882.0804
FAX: 785.882.0800

Civil Engineering
Landscape Architecture
Community Planning
Surveying



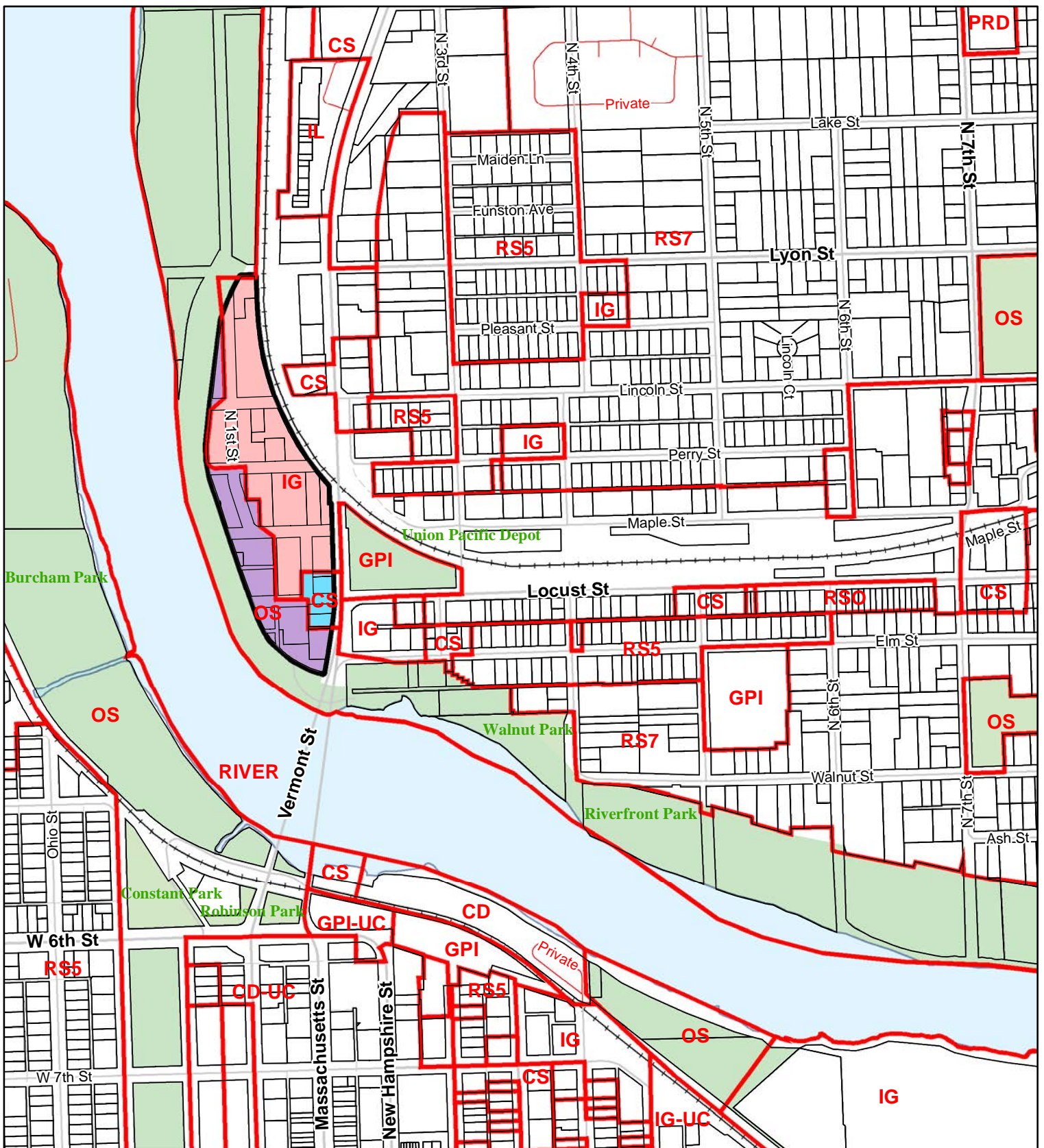
Landplan Engineering, P.A.
Lawrence, KS • Kansas City, MO • Manhattan, KS
Blue Springs, MO • The Woodlands, TX

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PROJECT # 26445
DECEMBER 14, 2011

RELEASE:
10

DATE:
2.10.12



Z-18-00505: Rezone approximately 16.116 acres from OS (Open Space) District, CS (Commercial Strip) District, & IG (General Industrial) District to CD-PD (Downtown Commercial with Planned Development Overlay) District located at 311, 317, 401, 409, 415, 501, & 505 N. 2nd STREET.



PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

PC Staff Report
1/25/12

ITEM NO. 11 COMPREHENSIVE PLAN AMENDMENT TO H2020 - CHP 6; NORTH MASS DEVELOPMENT (AAM)

CPA-11-8-11: Consider a Comprehensive Plan Amendment to Chapter 6 of *Horizon 2020* to expand the identified boundaries of Downtown Lawrence to accommodate a proposed mixed use project known as the North Mass Development. The request includes a proposal to exempt the proposed North Mass Development from the current requirement that individual stores in the Downtown Commercial Center have a maximum footprint of no more than 25,000 square feet. Proposed by Paul Werner Architects.

STAFF RECOMMENDATION: Staff recommends approval of this comprehensive plan amendment to *Horizon 2020*, to expand the identified boundaries of Downtown Lawrence to accommodate a proposed mixed use project known as the North Mass Development and recommends forwarding this comprehensive plan amendment to the Lawrence City Commission with a recommendation of approval.

STAFF RECOMMENDATION: If appropriate, approve and sign Planning Commission Resolution PCR-1-1-12.

KEY POINTS

1. This is a request to include the North Mass Development area in the Downtown Commercial Center designation in Chapter 6 of *Horizon 2020* in order to accommodate a new mixed-use development north of the Kansas River.
2. Applicant is requesting that the building footprint limitation of 25,000 square feet be lifted for the North Mass Development portion of the Downtown Lawrence district. Staff is recommending that there be a limitation on the building footprint in the North Mass Development area of 50,000 square feet.
3. *Horizon 2020* anticipates opportunities to expand Downtown but only following a comprehensive re-evaluation of downtown needs, assets, growth potentials, use mix, and preferred locations for conservation and development.

PROJECT SUMMARY

This is an applicant requested amendment to *Horizon 2020* to include the North Mass Development in the Downtown Commercial Center designation noted in Chapter 6 – Commercial Land Use. Adding the North Mass Development area to the Downtown Commercial Center will ultimately allow the applicant receive CD (Commercial Downtown) zoning and develop the area in accordance with that zoning district's regulations and policies. The applicant submitted the following amended section (from pages 6-3 and 6-4) of *Horizon 2020* for consideration:

New language is in *italics and underlined* and language to be removed is ~~struckthrough~~.

■ **Downtown Commercial Center**

The Downtown Commercial Center is the historic core of governmental, commercial, institutional, social and cultural activity. Transitions to adjacent neighborhoods are traditionally provided through alleyways or landscaping improvements rather than a change in use or density. The Downtown Commercial Center is restricted to the historic commercial core of Lawrence *and the North Mass Development*. The boundaries of Downtown Lawrence correspond with the boundaries outlined in the "Comprehensive Downtown Plan", and are described as: starting at the Kansas River, south along Kentucky Street to just south of Vermont Towers, then east to Vermont Street, south along Vermont Street to North Park Street, east along North Park Street to Rhode Island Street, north along Rhode Island Street to 11th Street, west along 11th Street to the alley east of New Hampshire Street, north along the New Hampshire Street alley to 9th Street, east on 9th Street to Rhode Island Street, then north on Rhode Island Street to the Kansas River. *In addition, from the intersection of the levy and North 2nd Street, then north along the levy to the centerline of Lyon Street, then east along the centerline of Lyon Street to the west right-of-way of the Union Pacific Railroad, then southeast along the west right-of-way of the Union Pacific Railroad to the centerline of North 2nd, then south along the centerline of North 2nd Street to the intersection of the levy and North 2nd Street.*

The Downtown Commercial Center is the Regional Retail/Commercial/Office/Cultural Center for the community and is considered a destination driver that attracts and serves the area beyond that of the local community. The Downtown Commercial Center has an established development and architectural/urban design pattern. Unique among commercial centers in Lawrence, the Downtown Commercial Center combines a variety of land uses, including governmental, retail, office, public facilities, institutions, churches, and residential. Linear in design, the Downtown Commercial Center is focused along Massachusetts Street with New Hampshire, ~~and~~ Vermont Streets *and the North Mass Development* serving as secondary activity areas. General building patterns are urban. Mixed-use, multi-story buildings are the most common building form and parking is provided on-street and through community parking lots and parking structures.

Building designs and public improvements are focused on providing a pedestrian-oriented commercial experience. Massachusetts Street has a distinct streetscape with sawtooth parking and a focus on first floor (pedestrian oriented) retail use. Vermont, ~~and~~ New Hampshire *and North 2nd* Streets provide the major vehicular movement patterns and provide access to the majority of the community parking areas. Alleyways, which provide service access, are one of the main character-defining elements that distinguish the Downtown Commercial Center from other commercial centers. To ensure there are a variety of commercial uses, the maximum footprint for an individual store is limited to approximately 25,000 gross square feet *unless the store is located in the North Mass Development and then the store size will be determined by its use (i.e. grocery store, movie theatre, etc.).* One of the keys to the success of the Downtown Commercial Center is the ability to provide a wide range of leasable square footage that is both flexible and capable of being tailored to a specific use. Construction within the Downtown Commercial Center is regulated by a set of design guidelines administered through an Urban Conservation Overlay Zoning District.

An important ingredient to ensuring the continued viability of Downtown is keeping it the center of the city's social and institutional activities. To maintain downtown as the city and County's hub of governmental functions; uses and buildings such as City Hall, the County Courthouse, Municipal Library, Douglas County Senior Center, Fire/Medical Department's Main Office, Police and Sheriff Offices, the Municipal Pool and the Municipal and District Courts shall remain located in Downtown.

STAFF REVIEW

This Comprehensive Plan Amendment request involves proposed changes to Chapter 6: Commercial Land Use of *Horizon 2020*, specifically the section that relates to Downtown Lawrence. The applicant is proposing to amend the language above to include the North Mass Development area as part of the Downtown Lawrence commercial area, including lifting the restriction on building footprints being no larger than 25,000 square feet for the North Mass Development portion of the Downtown Lawrence commercial area. The applicant is ultimately proposing to zone the North Mass Development area to the CD (Commercial Downtown) district and develop the area with a variety of uses.

The Downtown Lawrence commercial area is important to the vitality of Lawrence since it acts as the main cultural, civic, entertainment, specialty retail and office center for the city. The Downtown Lawrence District is historically significant to the culture of Lawrence and that contributes to the vitality of the area. It is important that the addition of the North Mass Development area not detract or be detrimental to either the historical significance or vitality of the existing Downtown Lawrence district that is south of the Kansas River.

Staff is supportive of the applicant's intent with their proposed language; however, staff identified that other elements of *Horizon 2020* should be revised to accommodate the proposal and suggests that the following changes be made to *Horizon 2020* in order to complete this request:

New language is in *italics and underlined* and language to be removed is ~~struck through~~.

The following section is staff suggested changes that would replace the applicant suggested changes. (From pages 6-3 and 6-4)

■ Downtown Commercial Center

The Downtown Commercial Center is the historic core of governmental, commercial, institutional, social and cultural activity. Transitions to adjacent neighborhoods are traditionally provided through alleyways or landscaping improvements rather than a change in use or density. The Downtown Commercial Center is restricted to the historic commercial core of Lawrence (Downtown Lawrence) and the area immediately north of the Kansas River (North Mass Development). The boundaries of Downtown Lawrence correspond with the boundaries outlined in the "Comprehensive Downtown Plan", and are described as: starting at the Kansas River, south along Kentucky Street to just south of Vermont Towers, then east to Vermont Street, south along Vermont Street to North Park Street, east along North Park Street to Rhode Island Street, north along Rhode Island Street to 11th Street, west along 11th Street to the alley east of New Hampshire Street, north along the New Hampshire Street alley to 9th Street, east on 9th Street to Rhode Island Street, then north on Rhode Island Street to the Kansas River. The boundaries of the North Mass Development are described as: starting from the intersection of the levy and North 2nd Street, then north along the levy to the centerline of Lyon

Street, then east along the centerline of Lyon Street to the west right-of-way of the Union Pacific Railroad, then southeast along the west right-of-way of the Union Pacific Railroad to the centerline of North 2nd, then south along the centerline of North 2nd Street to the intersection of the levy and North 2nd Street.

The Downtown Commercial Center is the Regional Retail/Commercial/Office/Cultural Center for the community and is considered a destination driver that attracts and serves the area beyond that of the local community. The Downtown Commercial Center has an established development and architectural/urban design pattern. Unique among commercial centers in Lawrence, the Downtown Commercial Center combines a variety of land uses, including governmental, retail, office, public facilities, institutions, churches, and residential. Linear in design, the Downtown Commercial Center is focused along Massachusetts Street with New Hampshire, ~~and~~ Vermont Streets and the North Mass Development serving as secondary activity areas. General building patterns are urban. Mixed-use, multi-story buildings are the most common building form and parking is provided on-street and through community parking lots and parking structures.

Building designs and public improvements are focused on providing a pedestrian-oriented commercial experience. Massachusetts Street has a distinct streetscape with sawtooth parking and a focus on first floor (pedestrian oriented) retail use. Vermont, ~~and~~ New Hampshire and North 2nd Streets provide the major vehicular movement patterns and provide access to the majority of the community parking areas. Alleyways, which provide service access, are one of the main character-defining elements that distinguish the Downtown Commercial Center from other commercial centers. To ensure there are a variety of commercial uses, the maximum footprint for an individual store is limited to approximately 25,000 gross square feet in Downtown Lawrence and 50,000 gross square feet in the North Mass Development area. One of the keys to the success of the Downtown Commercial Center is the ability to provide a wide range of leasable square footage that is both flexible and capable of being tailored to a specific use. Construction within the Downtown Commercial Center is regulated by a set of design guidelines administered through an Urban Conservation Overlay Zoning District.

An important ingredient to ensuring the continued viability of Downtown is keeping it the center of the city's social and institutional activities. To maintain downtown as the city and County's hub of governmental functions; uses and buildings such as City Hall, the County Courthouse, Municipal Library, Douglas County Senior Center, Fire/Medical Department's Main Office, Police and Sheriff Offices, the Municipal Pool and the Municipal and District Courts shall remain located in Downtown.

(From pages 6-13 and 6-14)

- **Downtown Lawrence**

Throughout the development of this Plan, the need to preserve, improve and enhance Downtown Lawrence has been shown to have broad community support. Goals and policies in the Plan are written to ensure Downtown Lawrence remains competitive and viable as a Regional Retail Commercial Center. Downtown Lawrence shall remain the Regional Retail/Commercial/Office/Cultural Center because it is: 1) a physical and cultural symbol of the strength of the community; 2) a gathering point for many civic and cultural functions; 3) the "historic core" of the community which establishes a vital continuity between the past and the present community; and 4) the site of major public and private investment.

The Comprehensive Downtown Plan reiterates the specific functions of a downtown. These functions include provisions for a retail core, office space, entertainment services, peripheral residential development, cultural facilities (including performing arts, museums and libraries) community social needs (including club and organizational meeting facilities), government offices and facilities, health services, convention and hotel facilities. The Comprehensive Downtown Plan also states this area should provide, "the economic, physical and aesthetic environment around which the populace can develop an intense pride in the community, a focal point for identification and drawing together for common interests, a meeting place where people can communicate and relax -- the heart of the city".

To distinguish Downtown Lawrence from other commercial and retail areas, and to preserve and enhance its role in the community, Downtown Lawrence is designated as the Regional Retail/Commercial/Office/Cultural Center and shall be the only location within the planning area developed for such use. Gateways to Downtown Lawrence should be emphasized and enhanced to contribute to the "sense of place" of this unique area of the community.

The distinction as the Regional Retail/Commercial/Office/Cultural Center, above and beyond other commercial areas within the community, is significant. Downtown Lawrence serves the greater needs of the community as a focal point for social, community and governmental activities. The Plan's goals and policies encourage the continued development of a broad mix of uses in downtown Lawrence with an emphasis on retail as a major land use. It is vital to the community's well-being that Downtown Lawrence remain the viable Regional Retail Commercial Center.

For Downtown Lawrence to remain economically stable and vital there is a need to expand the boundaries beyond the current configuration illustrated in the adopted Comprehensive Downtown Plan. The addition of the North Mass Development area serves this purpose, and provides for a unique development that will be complementary to the main Downtown Commercial Center located south of the Kansas River. ~~This anticipates the need to provide additional parking areas and locations for commercial and public-related development in the future. At this time,~~ With the exception of the North Mass Development, the Comprehensive Plan does not recommend areas for downtown expansion, but opportunities for expansion and redevelopment do exist within the current boundaries of Downtown Lawrence. Action to expand Downtown Lawrence can only be reasonably undertaken following a comprehensive re-evaluation of downtown needs, assets, growth potentials, use mix, and preferred locations for conservation and development. Re-study of the Comprehensive Downtown Plan should explore the following options to improve Downtown Lawrence: development of a comprehensive parking plan and implementation schedule, evaluation of transportation options, improvement of access to downtown from the east, west and south, and inclusion of more uses along the river and integration of these developments into downtown.

(From Page 6-23)

GOAL 1: Established Commercial Area Development

Encourage the retention, redevelopment and expansion of established commercial areas of the community.

Policy 1.1: Recognize and Emphasize Downtown Lawrence as the Regional Retail/Commercial/Office/Cultural Center

- A. Encourage and support the development of a broad mix of land uses, with an emphasis on retail as a major land use, the provision of parking facilities, improved accessibility, and the expansion of Downtown Lawrence while maintaining the integrity of surrounding neighborhoods.
- B. Strengthen, define and support neighborhood residential areas adjacent to Downtown Lawrence in order to reinforce the safety, image and identity of Downtown Lawrence.
- C. Closely analyze (through Policies 1.7, 3.7 G, 3.9 E, and any other relevant Policies) the impact of requests for development of community and/or regional shopping areas to ensure that such development does not have a negative impact on the Regional Retail/Commercial/Office/Cultural Center. This analysis would be used to evaluate the potential impact on the future viability of the Regional Retail/Commercial/Office/Cultural Center as a whole and not the potential impact on individual businesses or properties.
- D. *The North Mass Development is an extension of the Downtown Commercial Center that shall complement the existing Downtown Lawrence. The following policies shall be implemented for the North Mass Development:*
 - 1. The North Mass Development shall contain a mix of uses including residential, office and commercial.*
 - 2. The North Mass Development shall provide adequate parking spaces to accommodate the mix of uses in the development.*
 - 3. The footprint for an individual store in the North Mass Development shall be limited to 50,000 square feet.*
 - 4. The North Mass Development shall be developed either using the existing Downtown Conservation Overlay District (and its associated design guidelines), or through the PD overlay zoning regulations.*

The Comprehensive Downtown Plan referenced above is an outdated plan that was not adopted into Chapter 14: Specific Plans of *Horizon 2020*. Since this plan is outdated and is not included in *Horizon 2020*, staff is not recommending revising that plan as a part of this development request. However, *Horizon 2020* requires an analysis of the following items:

(Staff response is in *italics*)

- 1. **Downtown needs:** *In order to remain vital as the civic, cultural, retail, and office center for the community, Downtown Lawrence should encourage expansion through appropriate types of development. There is always a need for more residential development to support other types of development (office, retail), and help stabilize the existing downtown by providing an adequate mix of development. The increase in residential development will lead to an increased need for everyday goods and services to be purchased, therefore creating a need for a better mix of retail stores. Also, there is a need for continued emphasis on parking strategies to be considered. Currently, there is a need to take better advantage of the river, including appropriate access. Certain festivals and events will be located in the North Mass Development, especially those closely associated with the Kansas River, however,*

the majority of festivals, parades, and events should remain in the Downtown Lawrence historical core.

2. **Downtown assets:** *In addition to the historical significance and listing on the National Register of Historic Places, the Downtown Lawrence area acts as the regional civic, cultural, office, commercial and retail center for the community. It is a uniquely developed, mixed-use area that acts as a major draw for outside visitors. It is also a main draw for the community, since it houses the major governmental functions for the both the city and the county, as well as many community services, including an arts center, the community pool and the library. In addition there are multiple outdoor areas that serve as parks and which are used for many festivals throughout the year.*
3. **Growth potential:** *There are a few sites within the existing downtown area that are vacant and have the potential for new infill development. Expansion in to neighboring areas is difficult on the south, east and west due to the existing residential neighborhoods that border downtown on those sides. The North Mass Development presents an opportunity for growth without impacting the existing neighborhoods negatively.*
4. **Use mix:** *Staff recently completed an in-depth analysis of the existing use commercial uses in the Downtown Lawrence District (<http://www.lawrenceks.org/planning/documents/DowntownRetailMemo.pdf>). This document shows that in the Downtown district, the largest use category is Non-retail, Non-food services, which, in 2011 makes up 38% of the total square footage in the Downtown district. Typical uses in this category would be professional offices, financial institutions, light manufacturing uses, religious institutions, and any residential uses. The percentage of total square footage in the Food Services use category, including restaurants, coffee shops and bars, has remained steady since 2006, occupying around 15% of the total square footage, even though the amount of square footage has grown by roughly 36,000 square feet since 2006. Vacant square footage was at 11% in 2006, fell to 9% in 2010 and has risen to 13% in 2011. Overall, the percentage of total square footage that strictly Retail uses are occupying has declined since 2006, while the percentage of total square footage that Food Services uses are occupying have remained constant. The amount of vacant space has increased to 13% of the total square footage in the Downtown district since 2006. This survey primarily counted ground floor spaces in the downtown area, except where one use occupied an entire building (i.e. Eldridge Hotel, Riverfront Mall, etc.); therefore, the majority of non-ground floor residential and office uses are not included in this survey.*
5. **Preferred locations for conservation and development:** *As stated above, the current Downtown area contains some vacant sites that are appropriate for infill development, but encroachment into residential neighborhoods on the east, south and west is not feasible. The main historical core of the Downtown district is listed on the National Register of Historic Places and it is important to adequately plan for development or re-development of areas within the district or in the environs for any potential impact to that historic district. As a form of urban redevelopment, the subject area takes advantage of underutilized land and its connection to the river.*

The area north of the Kansas River that is the subject of this proposal is located in North Lawrence, where a mix of residential, commercial and industrial uses exist. Chapter 6 of *Horizon 2020* also contains a section describing the N.2nd and 3rd. Street Commercial area of North Lawrence. That section talks about the area being a gateway to Lawrence through its connection with the I-70 transportation corridor. This project fits more with the Downtown

Lawrence area because of the desire to develop it more with a downtown feel, its connection with the Downtown transportation corridor through the use of the Kansas River Bridge, the proposed mixed use nature of the development, the connection to the Kansas River and the proximity to the Downtown Lawrence district.

Retail Market Study: The applicant has submitted a project specific retail market study as required by Section 20-1107 of the Land Development Code and Chapter 6, Commercial Land Use of *Horizon 2020*, specifically Policy 3.13. That market study includes all of the required information, including analysis based on vacancy rates, income trends, population trends, mix of businesses, etc. The market study indicates that this request is to add roughly 230,000 sf of space split between commercial (retail), office, and hotel uses. The true amount of commercial space for the project is proposed to be roughly 150,000. That 150,000 sf of commercial space is proposed to be broken down as follows: approx. 50,000 sf for a movie theatre, 70,000 sf for retail uses, and 30,000 sf for food and dining uses.

Policy 3.13 in *Horizon 2020* requires a project specific retail market study for projects that would create 150,000 square feet or more of commercial space. Section 20-1107 of the Land Development Code applies to zoning or site plan applications that could create 50,000 square feet of retail space. The applicant has submitted for a zoning application and therefore staff is reviewing the market study based on the Land Development Code, in addition to the criteria in *Horizon 2020*, based on the submission of that application, and because the criteria in the development code is the most recently adopted set of criteria.

Horizon 2020, Policy 3.13 (b) states that, “The project shall not be approved if the market study indicates the commercial project or any proposed phase cannot be absorbed into the community within three years from the date of its estimated completion, or that it would result in a community-wide retail vacancy rate greater than eight percent.” The Development Code uses a vacancy rate threshold of 8% as one factor in order to determine market health, and the most recent citywide market study completed in Fall of 2010 figured the city-wide vacancy rate at 7%, slightly higher than the 2006 vacancy rate of 6.7%. (<http://www.lawrenceks.org/planning/documents/2010Retail.pdf>) The market study for this project shows that, when completed and entirely vacant, the project will push the city-wide vacancy rate to 9.2%. Staff requested additional analysis from the applicant to take into consideration other commercial projects that have received approvals, but have not been constructed to date. The below table illustrates the impact that other major projects that have been approved will have on the overall vacancy rate:

	Total Square Feet	Total Occupied Square Feet	Total Vacant Square Feet	City-wide Vacancy Rate
Total Current Retail Inventory	9,120,567	8,478,372	642,195	7.0%
Requested North Mass Development sf	217,337*	0	217,337*	9.2%
Bauer Farm – yet to be constructed	39,620	0	39,620	
Mercato	359,640	0	359,640	
Fairfield Farms	200,000	0	200,000	
Total	9,937,164	8,478,372	1,458,792	14.7%

*The 217,337 sf number is from the applicant supplied market study. Not all of this sf is proposed to be true commercial (retail) in nature.

If all approved commercial space that has been approved were to be constructed and assumed vacant, the city-wide vacancy rate would rise to 14.7%.

While the market study shows that the project, upon completion, will push the city-wide vacancy rate above 8%, this figure alone is not an adequate representation of the impact of this development. This figure is computed by assuming that the project will either be entirely vacant upon completion, or that it will cause the same amount of space to become vacant in other areas of town. While new commercial development can lead to vacancies in other parts of town, the current economic conditions have all but halted speculative commercial building in Lawrence. The current development trend is that buildings are built with known users or committed tenants and therefore, the applicant has stated that there it is unlikely that the space will be vacant upon completion.

While this particular proposal might not be vacant upon completion, there is concern that this development will cause vacancies in other commercial districts, most importantly the Downtown District. The applicant has provided information on the mix of business types and the potential impact on the downtown area. While the majority of the proposed uses (movie theatre, restaurants, and other retail) exist elsewhere in Lawrence, it is expected to have a limited impact on the city as a whole. Concern does exist because of the potential impact that the proposed commercial uses may have on the existing Downtown District. The applicant addresses this by compiling an analysis of the number, broken down by type, of establishments located in the Downtown District. This analysis shows an increase of approximately 9% of the number of establishments of the same type that are being proposed that are currently located in Downtown. While the impact on the number of establishments might be insignificant when analyzed in such a fashion, the impact on the Downtown District remains a concern. The North Mass Development has some unique factors that diminish the potential for negative impact on the current Downtown District. It is located within walking distance of the historical core, will be designed to expand the river's role as an attraction and will only add approximately 12% more square footage to the existing Downtown District. Those factors, combined with the proposed language to be added to Chapter 6, especially as it relates to complimenting and being secondary to the historical downtown core, should make the North Mass Development a complimentary addition to the Downtown Commercial District.

Other demand factors, such as income, employment and population need to be taken into account as well, when looking at the overall impact of this project on the market as a whole. The applicant supplied market study and the 2010 Retail Market Report show that since 2000, population has grown approximately 16%, while retail sales have only increased 3.3%, and income, adjusted for inflation, has only increased 3.9%. On the supply side of the market, retail stock has increased 69.7% since 2000, however, it is important to note that some of that increase is because of changes in the methodology for figuring total retail space. What is important to take away from the above number is that demand has not kept pace with supply as shown by the limited income, population, and retail sales growth.

The market study also provides an analysis of "pull factors" or a measure of local commerce based on a comparison of local spending to the state as a whole. A pull factor above 1.00 indicates that a community attracts retail sales, while a factor below 1.00 indicates that the community is losing retail sales to outside areas. The Kansas Department of Revenue issues pull factor reports for all of Kansas. The most recent, issued in 2011, states that Lawrence's pull

factor was 1.02 in 2010, which as noted in the market study, is a 9.7% decline over the last decade from a height of 1.13 in 2000. In addition, Douglas County's pull factor has been below 1.00 for the last decade and recently has fallen to .86 in 2010. The declining Lawrence and Douglas County pull factors indicate that the City is losing more and more retail sales to other areas outside of Douglas County. There is potential to recapture this leakage by increasing the demand factors mentioned above, as well as increasing the types of appropriately located retail stock.

The market study also provides a demand analysis based on the amount of square feet of retail space per capita. In 2010 in Lawrence there were approximately 98 sf of retail space per capita. Using an average growth rate from 2000-2010 of 1250 people per year, Lawrence could add as much as 121,000 square feet of retail each year in order to keep the ratio of retail square feet to people at 98. In Section 20-1107 (c)(3)(iv) of the Land Development Code, a maximum threshold of 100 square feet per resident is established to help maintain market health. The 230,000 square feet of commercial space being requested with this project keeps the ratio of 98 square feet per capita after two years. However, this analysis does not take into consideration any of the other approved commercial development. The addition of Mercato, Fairfield Farms and the remainder of the Bauer Farm development that is approved, but not constructed, would result in a ratio of 111 retail square feet per capita.

The market study satisfies the submission requirements of the Land Development Code and *Horizon 2020*. In staff's opinion, proposals to add retail space should be carefully scrutinized with respect to the indicators associated with demand not keeping pace with supply and because vacancy rates are arguably reaching unhealthy levels. The existing downtown Lawrence is designed to not only draw local shoppers, but also has a focus of a destination area for regional or non-local visitors. The North Mass Development is also being designed in a way to focus on drawing regional or non-local visitors to the area, which in turn is presumed to bring more visitors to the area through spill-over, but should not detract from the existing Downtown Lawrence draw. Even though this project will push the vacancy rate above the 8% threshold, and the square feet per capita figure rises above the 100 retail square feet per capita when other approved projects are factored in, it is unlikely that this development will be speculative in nature. The current economic conditions are showing a trend that buildings are built with committed tenants. It is presumed that this development will have a regional draw that results in a more favorable pull factor; therefore, the development should not have a detrimental impact on the community's retail market.

Staff reviewed this amendment based upon the comprehensive plan amendment review criteria listed below [identified in Chapter 13 (Implementation) of *Horizon 2020*].

COMPREHENSIVE PLAN AMENDMENT REVIEW

A. Does the proposed amendment result from changed circumstances or unforeseen conditions not understood or addressed at the time the Plan was adopted?

Applicant's response: H2020 states, "Re-study of the Comprehensive Downtown Plan should explore the following options to improve Downtown Lawrence:.....inclusion of more uses along the river and integration of these developments into downtown (p. 6-14)."

Staff's response: Chapter 6 anticipates changes and additions to the commercial framework in the City of Lawrence over time and provides criteria for which new areas and expansions to existing commercial areas can be made. More specifically, the Downtown Commercial Center

sections of the chapter also anticipate and even encourage the expansion of the center in order to strengthen the vitality and longevity of the Downtown area. Staff does not think that expanding the Downtown Commercial Center north across the river was addressed when the plan was initially written, and that has ultimately led to the request for this amendment.

B. Does the proposed amendment advance a clear public purpose and is it consistent with the long-range goals and policies of the plan?

Applicant's response: The public purpose for this amendment is to allow for development north of the river that will include retail, office and residential uses to compliment downtown. The river is a great amenity for Lawrence but is not being utilized to its full potential. This amendment does recognize H202's commercial land use goals and policies by striving to ensure that the cities commercial areas are encouraged to grow while at the same time supporting and complementing downtown.

Staff's response: The proposed amendment is generally consistent with the goals and policies outlined in Chapter 6. It is important that any expansion to the Downtown Commercial Center recognize that the historical core of Downtown should remain and that new areas shouldn't have a negative impact on the historical core. The applicant has stated that the proposed development is meant to complement and not harm the existing downtown area. Staff has suggested alternate language that further cements this idea in *Horizon 2020*.

C. Is the proposed amendment a result of a clear change in public policy?

Applicant's response: No.

Staff's response: The proposed amendment is not a result of a clear change in public policy, but instead furthers the goals and policies already established in Chapter 6, especially those relating to the expansion of the Downtown Commercial center.

In addition, the following shall be considered for any map amendments:

A. Will the proposed amendment affect the adequacy of existing or planned facilities and services?

Applicant's response: The proposed amendment will support Downtown's existing and planned facilities and services by being in close proximity across the river and by allowing development across the river to draw and attract additional people downtown.

Staff's response: At the comprehensive plan level, it is difficult to anticipate the impact the addition of the North Mass Development area will have on the existing or planned facilities or services. If approved, further review will be needed to determine whether changes will be necessary to the facilities and services in the area. At minimum, the applicant will need to work with the Army Corps of Engineers, utility companies, the public works department in addition to obtaining necessary approval from the city, including completing public improvement plans in accordance with city policies. Addressing stormwater management needs and effects should also be considered as the project moves forward, because there has already been an identified problem with stormwater management in the North Lawrence area.

B. Will the proposed change result in reasonably compatible land use relationships?

Applicant's response: The expansion of downtown will allow the river to play an important role in tying together both the southern and northern ends of Massachusetts Street.

Staff's response: The project will provide for an expanded Downtown Commercial Center that ties in the historic Santa Fe Depot and the Kansas River with the existing Historic Downtown core. The mixed-use nature of the proposal is consistent with the existing Downtown Commercial Center, however, adequate transitioning with the existing North Lawrence neighborhood, which consists of commercial, industrial and residential uses, should be encouraged at the site planning level.

C. Will the proposed change advance the interests of the citizens of Lawrence and Douglas County as a whole, not solely those having immediate interest in the affected area?

Applicant's response: The proposed development will be located on the river and act as a strong link to connect Downtown and the North Mass development to each other and include possible uses such as a movie theater, hotel, offices, restaurants and residential opportunities which will attract users to Downtown and the City and County as a whole. The North Mass Development along with the existing Visitor Information Center will serve as an impressive Gateway to Lawrence.

Staff's response: The expansion of this commercial area will provide new opportunities for shopping and entertainment for the community as a whole. The mixed-use, pedestrian friendly nature of the proposal will add to the Historic Downtown core and tie together the area immediately north of the Kansas River with the existing commercial area south of the river. The opportunity for additional residential uses would be good for the Downtown and would be appropriate, as new infill or redevelopment.

PROFESSIONAL STAFF RECOMMENDATION

Staff recommends forwarding a recommendation of approval of this comprehensive plan amendment to *Horizon 2020*, to the Lawrence City Commission to amend sections of Chapter 6 with staff's suggested language in order to expand the Downtown Commercial Center to incorporate the North Mass Development.

ITEM NO. 4 COMPREHENSIVE PLAN AMENDMENT TO H2020 - CHP 6; NORTH MASS DEVELOPMENT (AAM)

CPA-11-8-11: Consider a Comprehensive Plan Amendment to Chapter 6 of Horizon 2020 to expand the identified boundaries of Downtown Lawrence to accommodate a proposed mixed use project known as the *North Mass Development*. The request includes a proposal to exempt the proposed *North Mass Development* from the current requirement that individual stores in the Downtown Commercial Center have a maximum footprint of no more than 25,000 square feet. Proposed by Paul Werner Architects.

STAFF PRESENTATION

Ms. Amy Miller presented the item.

APPLICANT PRESENTATION

Mr. Paul Werner, Paul Werner Architects, said this was an exciting project and would be a long process. He thanked staff for their work and he agreed with the staff recommendation. He said regarding the vacancy rate it was all about timing. He said if it could get plated and rezoned this year that would be good progress. He said it took 1 ½ years for the Army Corps of Engineers to figure out where the levy was defined. He said the 25,000 sq ft was an opportunity to get it lifted. He felt it was arbitrary. He said a lot of grocery stores were 45,000 sq ft.

PUBLIC HEARING

Mr. Dan Hughes, Sunflower Outdoor and Bike Shop, said he was not against development along the river. He appreciated the efforts of Mr. Rick Renfro and his group to clean up and improve North Lawrence. He expressed concern about retail vacancies. He said in order to increase the pull factor of luring out of town shoppers to Lawrence there needed to be unique destinations that were not available elsewhere and provide a special experience. He said this project, as presented, did not fulfill those requirements. He said the proposal was being pitched as a compliment to downtown but he felt it was mostly a reproduction of the existing downtown. He said it would end up being a downtown A. and downtown B. He said it had the potential to further divide the retail pie to a point where nobody would operate a sustainable business. He said the one glaring addition would be allowing a 25,000+ sq ft 'big box' retailer, which had the potential to crush already established businesses downtown. He said Mr. Renfro told him he did not want a 'big box' retailer and all he wanted was a new Johnny's, some residential, possibly a movie theatre, hotel, and grocery store. He said the developer was asking for carte blanche to build anything they could. He did not feel now was the time to add more retail space. He urged Planning Commission to vote no to the Comprehensive Plan Amendment and rezonings at this time.

Mr. Rob Riley, Lawrence Jiu-Jitsu, supported what Mr. Hughes said. He expressed concern about vacancies downtown. He also expressed concern about the possibility of his business being pushed out.

Mr. Peter Zacharias, Downtown Lawrence Inc. Board of Directors, said he visited with the developers several times. He expressed concern about vacancies. He did not see how the North Mass Development could ever be part of downtown. He felt the North Mass Development would affect downtown negatively. He felt it should just be limited to residential development with moderate commercial but that was not what was proposed. He urged Planning Commission to vote against the project until there was more demand in Lawrence and less vacancies. He said he would like to see the use linked to the zoning before a decision was made.

Mr. Ted Boyle, North Lawrence Improvement Association, said this was an exciting project and felt it would be good for North Lawrence. He said regarding the vacancies mentioned by other speakers the uniqueness of the North Mass Development was the river. He felt it was about time to break the barrier of separating North Lawrence from the rest of the city. He said it would bring money to North Lawrence. He stated the stormwater would dictate what could be built there and define what could be developed. He said the developer would not be able to tie into the city stormwater project and would have to have another big pump, which could cost 5 million dollars. He felt the North Mass Development would benefit the entire community.

Mr. Kirk McClure, Old West Lawrence Association, discussed market conditions, the absence of development controls, and the process. He said Planning Commission did not have sufficient information to make a decision tonight. He stated Lawrence was horribly overbuilt and adding to it would only make the situation worse. He said the City of Lawrence allowed a retail bubble to be built from 1997-2005 and during that time period the stock of retail space grew 34%, adding 1.6 million square feet. During that time the demand for retail only grew 10%. He said the recession slowed the rate of absorption. He felt the North Mass Development, as proposed, would threaten the one unique destination shopping Lawrence has; downtown. He said new 'big boxes' on the other side of the river would not be complementary to downtown and would only compete with it. He said the developer should not be trusted to provide the market studies. He said if the market study was correct then the numbers show the city was more over built than his own numbers. He felt more should be asked of Planning staff since they were the only people who had the demand side data. He recommended the project be denied.

Mr. Chuck Magerl asked Planning Commission to look at the numbers associated with the project. He referenced Horizon 2020 where it states *'The project shall not be approved if the market study indicates the commercial project or any proposed phase cannot be absorbed into the community within three years from the date of its estimated completion, or that it would result in a community-wide retail vacancy rate of greater than eight percent.'*

He said he didn't know how an estimated completion could be established to create a benchmark. He said the Land Development Code established a maximum threshold of 100 sq ft per resident to help maintain market health. He said right now the challenge was that the City was disputing the resident count with the Census Bureau. He said until there was a sense of confidence that the numbers were real, the guidelines of Horizon 2020 and the Land Development Code pose real questions about the merit of increasing additional retail and commercial development within the city. He asked them to make sure the numbers justified the idea of seeing something new and growing in the community.

APPLICANT CLOSING COMMENTS

Mr. Werner said there was no development plan in front of them and that the Comprehensive Plan Amendment was about the boundary of the downtown commercial area. He said the amount of retail and residential needed to be talked about. He said currently the land was zoned industrial. He said he was currently working on four storefronts downtown and was aware of the vacancies downtown. He said he was currently working on a 36 room addition for the Eldridge Hotel and they were not concerned about competition. He said sometimes the private sector doesn't need other people to protect them or they would be present tonight at the meeting. He said residential would probably be the first thing built for the North Mass Development. He said 'big boxes' would not want to locate there.

COMMISSION DISCUSSION

Commissioner Hird asked if staff was satisfied regarding the thoroughness and completeness of the retail market study.

Mr. McCullough said staff summarized on page 106 of the packet that the proposal does go past some of the indicators of supply and demand. He said what had changed since the indicators were established as indicators to review with any development proposal, was that staff was not seeing speculative building in the market right now, they are real projects. The economy has changed that for the most part in Lawrence. He said yes, in theory if everything was vacant, indicators would suggest that this should not be an approved project. He said the private sector was waiting for real tenants before bringing forth projects.

Commissioner Hird inquired about the limitation of 25,000 square feet on a building downtown.

Mr. McCullough said the applicant made some points regarding the dynamics of the pattern, street, ally, block, and lot size, that would be different with this development. He said there would perhaps be a little more room for a bigger project. He said staff did not want a 'big box' to compete with that market.

Commissioner Hird asked how the proposal prevented a 'big box' from competing.

Mr. McCullough said 'big boxes' were typically larger than 50,000 square feet.

Commissioner Hird asked how 'big box' was defined.

Mr. McCullough said an example of a small 'big box' would be Wal-Mart on 6th Street, which was approximately 99,000 square feet.

Commissioner Burger asked staff what the square footage of the former Borders building was.

Ms. Miller said it was roughly 20,000 square feet. She said regarding the 50,000 square feet staff was trying to accommodate the applicants proposed idea with the potential for a movie theatre.

Commissioner Hird said a grocery store was needed in North Lawrence and nobody would object to that but he was not interested in a 'big box' store on the edge of downtown.

Mr. McCullough said it would be a linear progression of approvals, starting with the umbrella policies and then getting down to the site plan and details. He said it was a unique enough area that when the zoning presentation was heard tonight they would see a recommendation for design guidelines to help guide the details of the aesthetics, physical development, ratios of residential to commercial, etc. He said there was a lot of work still to be done on the project.

Ms. Miller clarified that there was no definition of 'big box' in the Land Development Code but it is listed in the table and refers to Retail Establishment - Large, which is a store over 100,000 square feet.

Commissioner Burger referred to page 95 of packet where it states *'For Downtown Lawrence to remain economically stable and vital there is a need to expand the boundaries beyond the current configuration illustrated in the adopted Comprehensive Downtown Plan.'* She wondered if there was any history information to support that statement.

Ms. Miller said that was existing original language in the Comprehensive Plan from its conception in the mid 1990's so she could not provide history.

Commissioner Blaser said he was surprised that Downtown Lawrence Inc. sent a letter of support but then spoke in opposition tonight.

Mr. Zacharias said from the audience that Downtown Lawrence Inc. did not see the final plan until today.

Commissioner Blaser expressed concern regarding the pull factor. He said what Lawrence was doing was not working. He said the auto industry and 'big box' industry figured out that it may be better to have competition. If there is competition then it draws customers. He said Lawrence does not draw customers. He said he was not sure what the answer was. He said North Lawrence on 2nd Street was one of the entrances to the city and he was in favor of cleaning up that area. He thought the proposal was a great idea and would help downtown. He said there would be some logistics to be worked out for getting across the bridge. He said unless he heard strong arguments he would vote in favor of this because he felt something needed to be done.

Commissioner Singleton said she did not think most people were aware of the project since there were only five people present tonight. She felt that changing the boundaries of the heart of the community needed a much more comprehensive study than what they had. She said the proposal was nice but that the best waterfront property in Lawrence was now Abe and Jake's and the Marriott. She felt there would be more conversations if more people were aware of the topic. She was not comfortable changing the borders of downtown. She said the market study did not seem to be accurate with what she sees downtown. She said the economy and culture fundamentally changed with how businesses are developed and loans being granted,

as a result of the collapse of the economy. She said she was not comfortable changing the boundaries of the heart of the community based on the information received tonight.

Commissioner Burger said the plan was ambitious and creative. She said the language the Comprehensive Plan included was from a mid 1990's context. She said in the mid 1990's there was somewhat of a boom. She agreed with some of the speakers this evening on a few issues. She discussed a 'new to new' type of customer that goes to the newest area. She said when the newness wears off and there's another new area there's a lot of infrastructure that everyone is invested in. She said there was the potential for locally owned businesses to suffer and perhaps not make it through that wave. She had concerns regarding environmental issues. She wondered if the project had been put forth before insurance agents to see if it would work. She was concerned this wasn't the right time for a project like this that seemed to be driven by a boom market concept. She stated the former Borders building was still empty and that Sears and Old Navy had left as well. She did not think this was the right Comprehensive Plan. She said labeling it North Mass was a nice idea but with the river it may not be the best way to do something good for North Lawrence. She said a good Comprehensive Plan would include an increase in residential to bring more people into North Lawrence before the retail component. She said a theatre was a nice idea but a grocery store was a great idea. She would like to see more change in the Comprehensive Plan to be more specific in those areas. She appreciated the creative and ambitious thinking behind the project and thanked the developers for wanting to invest in North Lawrence. She said the retail component was too critical and she agreed that they may not have the right information to be able to make a decision. She did not think that was necessarily what the Comprehensive Plan indicated with the mid 1990's content. She appreciated the public comment. She said she could not support the plan at this point.

Commissioner Liese inquired about the rationale for a Comprehensive Plan Amendment. He asked about the parameters of what the developers could do in North Lawrence without the Comprehensive Plan Amendment.

Mr. McCullough said this was not in a neighborhood plan currently. He said with the proposed mixed-use type development staff looked to the Comprehensive Plan first which says that to do that kind of development pattern you need to look at expanding downtown. He said there may have been a different avenue to take with designating it for mixed-use but it was the most efficient and best path with the fact that the applicant wanted to use the CD zoning and mixed-use nature of it. He said the proximity of this to downtown led to looking at that section of the Comprehensive Plan which already talked about a need to expand downtown.

Ms. Miller said the current sites are zoned to a multitude of zoning categories. She said in looking at the Comprehensive Plan the only thing identified in the area were some heavy commercial uses. She said there were no other categories in the Comprehensive Plan that specifically talk about this piece directly north of the Kansas River.

Commissioner Culver had some concerns with the openness of establishing a policy amendment to extend downtown. He said there were a lot of things that were exciting, promising, and opportunistic about expanding downtown into North Lawrence. He agreed with Commissioner Blaser regarding the pull factor and bringing in retail dollars, which wouldn't change if they continued to do the same thing. He thought there was opportunity with this area to follow some of the guidelines within the Comprehensive Plan which seemed to fit. He said he had some difficulty when looking at a current snapshot of some of the vacant buildings discussed this evening. He said buildings may be sitting open today but that it was just a snapshot and they needed to look ahead and start somewhere. He said if plans were not put in place and moved forward the opportunity may diminish. He said the area was an underutilized area of the community with a lot of opportunity. He felt they would be rushed to discount the amendment to the Comprehensive Plan completely and felt they should do their due diligence in looking at it as an opportunity.

Commissioner Britton said he was struggling with this one. He was exciting about the prospect of anything new. He agreed that Massachusetts Street and the historic downtown was the heart of the community and the number one priority needed to be to protecting that. He did not know if this would be a threat to that or not. He said if there was better or more comprehensive information out there then they should take it slow and

know they are going in the right direction with the proper controls. He thought in general this was a great dream development to have but he did not feel like he had enough information and surety that Massachusetts Street would come out as healthy or healthier. He said he looked forward to being able to support something like this but could not support it tonight.

Commissioner Liese said he had all the confidence in the world that the businesses on Massachusetts Street would do fine with competition introduced. He said a really good development would bring in more business. He felt this project was great and he would support it 100%. He felt the pull factor would be great for Massachusetts Street. He wished Mr. Renfro and others from the community were present tonight for more input.

ACTION TAKEN

Motioned by Commissioner Liese, seconded by Commissioner Belt, to defer and direct staff to provide more information to address issues raised.

Commissioner Hird said he would vote against the motion to defer. He said Mr. McClure's comments about vacancy rates were well taken. He said it was always a difficult decision process with this type of proposal. He said it would expand boundaries of downtown to accommodate a mixed-use project that nobody knows what will look like yet. He had great reservations about the square footage limitation and whether that should be doubled for this project. He felt the people from North Lawrence who want development in North Lawrence should be heard. He said it was an exciting proposal in terms of using the waterfront for a change as an asset to the city. He did not want to defer it because he wanted to get this in front of the City Commission so that the elected officials could weigh in and shape it.

Commissioner Liese felt the support Mr. Boyle claims existed in North Lawrence should be present at the meeting. He felt Mr. Boyle representing North Lawrence was inadequate. He wanted more input from the North Lawrence residents.

Commissioner Hird said Mr. Boyle representing the North Lawrence residents was entirely adequate and he didn't personally need ten more people saying the same thing.

Commissioner Singleton said she was not supportive of a deferral because 30 days would not necessarily give her the information she would need to vote to change the boundaries of downtown Lawrence. She said even if they vote it down that did not mean it would not be brought back. She wanted more information and a comprehensive understanding of what would be built.

Commissioner Blaser said he was not sure what kind of information Commissioner Singleton would need to be more confident. He said the Comprehensive Plan Amendment was only to extend the boundary of downtown. He said the plan was not set in stone. He said buildout would not occur until they had someone to pay the bill. He said he would like to move forward with it.

Motion failed 2-6. Commissioners Belt and Liese voted in favor of the motion. Commissioners Blaser, Britton, Burger, Culver, Hird, and Singleton voted in opposition.

Motioned by Commissioner Singleton, seconded by Commissioner Britton, to deny the Comprehensive Plan Amendment.

Commissioner Liese said he thought it was a good idea to extend the boundaries. He said as much as he loved Massachusetts Street and the business represented tonight there was something going on that was reducing the amount of pull Massachusetts Street has. He opposed the motion. He felt it was a mistake to overlook the residents of North Lawrence who want this project.

Commissioner Burger said she would vote in favor of the motion but would really like to see a project in North Lawrence with this density of residential and a grocery store. She said beyond that she would need a lot of convincing. She said it was an exciting project from a design standpoint. She hoped City Commission would give the public the opportunity to fully process this at their level.

Commissioner Hird asked if Planning Commission denies this would City Commission have to have a super majority to approve it.

Mr. McCullough said that was correct. He said City Commission's options would be to send it back to Planning Commission with further direction or override the Planning Commission decision with a 4/5 vote.

Commissioner Belt felt a lot of good could come from the project but he wanted more information and a lot more input from the public. He said downtown was currently expanding to Vermont Street and New Hampshire Street and that there was continued pressure for real estate on those streets because that's where people want to be, although that's not to say they wouldn't also be equally attracted to North Lawrence.

Commissioner Britton said he really liked the idea and wanted to be certain Massachusetts Street would be okay with assurances that the market could sustain that.

Commissioner Burger said Johnny's was a big component in the North Mass proposal and she had no objection to that.

Commissioner Hird felt it was an exciting project. He felt sending it to City Commission with a recommendation of denial would send the wrong message. He felt they should forward it to City Commission with a recommendation of approval and let the process take place at the City Commission level and then again at the Planning level for the proper platting and development of the project. He did not see what would be gained by saying no and hoping the developer would come back with a different proposal. He said he would vote against the motion.

Commissioner Liese said it looked like the vote would be an even split. He said if that was the case he would move for approval of the project.

Commissioner Blaser said he would vote against the motion and agreed with Commissioner Hird that sending it forward to City Commission with a recommendation of denial would send the wrong message since everybody was enthused about the project.

Commissioner Burger asked staff if the Comprehensive Plan Amendment was approved would it enable anybody at any level to reduce the amount of retail, or would this be giving by right the developer to do retail beyond 25,000 sq ft.

Mr. McCullough said the Comprehensive Plan Amendment only sets policy guidance in Horizon 2020. The next application would be where they would have a greater sense of control. He said the zoning would provide those uses by right, restrictions, conditions, etc. He said the Development Code requires a project be consistent with the Comprehensive Plan, which is why the amendment is required with the zoning when it's not consistent with the Comprehensive Plan. He said the Comprehensive Plan Amendment helps the zoning application to be in alignment with the Comprehensive Plan.

Commissioner Liese said in the Comprehensive Plan, Chapter 6, it states downtown Lawrence was expected to change over time. He said he viewed the Comprehensive Plan Amendment as an opportunity to preserve downtown. He did not see any data that it would detract from Massachusetts Street. He said the Comprehensive Plan Amendment would just support the concept of spreading downtown over the bridge.

Mr. McCullough said a Comprehensive Plan policy could be set as specific as they wanted.

Motion failed 4-4. Commissioners Belt, Britton, Burger, and Singleton voted in favor of the motion. Commissioners Blaser, Culver, Hird, and Liese voted in opposition.

Motioned by Commissioner Liese, seconded by Commissioner Blaser, to approve the Comprehensive Plan Amendment.

Commissioner Liese said this was just a Comprehensive Plan Amendment, not a zoning, plats, etc.

Commissioner Britton said the Comprehensive Plan was not law but was very important. He was not comfortable with this and did not support it. He said he was excited about the concept down the road if he could be assured that historic downtown Lawrence would not be negatively affected. He said he would oppose the motion.

Commissioner Burger said she would not support the motion. She said there was too much text in the document that they would be voting to send forward that she did not think had been proven. She said the project was unique and exciting but felt that by adopting the plan they would be indebted to a certain extent to approve future and pending rezonings that she did not want to be forced to vote for.

Motion failed 4-4. Commissioners Blaser, Culver, Hird, and Liese voted in favor of the motion. Commissioners Belt, Britton, Burger, and Singleton voted in opposition.

-----Original Message-----

From: Madison Mater <madisonrmater@gmail.com>

Sent: Friday, December 14, 2018 10:23 AM

To: Denny Ewert <dewert@lawrenceks.org>

Subject: Planning Commissioners

Hello there!

I just wanted to write to express that as a property owner and hopefully long term resident of North Lawrence, I am so very excited and supportive of the proposed development along the river levee. I feel that it will improve the value of my property as well as provide more retail options for myself and my neighbors of North Lawrence.

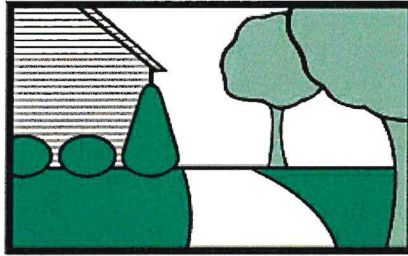
I personally walk along the levee often and am saddened by how under-utilized one of the most beautiful places of Lawrence is. As a city that was founded along the river I wonder why it is not more appreciated and enjoyed.

I bought my house with the idea that someday my elderly mother will live there and I would love for her to have such easy access to more retail and a place she can sit and enjoy watching the eagles in the winter as she so loves to do.

I hope that you plan to move forward with this project as it would personally mean a great deal to me. Thank you for your time and your consideration.

Respectfully,

Madison R. Mater



LAWRENCE
Landscape

Design, Installation, Maintenance, and Sprinkler Systems

December 17, 2018

Douglas County Planning Commission members,

I wish to offer my support to the professional staff's recommendation for approval of the rezoning and preliminary development plan for the North Lawrence Riverfront project. My interest is more than casual, being a business owner, as well as industrial and residential property owner for over 20 years in this neighborhood. As North Lawrence goes, so goes my future.

It seems this project makes sense on many levels.

Capitalizing on a defining natural feature, The Mighty Kaw.

Infill development- repurposing the collection of properties to their highest & best use

Continuing the charm & allure of downtown without expansion into the neighborhoods

Creating an attractive gateway into downtown from the North.

Leveraging the existing infrastructure.

An attractive project that will increase sales tax, and property tax revenue.

Increased residents in North Lawrence, hopefully enough for a viable grocery store.

Updated mechanical systems to Johnny's while preserving its charm.

Local ownership of this project, by successful Lawrencians, reinvesting here.

This seems to be the kind of project that the community should embrace.

It checks all the boxes.

Thanks for considering my comments.

A handwritten signature in blue ink that reads "Frank D. Male". The signature is fluid and cursive, with a long, sweeping underline.

Frank Male

785.979.4129

frank@lawrencelandscape.com

600 Lincoln Lawrence, Kansas. 66044
785.843.4370 info@lawrencelandscape.com

**PLANNING COMMISSION REPORT
NON PUBLIC HEARING ITEM
Public Hearing on variance only**

PC Staff Report
12/19/18

**ITEM NO 2B: PRELIMINARY PLAT FOR NORTH LAWRENCE RIVERFRONT
ADDITION; 311, 317, 401, 409, 415, 501, 505 N 2ND ST (SLD)**

PP-18-00504: Consider a one-lot Preliminary Plat for North Lawrence Riverfront Addition, located at 311, 317, 401, 409, 415, 501, & 505 N 2nd St for mixed-use development including residential and commercial uses. Variances related to block length and right-of-way dedication for N. 2nd Street as a principal arterial. Submitted by Paul Werner Architects on behalf of Abfield Investments LLC, City of Lawrence, Douglas County Kaw Drainage District, D&D Rentals of Lawrence LLC, Exchange Holdings LLC, HDD of Lawrence LLC, Kaw River Estates LLC, Patience LLC, Loosehead Investments LLC, and Riverfront Properties of Lawrence LLC, property owners of record.

STAFF RECOMMENDATION:

Staff recommends deferral of the preliminary plat to allow additional time for the applicant and the City to negotiate the phasing requirements related to the potential transfer of the city's property to the applicant.

Reason for Request:

Preliminary development requirement.

KEY POINTS

- Current ownership includes both public and private property owners.
- Proposed subdivision combines multiple lots into one single lot.
- Proposed subdivision includes vacation of existing public right-of-way for N. 1st Street, Maple Street and N. Massachusetts Street.
- The Planning commission unanimously approved the Preliminary Plat PP-2-1-12 on July 22, 2015. Approval is valid for 24 months unless an extension is approved.
- The Planning commission approved an extension per Section 20-809 (J) on May 24, 2017. The approval was extended to July 22, 2019.
- The applicant submitted this revised preliminary plat application that modified the proposed interior easements from the approved preliminary plat.

SUBDIVISION CITATIONS TO CONSIDER

- This application is being reviewed under the Subdivision Regulations for Lawrence and Unincorporated Douglas County, effective Jan 10, 2012.
- Section 20-810 Subdivision Design Standards
- Section 20-813 (g) Variances

ATTACHMENTS

Attachment A: Preliminary Plat

ASSOCIATED CASES/OTHER ACTION REQUIRED

Associated Cases:

Note: the City Commission has approved rezoning request Z-12-29-11, Z-12-30-11, Z-12-32-11, Z-12-33-11, Z-12-34-11, Z-12-35-11, and Z-12-36-11 on May 1, 2012 to the Downtown Commercial

(CD) District subject to approval and adoption of design guidelines. The design guidelines were not approved and the change to CD is not published or effective. The current zoning designations are the effective district designations. The 2011 applications, listed below, are combined, and replaced with Z-18-00505.

- **Z-12-29-11:** 1.38 acres from IG (General Industrial) and CS (Commercial Strip) to CD (Downtown Commercial), located at 401 & 415 North 2nd Street.
- **Z-12-30-11:** 2.14 acres from IG (General Industrial) to CD (Downtown Commercial), located at 0 & 100 Lincoln Street and 151 & 100 Perry Street.
- **Z-12-32-11:** .83 acres from IG (General Industrial) and CS (Commercial Strip) to CD (Downtown Commercial), located at 409 & 501 North 2nd Street.
- **Z-12-33-11:** .34 acres from OS (Open Space) and CS (Commercial Strip) to CD (Downtown Commercial), located at 300, 311, & 317 North 2nd Street.
- **Z-12-34-11:** 1.61 acres from IG (General Industrial) to CD (Downtown Commercial), located at 139 Perry Street, 505 North 2nd Street & 141 Maple Street.
- **Z-12-35-11:** .55 acres from IG (General Industrial) to CD (Downtown Commercial), located at 133 Perry Street.
- **Z-12-36-11:** 1.38 acres from IG (General Industrial) to CD (Downtown Commercial), located at 600 North 1st Street, Block 3.

Z-18-00505: 16.116 acres from OS (Open Space) District, CS (Strip Commercial) District, & IG (General Industrial) District to CD-PD (Downtown Commercial with Planned Development Overlay) District.

PDP-18-00506: Preliminary Development Plan for North Lawrence Riverfront for a mixed-use development including residential and commercial uses and structured garage parking.

PP-02-01-12: Preliminary Plat for North Lawrence Riverfront Addition.

Other Action Required:

- Submittal of final plat for administrative approval and recordation.
- City Commission acceptance of dedication of easements and vacations of existing right-of-way as depicted on the on the final plat.
- Submittal and approval of public improvement plans and provision of means of assurance of completion shall be submitted prior to the recording of the final plat.
- Submittal and approval of a final development plan and recording the Plan with the Register of Deeds Office.

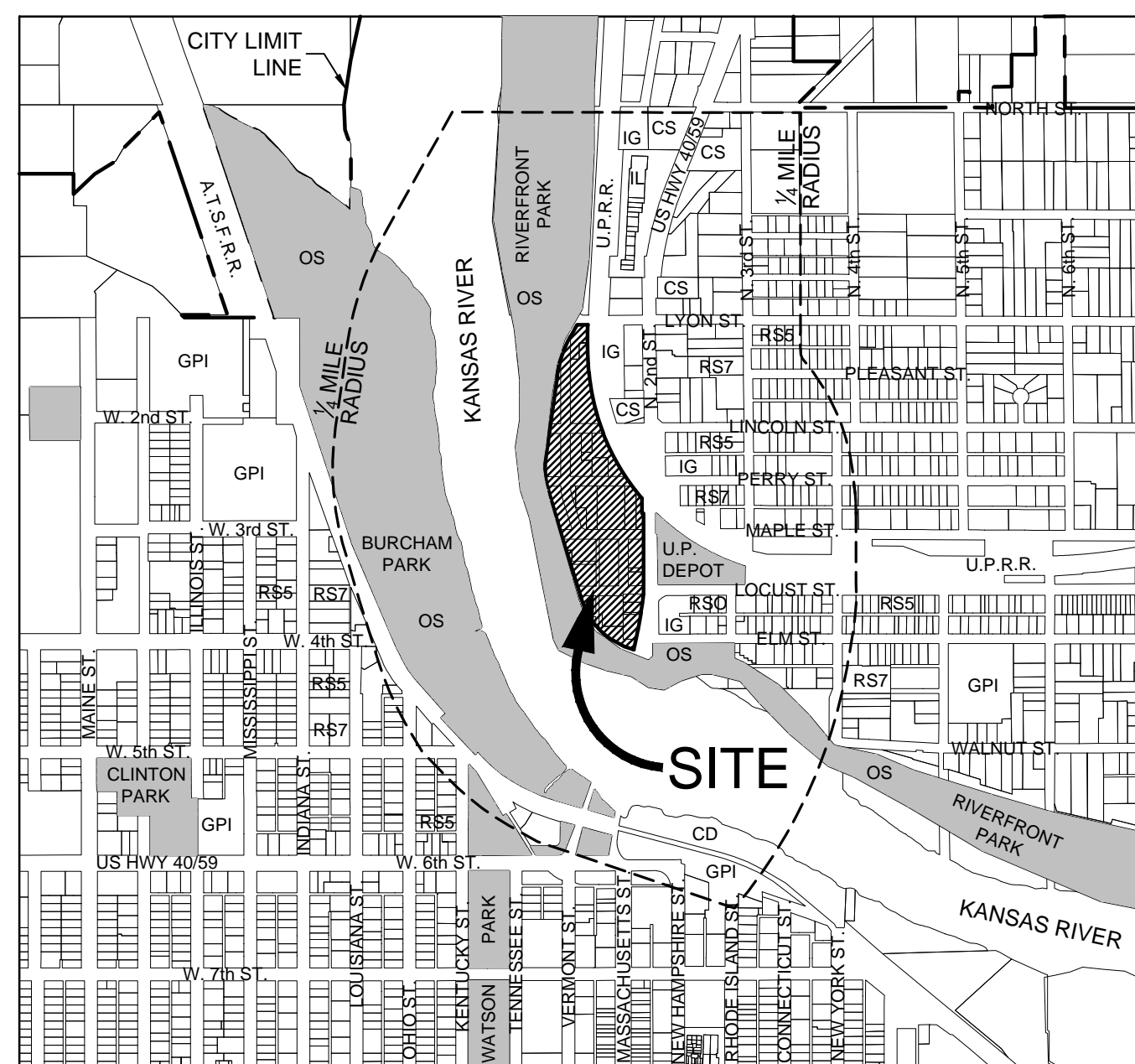
SUMMARY

The proposed development application includes properties owned by the City of Lawrence. The applicant has been working with the City to transfer ownership based on an approved plan. Discussions continue regarding how and when transfer of ownership will occur if it occurs. As a result, the subdivision plat process is temporarily deferred to allow a plan to be fully developed that aligns the transfer of ownership with the development phasing, which may impact the design of the plat. Rezoning application no. Z-18-00505 and preliminary development plan application no. pdp-18-00606 can proceed while the plat remains under review.

The preliminary plat will be added as a future Planning Commission agenda item.



LOCATION MAP:



GENERAL NOTES:

- OWNERS:
 - CITY OF LAWRENCE
123 W. 8th STREET, SUITE B2
LAWRENCE, KANSAS 66044
 - EXCHANGE HOLDINGS, LLC
PO BOX 505
LAWRENCE, KANSAS 66044
 - HDD OF LAWRENCE, LLC
PO BOX 706
LAWRENCE, KANSAS 66044
 - KAW RIVER ESTATES, LLC
PO BOX 706
LAWRENCE, KANSAS 66044
 - RIVERFRONT PROPERTIES OF LAWRENCE, LLC
PO BOX 505
LAWRENCE, KANSAS 66044
- LAND PLANNERS:
 - PAUL WERNER ARCHITECTS
123 W. 8th STREET, SUITE B2
LAWRENCE, KANSAS 66044
 - LANDPLAN ENGINEERING, P.A.
1310 WAKARUSA DRIVE
LAWRENCE, KANSAS 66049
- CIVIL ENGINEER/
LAND SURVEYOR:
 - LANDPLAN ENGINEERING, P.A.
1310 WAKARUSA DRIVE
LAWRENCE, KANSAS 66049
- TOPOGRAPHIC INFORMATION GENERATED FROM LIDAR DATA OBTAINED BY DOUGLAS COUNTY IN MARCH, 2013.
- EXISTING ZONING: IG, CS, OS
- EXISTING LAND USE: COMMERCIAL, RETAIL, RESIDENTIAL, VACANT
- PROPOSED LAND USE: COMMERCIAL, RETAIL, RESIDENTIAL
- NO PORTION OF THE SUBJECT PROPERTY LIES WITHIN A DESIGNATED "SPECIAL FLOOD HAZARD AREA" AS DEFINED BY FLOOD INSURANCE RATE MAP (FIRM; PANEL NO. 176, MAP NUMBER 2045C0176D, DOUGLAS COUNTY, KANSAS, BEARING AN EFFECTIVE DATE OF AUGUST 5, 2010).
- TYPICAL SOIL TYPE: EUDORA-URBAN LAND COMPLEX, RARELY FLOODED.
- DEVELOPER IS RESPONSIBLE FOR THE COST OF ANY RELOCATION OF EXISTING UTILITIES, IF NECESSARY TO SERVE THE PROPOSED SUBDIVISION.
- ALL UTILITIES WILL BE LOCATED UNDERGROUND, PER SECTION 20-809(d)(4)(v) OF THE CITY CODE.
- IF A BASEMENT IS BUILT ON A LOT WHERE A MINIMUM ELEVATION HAS BEEN ESTABLISHED, THE BUILDING DESIGN IS ENCOURAGED TO INCORPORATE A SUMP PUMP.
- ALL EXISTING STRUCTURES ON SITE WILL REMAIN FOLLOWING THE FINAL PLAN.
- ALL NORTH LAWRENCE AND ADDITION PLATTED RIGHTS-OF-WAY LOCATED WITHIN THE BOUNDARY DEPICTED ON THIS PRELIMINARY PLAT WILL BE VACATED WITH THE FINAL PLAT.
- PROPOSED UTILITY LOCATIONS AND ASSOCIATED EASEMENTS ARE PRELIMINARY AND WILL BE FINALIZED WITH FORTHCOMING PUBLIC IMPROVEMENT PLANS AND FINAL PLATS.

LEGAL DESCRIPTION:

A TRACT OF LAND LOCATED IN NORTH LAWRENCE AND ADDITIONS, DOUGLAS COUNTY KANSAS, MORE PARTICULARLY DESCRIBED, AS FOLLOWS:
COMMENCING AT THE NORTHEAST CORNER OF THE NORTHWEST QUARTER OF SECTION 30, TOWNSHIP 12 SOUTH, RANGE 20 EAST OF THE 6th PRINCIPAL MERIDIAN; THENCE SOUTH 21°15'22" EAST ALONG THE EAST LINE OF SAID NORTHWEST QUARTER, 2649.36 FEET TO THE SOUTHEAST CORNER OF SAID NORTHWEST QUARTER; THENCE SOUTH 88°12'57" WEST ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER, 867.01 FEET TO THE POINT OF BEGINNING; THENCE NORTH 1°08'09" WEST, 31.76 FEET; THENCE NORTH 1°47'13" WEST, 100.00 FEET; THENCE SOUTH 88°12'57" WEST, 10.00 FEET; THENCE NORTH 1°47'13" WEST, 36.46 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID WEST RAILROAD RIGHT-OF-WAY LINE ON A CURVE TO THE RIGHT HAVING A RADIUS OF 1402.45 FEET, AN ARC LENGTH OF 472.49 FEET, A CHORD BEARING NORTH 32°31'19" WEST AND A CHORD LENGTH OF 470.06 FEET; THENCE CONTINUING ALONG SAID WEST RAILROAD RIGHT-OF-WAY LINE NORTH 21°30'30" WEST, 63.61 FEET; THENCE CONTINUING ALONG SAID WEST RAILROAD RIGHT-OF-WAY LINE ON A CURVE TO THE RIGHT HAVING A RADIUS OF 1402.45 FEET, AN ARC LENGTH OF 472.49 FEET, A CHORD BEARING NORTH 32°31'19" WEST AND A CHORD LENGTH OF 470.06 FEET; THENCE CONTINUING ALONG SAID WEST RAILROAD RIGHT-OF-WAY LINE ON A CURVE TO THE RIGHT HAVING A RADIUS OF 2750.00 FEET, AN ARC LENGTH OF 266.78 FEET, A CHORD BEARING NORTH 7°55'23" WEST AND A CHORD LENGTH OF 266.83 FEET TO THE INTERSECTION OF THE WEST RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD WITH THE SOUTH RIGHT-OF-WAY LINE OF LYON STREET; THENCE ALONG SAID SOUTH RIGHT-OF-WAY LINE SOUTH 88°12'57" WEST, 68.51 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF THE LEVEE; THENCE ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 27°24'55" WEST, 83.11 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF 700.00 FEET, AN ARC LENGTH OF 266.44 FEET, A CHORD BEARING SOUTH 16°15'19" WEST AND A CHORD LENGTH OF 266.81 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 88°12'57" WEST, 51.73 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF 650.00 FEET, AN ARC LENGTH OF 308.18 FEET, A CHORD BEARING SOUTH 69°01'13" EAST AND A CHORD LENGTH OF 207.17 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 18°24'09" EAST, 487.24 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 18°24'09" EAST, 219.86 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF 553.50 FEET, AN ARC LENGTH OF 124.32 FEET, A CHORD BEARING SOUTH 29°53'02" EAST AND A CHORD LENGTH OF 124.55 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 32°11'02" EAST, 28.07 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE SOUTH 45°24'02" EAST, 75.24 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF 1854.20 FEET, AN ARC LENGTH OF 121.21 FEET, A CHORD BEARING NORTH 10°09'20" EAST AND A CHORD LENGTH OF 121.19 FEET; THENCE CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE NORTH 15°15'08" EAST AND A CHORD LENGTH OF 122.75 FEET; THENCE CONTINUING ALONG SAID WEST RIGHT-OF-WAY LINE ON A CURVE TO THE LEFT HAVING A RADIUS OF 1670.50 FEET, AN ARC LENGTH OF 106.50 FEET, A CHORD BEARING NORTH 1°08'09" EAST AND A CHORD LENGTH OF 106.57 FEET; THENCE CONTINUING ALONG SAID WEST RIGHT-OF-WAY LINE NORTH 3°31'37" WEST, 50.03 FEET; THENCE CONTINUING ALONG SAID WEST RIGHT-OF-WAY LINE NORTH 1°38'22" WEST, 292.34 FEET; THENCE CONTINUING ALONG SAID WEST RIGHT-OF-WAY LINE NORTH 1°08'09" WEST, 27.74 FEET TO THE POINT OF BEGINNING; CONTAINS 15.116 ACRES, MORE OR LESS.

FIRE/MEDICAL ACCESS:

- SINCE THE SECOND ENTRANCE (EMERGENCY ENTRANCE AND RIGHT-OUT) SOUTHBOUND EXIT AS DEPICTED ON THE PRELIMINARY PLAT, IS CLOSER TO THE MAIN ENTRANCE THAN IS PERMITTED BY THE 2012 IFC, THE OWNERS OF THE GROUND HAVE AGREED TO THE FOLLOWING:
 - a) THE FOOTPRINT OF ALL HABITABLE STRUCTURES SHALL NOT EXCEED 124,000 SF.
 - b) ALL STRUCTURES SHALL BE PROTECTED BY AN AUTOMATIC SPRINKLER SYSTEM MEETING NFPA 13.
 - c) STRUCTURES SHALL BE LIMITED TO 75'0" ABOVE THE ELEVATION OF THE LEVEE.
 - d) THERE SHALL BE NO OTHER LIMITS ON ALLOWED OCCUPANCIES OTHER THAN THE 124,000 SF FOOTPRINT.
 - e) STAND ALONE PARKING STRUCTURES SHALL NOT COUNT TOWARDS THE 124,000 SF LIMIT.
 - f) THE ENTIRE AREA OF THIS PRELIMINARY PLAT IS AVAILABLE FOR DEVELOPMENT.
- IF AN ALTERNATIVE ACCESS POINT BECOMES AVAILABLE, THESE STIPULATIONS MAY BE AMENDED OR ELIMINATED BASED ON THE 2012 IFC.

BENCHMARKS:

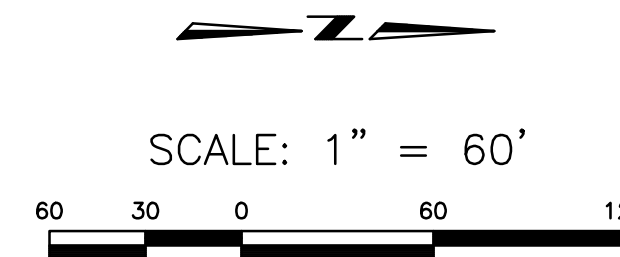
- BM #1 CHISELED "C" NEAR THE SOUTHEAST CORNER OF A CURB INLET ON THE EAST SIDE OF NORTH 2nd STREET. INLET IS LOCATED JUST NORTH OF THE INTERSECTION WITH LYON STREET. ELEV. 818.06
- BM #2 CHISELED "C" ON THE EAST SIDE OF THE MANHOLE IN THE TOP OF A CURB INLET ON THE EAST SIDE OF NORTH 2nd STREET, ABOUT 250' NORTH OF THE NORTH LAWRENCE PUMP STATION AT 734 N. 2nd STREET. ELEV. 817.99
- BM #3 CHISELED "C" ON THE EAST EDGE OF A WET WELL NEAR THE NORTHEAST CORNER OF THE UNION PACIFIC OVERPASS, CHISELED "C" IS 59.50' EAST OF THE CENTERLINE OF NORTH 2nd STREET AND 37' SOUTH OF THE CENTERLINE OF PERRY STREET. ELEV. 825.66

PROVISION AND FINANCING OF ROADS, SEWER AND OTHER PUBLIC SERVICES:

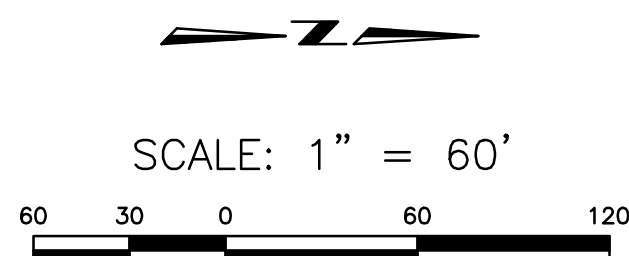
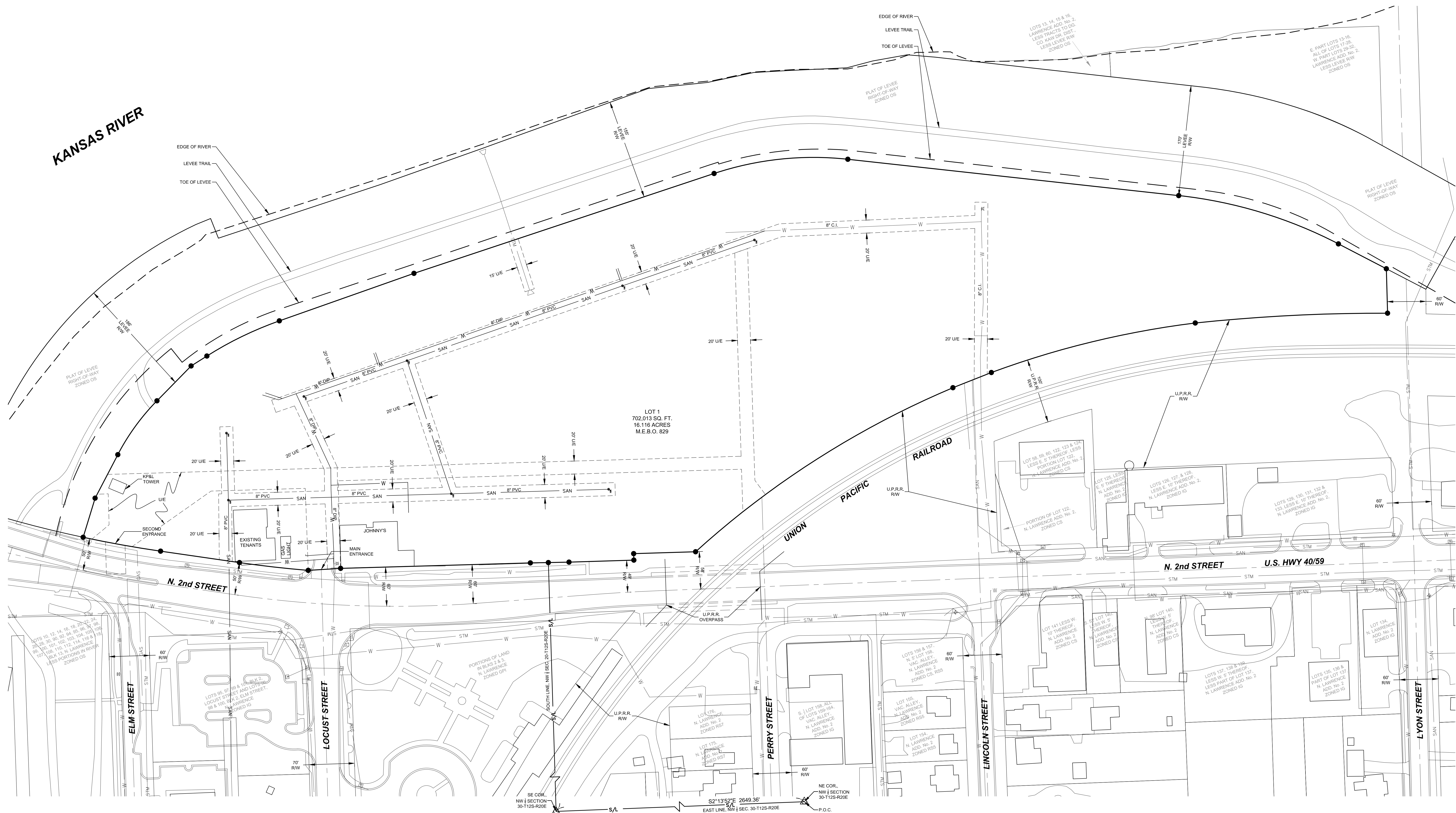
- THE SUBDIVISION WILL DEDICATE NEW PUBLIC RIGHTS-OF-WAY. THE SUBDIVISION WILL TAKE ACCESS TO AND FROM EXISTING PUBLIC ROADS, NAMELY N. 2nd STREET.
- THE SUBDIVISION WILL PROVIDE CONNECTIONS TO THE CITY OF LAWRENCE WATER SYSTEM VIA AN EXISTING 12" MAIN LOCATED IN THE LOCUST STREET RIGHT-OF-WAY AND AN EXISTING 8" MAIN LOCATED IN THE LYON STREET RIGHT-OF-WAY.
- THE SUBDIVISION WILL PROVIDE A CONNECTION TO THE CITY OF LAWRENCE SANITARY SEWER SYSTEM VIA AN EXISTING 8" MAIN LOCATED IN THE LYON STREET RIGHT-OF-WAY.
- PURCHASERS OF LOTS IN THE SUBDIVISION MAY OR MAY NOT BE SUBJECT TO SPECIAL ASSESSMENTS OR OTHER COSTS OF STREETS, ROADS, WATER LINES AND TREATMENT, AND/OR WASTEWATER LINES AND TREATMENT.
- THE PROVISION OF IMPROVED ROADS, WATER SERVICE AND/OR WASTEWATER SERVICE MAY OR MAY NOT DEPEND ON A VOTE, PETITION OR OTHER COLLECTIVE ACTION OF PROPERTY OWNERS IN THE SUBDIVISION AND/OR FINANCIAL INCENTIVES OR CONTRIBUTIONS FROM THE CITY OF LAWRENCE.

SITE SUMMARY:

GROSS AREA:	702,013 SF / 16.116 AC
NET AREA:	702,013 SF / 16.116 AC
TOTAL LOTS:	1
AVERAGE LOT SIZE:	702,013 SF / 16.116 AC
MINIMUM LOT AREA:	702,013 SF / 16.116 AC
MAXIMUM LOT AREA:	702,013 SF / 16.116 AC

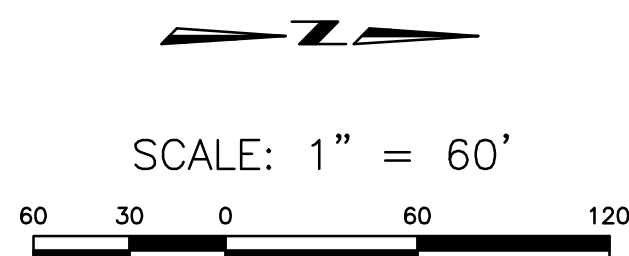
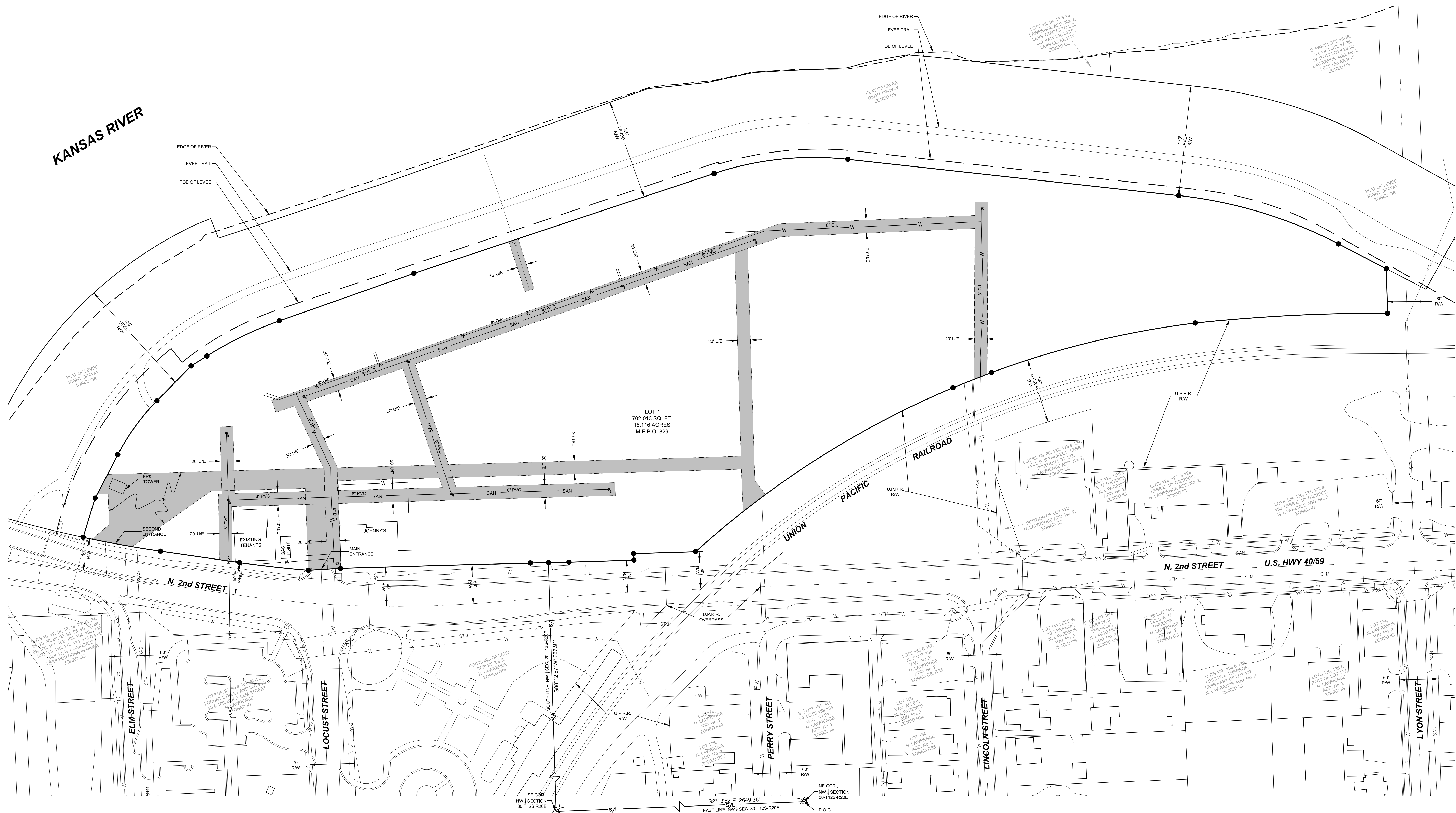


REV	DATE	DESCRIPTION
1	10/12/2018	CL, PL, & TOS
2	10/12/2018	MASS ST
3	10/12/2018	AGE & I/P
4	10/12/2018	REVISIONS
5	10/12/2018	17th DIST. COMMENTS
6	10/12/2018	17th DIST. COMMENTS

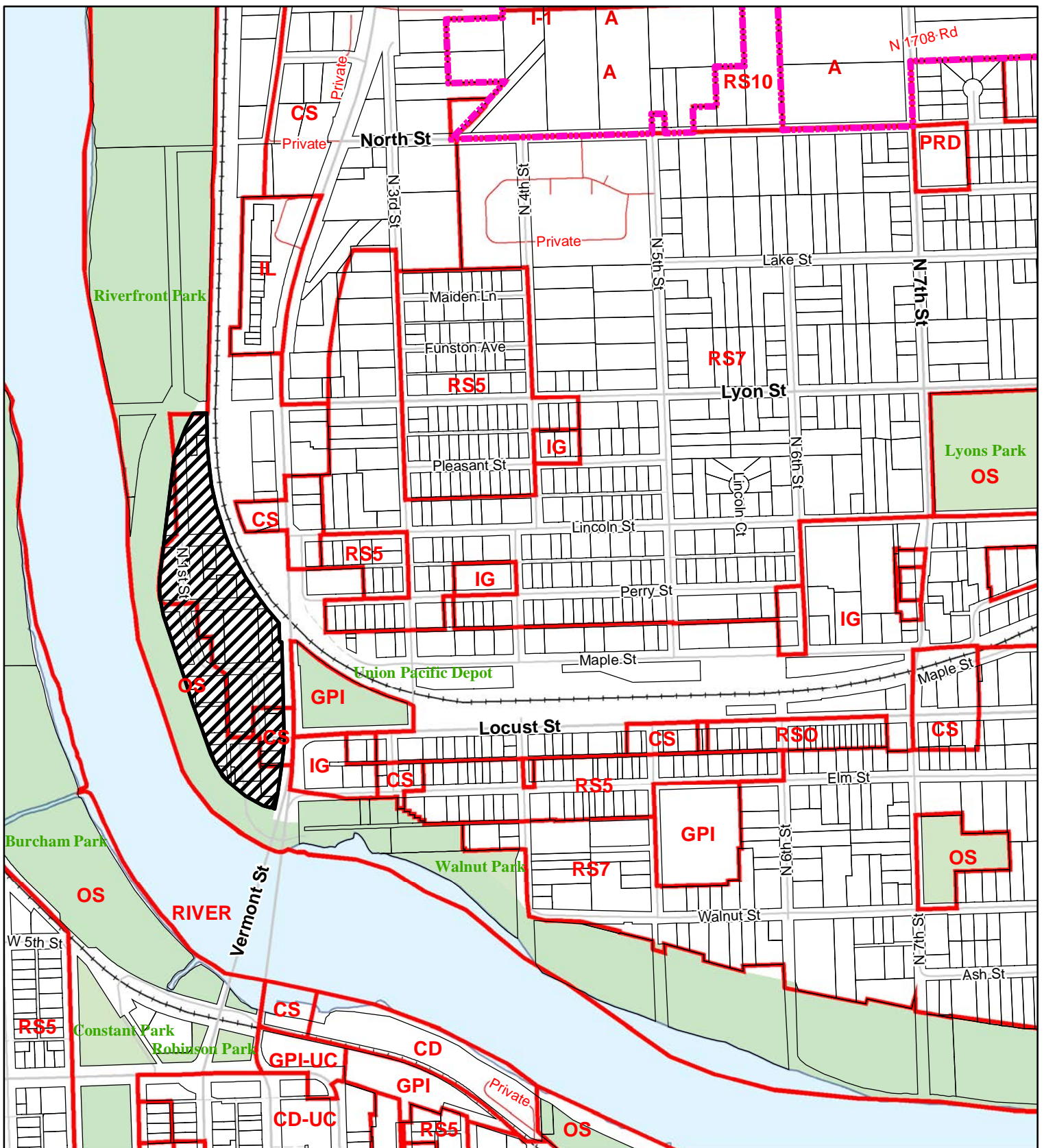


REV	DATE	DESCRIPTION
1	5/20/10	C.L. PL & TOE
2	5/23/10	MASS ST.
3	5/25/10	A/E & U/E
4	7/26/12	REVISIONS
5	5/27/15	PER REPT. COMMENT'S
6	10/12/18	UPDATE SANITARY, WATERLINE & U/E

DATE:	1/13/10
PROJECT NO.:	20153011
DESIGNED BY:	PWA/LPE
DRAWN BY:	BS
CHECKED BY:	CMS



REV	DATE	DESCRIPTION
1	5/20/10	CL, PL & TOE
2	5/23/10	MASS ST.
3	5/26/10	A/E & U/E
4	7/26/12	REVISIONS
5	5/27/15	FOR DEPT. COMMENTS
6	10/12/18	UPDATE SANITARY, WATERLINE & U/E



PP-18-00504 & PDP-18-00506:
 Consider a Preliminary Plat and Preliminary Development Plan for
 approximately 16.116 acres
 located at 311, 317, 401, 409, 415, 501, & 505 N. 2nd STREET.



-----Original Message-----

From: Madison Mater <madisonrmater@gmail.com>

Sent: Friday, December 14, 2018 10:23 AM

To: Denny Ewert <dewert@lawrenceks.org>

Subject: Planning Commissioners

Hello there!

I just wanted to write to express that as a property owner and hopefully long term resident of North Lawrence, I am so very excited and supportive of the proposed development along the river levee. I feel that it will improve the value of my property as well as provide more retail options for myself and my neighbors of North Lawrence.

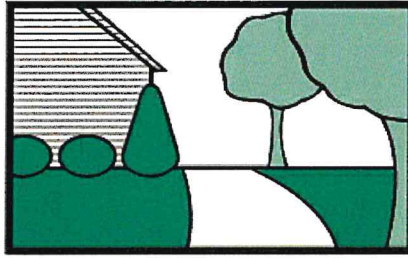
I personally walk along the levee often and am saddened by how under-utilized one of the most beautiful places of Lawrence is. As a city that was founded along the river I wonder why it is not more appreciated and enjoyed.

I bought my house with the idea that someday my elderly mother will live there and I would love for her to have such easy access to more retail and a place she can sit and enjoy watching the eagles in the winter as she so loves to do.

I hope that you plan to move forward with this project as it would personally mean a great deal to me. Thank you for your time and your consideration.

Respectfully,

Madison R. Mater



LAWRENCE
Landscape

Design, Installation, Maintenance, and Sprinkler Systems

December 17, 2018

Douglas County Planning Commission members,

I wish to offer my support to the professional staff's recommendation for approval of the rezoning and preliminary development plan for the North Lawrence Riverfront project. My interest is more than casual, being a business owner, as well as industrial and residential property owner for over 20 years in this neighborhood. As North Lawrence goes, so goes my future.

It seems this project makes sense on many levels.

Capitalizing on a defining natural feature, The Mighty Kaw.

Infill development- repurposing the collection of properties to their highest & best use

Continuing the charm & allure of downtown without expansion into the neighborhoods

Creating an attractive gateway into downtown from the North.

Leveraging the existing infrastructure.

An attractive project that will increase sales tax, and property tax revenue.

Increased residents in North Lawrence, hopefully enough for a viable grocery store.

Updated mechanical systems to Johnny's while preserving its charm.

Local ownership of this project, by successful Lawrencians, reinvesting here.

This seems to be the kind of project that the community should embrace.

It checks all the boxes.

Thanks for considering my comments.

A handwritten signature in blue ink, reading "Frank D. Male". The signature is fluid and cursive, with a long, sweeping underline.

Frank Male

785.979.4129

frank@lawrencelandscape.com

600 Lincoln Lawrence, Kansas. 66044
785.843.4370 info@lawrencelandscape.com

PLANNING COMMISSION REPORT
Regular Agenda -Public Hearing Item

PC Staff Report
12/19/2018

**ITEM NO. 2C PRELIMINARY DEVELOPMENT PLAN FOR NORTH LAWRENCE
RIVERFRONT; 311, 317, 401, 409, 415, 501, 505 N 2ND ST (SLD)**

PDP-18-00506: Consider a Preliminary Development Plan for North Lawrence Riverfront, located at 311, 317, 401, 409, 415, 501, & 505 N. 2nd St. The project includes multiple phases and mixed residential and commercial development. Submitted by Paul Werner Architects on behalf of Abfield Investments LLC, City of Lawrence, Douglas County Kaw Drainage District, D&D Rentals of Lawrence LLC, Exchange Holdings LLC, HDD of Lawrence LLC, Kaw River Estates LLC, Patience LLC, Loosehead Investments LLC, and Riverfront Properties of Lawrence LLC, property owners of record.

STAFF RECOMMENDATION ON PRELIMINARY DEVELOPMENT PLAN: Planning Staff recommends approval of PDP-18-00506, North Lawrence Riverfront Preliminary Development Plan, based upon the findings of fact presented in the body of the staff report and forwarding a recommendation for approval to the City Commission subject to the following conditions:

1. The applicant shall provide a revised preliminary development plan drawing with the following notes and changes:
 - a. Provision of a note on the face of the preliminary development plan that states, "Off-street parking shall be required and approved for all uses per the approved preliminary and final development plans for this development."
 - b. Provision of a note on the face of the preliminary development plan that states, "Prior to the development of buildings, IX, X, or XI the developer shall submit a revised preliminary development plan for review and approval per Section 20-1304 of the Land Development Code."
 - c. Provision of a note on the face of the preliminary development plan that states, "Provision of a local floodplain permit is required for review and approval for phases that impact levee and regulatory floodplain and shall be submitted concurrently with an application for a final development plan."
 - d. Provision of a note on the face of the preliminary development plan that states, "Prior to the submission of a final development plan for any phase of the development the site project shall demonstrate compliance with applicable conditions for building height, massing, and scale as approved by the Historic Resources Commission."
 - e. Provision of a revised plan showing building footprints comply with the maximum 25,000 square feet standards per Section 20-210

Applicant Reason for Request Over the last twelve years, we have met with the North Lawrence Improvement Association, Wester Energy, the Union Pacific Railroad, Kaw Drainage District, and City Staff on several occasions. We have also resubmitted a plan to the Corp of Engineers through Matt Bond with the City of Lawrence.

We plan to continue to reach out to these various entities, met with, and answer any questions anyone may have.

This project could be developed under the “CD” district and the standard site planning process. The proposed CD-PD offers greater oversight, review and comments from city staff and the public in general. The goal/intent of this project is to provide a mixed-use development that is beneficial not only to the residents of the development, but for the use and enjoyment of all residents in North Lawrence and the City of Lawrence residents as a whole. For these reasons, we believe that the PDP is a better process to meet the goals of the developers and the city.

We feel that a planned development offers a more transparent process moving forward with the project as opposed to the standard site planning process. We agree with the city that this is the best approach for everyone involved. It gives the City and the public more notification and involvement in the development of the area.

KEY POINTS

- Project includes multiple parcels and property owners.
- Project is proposed as phased development between 2019 -2021 and beyond.
- Full buildout of project requires second access that must be negotiated with Union Pacific Railroad.
- Existing buildings will remain as follows:
 - 401 N. 2nd Street – Johnny’s Tavern
 - 317 N. 2nd Street – Gaslight
 - 311 N. 2nd Street – Commercial building – multi-tenant.
 - 401 N. 2nd Street – grain elevator
- A significant feature of proposed project is the incorporation of the riverfront as a focal point of the development.
- Project includes surface and garage parking. Typically, off-street parking is not required in the CD (Downtown Commercial) District.
- The original application was for conventional zoning with the intention and requirement for the development of project specific development guidelines. The PD overlay is submitted in lieu of the development of separate design guidelines.
- The project is subject to historic environs review as it relates to the Union Pacific Depot (402 N. 2nd Street).

FACTORS TO CONSIDER

- Conformance with the purpose of Planned Developments (Section 20-701, Land Development Code).
- Compliance with Development Code.
- Conformance with *Horizon 2020*.
- Conformance with Subdivision Regulations.

ASSOCIATED CASES

- CPA-11-08-11; The Planning Commission considered the item on February 27, 2012, and the vote resulted in a failure to recommend. The City Commission approved the CPA 3-2.
- Z-18-00505; IG, CS, and OS to CD-PD, Item 2A – See staff report
- PP-18-00504; one lot preliminary plat, Item 2B – Deferred

OTHER ACTION REQUIRED

- City Commission approval of preliminary development plan and requested modifications.
- Submittal and approval of final development plan.
- Submittal and approval of public improvement plans.
- Recording of final development plan with Register of Deeds Office.
- Application and release of building permits prior to development.
- Submission and approval of final plat.
- Execution of agreement with City for ownership transfer of public properties.

ATTACHMENTS

1. Preliminary Development Plan
2. Traffic Impact Study
3. Drainage Study
4. Applicant's summary memo

PUBLIC COMMENT

- Non received

GENERAL INFORMATION	
Current Zoning and Land Use:	<p>Existing zoning includes CS (Commercial Strip) District, IG (General Industrial) District, and OS (Open Space) District. The Commission previously approved CD (Downtown Commercial) subject to the condition of approving specific design guidelines for the project. This condition was not met; the zoning was not published and is not effective.</p> <p>Uses include existing <i>Restaurant</i>, <i>Bar</i>, and commercial uses. Some <i>Residential</i> uses are located along the west side of the property.</p> <p>Proposed zoning is discussed in staff report for Z-18-00505.</p>
Surrounding Zoning and Land Use:	
To the north; north of Lyon Street:	OS (Open Space) District; existing Riverfront Park.
To the east; east of the railroad tracks north of Lincoln Street:	IG (General Industrial) District and CS (Commercial Strip) District; existing automotive sales and services and contractor shop.
To the east; east of N. 2 nd Street south of Lincoln Street:	CS (Commercial Strip) District; existing parking lot. IG (General Industrial) District; manufacturing use. GPI (General Public and Institution) District; Union Pacific Depot.
To the south:	OS (Open Space) District; existing Riverfront Park and River.
To the west:	OS (Open Space) District; existing levee and Riverfront Park.

PROJECT SUMMARY

This project includes multiple uses including *Office, Commercial Sales and Services, Transient Accommodations* (hotel), and *Multi-Dwelling Residential* uses. Uses also include public open space and structured parking.

The development application includes a preliminary plat that consolidates parcels and vacates existing public right-of-way to create a single platted lot. The preliminary plat is deferred to allow additional review by City staff as it pertains to phasing and the transfer of ownership to the developer. Preliminary development plans may also be considered preliminary plats. This application is not intended to be reviewed for subdivision compliance. Extensive easement dedications are required with this project and can more efficiently as a separate document. Preliminary plats require Planning Commission approval, as such; the Planning Commission should anticipate a future agenda item to replat the subject property. The proposed preliminary development plan provides a framework for assessing the project with respect to land use, adequacy of public infrastructure, phasing, and general building arrangement. Maximum building height and massing are the purview of the Historic Resources Commission. Action by the Historic Resources regarding height, bulk, and massing will be incorporated into the preliminary development plan.

The development is intended to occur in phases. Much of the phasing is restricted by access. Only one full access is provided to the development. Access is located on the west side of the N. 2nd Street and Locust Street intersection. A secondary access is located south of the intersection and provides an exit only, southbound option for the development.

Table 1 Site Summary All Phases

Summary listed in square feet unless noted.	Existing	Percent	Proposed		Change
Land Area: <i>28 parcels</i> <i>4.56 acres right-of-way</i>	702,013 16.12 acres		702,013 16.12 acres		<i>none</i>
Total Footprint of Buildings:	36,069	5%	95,850	14%	59,781 increase
Total Pavement:	3,421 (excluding existing streets)	0%	192,382	27%	188,961 increase
Total Impervious Area:	39,490	6%	292,232	42%	252,742 increase
Total Pervious Area:	662,523	94%	409,781	58%	252,742 decrease

The project as proposed is a single lot with multiple phases. Existing streets and public right-of-way would be vacated with this project. If approved, the preliminary development plan will be used as the baseline of development moving forward.

STAFF ANALYSIS

Section 20-908 of the Land Development Code states that required off-street parking shall be located on the same lot as the principal use. Exceptions to this standard are provided in 20-909 and include shared and off-site parking. The CD District is except from this requirement. The 2011 rezoning approval included the following conditions.

1. The maximum footprint of an individual store shall not exceed 50,000 gross square feet.
2. Due to the unique characteristics of the Downtown Commercial District (city-provided parking), current Section 20-901 (f) of the Land Development Code exempts uses in the CD district from the requirement to provide off-street parking and off-street loading spaces. One of the characteristics of the subject rezoning is that the city will not provide

parking; therefore, the Downtown Commercial District of the subject request shall not have said exemption and shall instead provide parking at the code prescribed ratio for the allowed uses as outlined in the Land Development Code or as adjusted in approved design guidelines.

3. Submission and approval by the City Commission, with a recommendation by the Planning Commission, of design guidelines to address development standards as identified in the staff report.

The original intention of the applicant was to seek conventional zoning. This development application includes a PD overlay; therefore, a preliminary development plan is required concurrently with the zoning request. Restrictions may be added to the development plan including approval of a list of uses, lot size, residential density, building setback reductions, building height, and off-street parking may be decreased or increased. Conditional zoning is not proposed with this development application. Appropriate land use controls will be made through the development plan process.

LAND USE

The original condition related to off-street parking for the proposed development remains applicable as a development plan condition. The same rationale for required off-street parking is still appropriate for the proposed development. Staff recommends a general note be added to the face of the drawing that states, "Off-street parking shall be required and approved for all uses per the approved preliminary and final development plans for this development."

The applicant lists the permitted uses on the face of the plan. Uses permitted in the CD District subject to a special use permit would still require a special use permit. Specific uses standards applicable to uses in the CD district listed in Article 5 of the Land Development Code would be applicable to this development, unless otherwise conditioned as part of the development plan.

Specific standards:

20-505	Funeral and Internment	Access arterial road and specific requirements regarding the operation and management of cemeteries.
20-509	Eating and Drinking Establishments	Limitations on accessory bars, bars in CD districts must derive from the sales of food for consumption on the premises not less than 55% of all the licensed premises' gross receipts for a calendar year from sales of food and beverage on such premises. Expansion or alteration of non-conforming uses subject to Article 15 of the Land Development Code.
20-512	Lodge, Fraternal and Civic Assembly	Limitations when located in proximity to an R District.
20-517	Multi-Dwelling Structure, Non-Ground Floor Dwelling	Limits residential uses within the CD district if the units are situated above the ground floor when located on Massachusetts Street and require a special use permit when ground floor residential uses are proposed along numbered streets, Vermont or New Hampshire Streets.
20-522	Religious Assembly	Relates to accessory uses and requirements.
20-526	Retail Establishments	Applicability of a retail market study
20-528	Sexually Oriented Businesses	Location, separation, and display standards.
20-529	Wireless Facilities	New wireless facilities (towers) subject to Special Use Permit and specific design standards.
20-542	Non-ground floor dwelling units	Defines characteristics
20-544	Temporary Shelters.	Specific use standards, management and operation requirements.

Residential Uses

The proposed development includes several vertical mixed-use buildings that provide space for covered parking (garage), office and/or commercial space, and multi-dwelling units. Buildings are labeled on the face of the plan and correspond with phases and uses listed on sheet PDP-103.

Buildings I and II do not include a residential component. These buildings are Johnny's Tavern (Building I) and Gaslight (Building II). Buildings III–VII would include multi-dwelling units. Uses proposed are generally identified by the applicant as, *Multi-Dwelling*, *Office*, *Retail*, and *Hotel*. The preliminary development plan shows 330 units. The total gross density is 20.4 units per acre. Maximum density is not defined in the CD District. It is by definition intended as an intensive mixed-use district. The most intensive residential zoning district is RM32, permitting a maximum 32 dwelling units per acre. The proposed *multi-dwelling* uses may include owner-occupancy as a condominium.

Table 2 Residential Use Summary Table

RESIDENTIAL USE SUMMARY – PRELIMINARY DEVELOPMENT PLAN			
Phase	Building	Residential Units	Total Bedrooms
Phase 1	I	0	0
	II	0	0
	III	24	24
	IV	90	200
Phase 2	V	36	224
Phase 3	VI	36	72
Phase 4	VII	90	240
Phase 5	VIII	90	240
Future Concept	IX	0	0
	X	0	0
	XI	0	0
TOTAL		330 units	809 bedrooms

Non-Residential use:

Buildings I, II and VIII are single use commercial buildings. All other buildings include both residential and non-residential uses. Specific uses or tenants are not listed on the preliminary development plan and are unknown at this time. Determination of specific uses will be determined with the submission of a final development plan for each phase.

Section 20-210 restricts the maximum floor area of any building footprint for a principal use within the CD district shall not exceed 25,000 gross square feet. Buildings shown on the proposed preliminary development plan comply with the maximum size permitted with the exception of the hotel use, phase 5. The plan shows the building footprint of 30,000 square feet. The project must be revised to reduce the total square feet to meet this design standard.

The proposed preliminary development plan provides a phasing summary for buildings I-VII. Buildings IX, X and XI, are included in the plan as a “future concept”. These buildings are located in the northern half of the development.

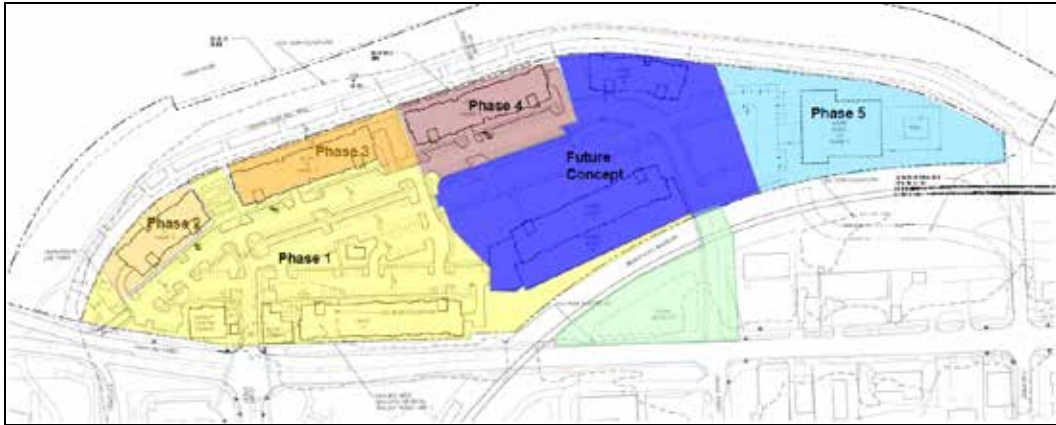


Figure 1: Conceptual Phasing

The preliminary development plan, as proposed, includes vertical mixed-use buildings. Building size and massing are subject to review and approval by the Historic Resources Commission. At this time, buildings III and IV are recommended to be reduced by one story. This change will impact the intensity of development and thus the required off-street parking. The following off-street parking summary is based on the preliminary development plan as submitted by the applicant. The developer should expect changes to the intensity and off-street parking as phases are more fully developed with the submission of final development plans.

Table 3 Land Use Table

USE TALBE SUMMARY – PRELIMINRY DEVELOPMENT PLAN – LAND USES								
Uses/ Phase	Building	Footprint	Gross Square Feet	Eating & Drinking	Patio	Total	Office	Retail
Phase 1	I	4,000	8,000	3,200	100	3,300	1,000	-
	II	900	900	600	300	900	-	-
	III	5,600	22,400	-	-	-	2,000	3,000
	IV	28,000	168,000	1,000	200	1,200	3,000	2,000
Phase 2	V	18,000	108,000	4,000	1,000	5,000	6,000	11,000
Phase 3	VI	25,000	225,000	4,000	1,000	5,000	3,000	15,000
Phase 4	VII	25,000	225,000	4,000	1,000	5,000	3,000	15,000
Phase 5	VIII	30,000	21,000	3,000	800	3,800	-	2,500
Future Concept	IX	25,000	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI
	X	25,000	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI
	XI	25,000	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI	Same as Bldg. VI
Phase 1-5	TOTAL	136,500	778,300	19,800	4,400	24,200	18,000	48,500
All Development	TOTAL	211,500	1,443,300	31,800	7,400	39,200	45,000	93,500

Proposed Off-Street Parking

Specific uses and tenants are unknown at this time. Uses are estimated. The required off-street parking is likely to change when more development information is available. Off-street parking has been reviewed using the following information.

1. Summary of uses and parking shown on the face of the proposed preliminary development plan
2. Use summary from the submitted traffic study
3. Use summary from the sanitary sewer design summary memo

The scope and intensity of uses indicated in the various documents highlight the conceptual nature of the project with unknown specific tenants.

The CD (Downtown Commercial) District is not required to provide off-street parking. Parking is provided in public garages and surface lots within the existing downtown core. The area included in this application is too remote from existing public lots to benefit from the publically available parking to serve the development; therefore, off-street parking must be provided within the development boundary.

The preliminary development plan shows off-street parking within buildings (garages) and surface parking. Parking located in the central and north portion of the lot would be constructed with later building phases of the development.

This application groups non-residential uses into the following categories: *Eating and Drinking*, including patio space; *Office*; *Retail*; and *Hotel*. Within these categories, more specific uses are listed in the Land Development Code with associated off-street parking requirements. The preliminary development plan shows the hotel in Phase 5 (sheet PDP-103).

This parking analysis assumes parking will be provided based on Section 20-902 without a discount for shared parking or for a reduction acknowledging the CD district. The applicant calculates off-street parking only for phases 1-5 (buildings I-VIII). Buildings IX, X, and XI are identified as part of the plan but are not included in the use table summary or the off-street parking summary. Development of the later phases will require additional review.

Bicycle parking and accessible parking spaces are not assessed at this time. These details must be code compliant with each phase of development and will be evaluated with the submission of a final development plan.

Table 4 Off-Street Parking Summary

PARKING SUMMARY -- PHASES I-4 ONLY		
Use	Req. per Sec 20-902	Parking Required
<i>Multi-Dwelling Structure</i>	1 per bedroom + 1 per 10 units (visitors and guests)	330 Dwelling Units (33 quest parking @ 1 per 10 units) 809 Beds (809 spaces @ 1 per bedroom) 842 total residential parking spaces
		520 Dwelling Units per Sanitary Sewer Design Bedrooms not listed in study
		390 Dwelling Units per TIS Bedrooms not listed in study
<i>Office</i>	1 space per 300 square feet	30,000 square feet (phase 1-5) 100 spaces or
		42,000 Square Feet per Sanitary Sewer Design 140 spaces
		30,000 square feet per TIS 100 spaces
<i>Eating and Drinking</i> <i>Accessory Bar, Accessory Restaurant, Bar or Lounge, Brewpub, Fast Order Food, Night Club, Quality Restaurant (See Note 1)</i>	1 space per 100 square feet of customer service area + 1 space per employee based on largest shift or 1 space per 3 persons based on maximum occupancy + 1 per employee based on largest shift	21,200 square feet 21,200/ 100 = 212 spaces Employee shift unknown – not calculated
<i>Retail</i> <i>(Gross Floor Area = 1 – 45,000 square feet)</i>	Variable 1 space per 300 square feet	46,000 square feet 46,000/ 300 = 154 spaces
<i>Total Commercial</i>		21,200 + 46,000 = 67,200 square feet 97,500 square feet per Sanitary Sewer Design 74,300 square feet per TIS
<i>Hotel Building VIII (Phase 5)</i>	1 space per guest room + 1 space per 1.5 employees for associated uses Associated uses are unknown at this time	150 rooms 150 spaces or
		200 rooms per Sanitary Sewer Design 200 spaces
		150 rooms 150 spaces
Total Required Off-Street Parking Phase 1-4 Buildings I-VII		Residential spaces 842 Office spaces 100 Eating and Drinking spaces 212 Retail spaces 154 Hotel spaces 0 1,308 off-street Parking Spaces Required
Notes: 1. Development plan includes eating and drinking establishments but does not distinguish between the two off-street parking standards. The development includes at least one existing <i>Bar or Lounge</i> use that requires parking at 1 space per 3 persons based on maximum occupancy. There is no way to identify the number of employees at maximum shift since specific uses are unknown. Off-street parking for this use can only be estimated at this time.		

The applicant proposes to provide off-street parking per Section 20-909. Section 20-909 allows shared and off-site parking. It is intended to encourage efficient use of land and resources by allowing users to share off-street parking facilities in situations where a mix of uses creates staggered peak periods of parking demand. This application has been submitted using shared parking for the development. Mixed-use developments are an appropriate application of this off-street parking option.

The required parking is determined on a matrix that was adopted by the Planning Office in 2006. This tool has been applied to other commercial developments in the community through the site plan review process. Uses are categorized into seven basic use groups and assigned a percentage of assumed parking demand based on weekday or weekend use, and day, evening, or overnight activity.

Table 5 Shared Parking Matrix

		Weekday			Weekend	
		Night	Day	Evening	Day	Evening
		2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential		100%	60%	90%	80%	90%
Office		5%	100%	10%	10%	5%
Retail		5%	70%	90%	100%	70%
Hotel		80%	55%	100%	50%	100%
Eating & Drinking Establishment		10%	0%	70%	0%	80%
With Drive-Thru						
Open 24 hours		10%	50%	100%	50%	100%
Closed Overnight		5%	50%	100%	50%	100%
Without Drive-Thru						
Open 24 hours		70%	50%	100%	50%	100%
Closed Overnight		0%	50%	100%	50%	100%
Entertainment, Public & Civic, Recreation		10%	40%	40%	70%	100%
All Others		100%	100%	100%	100%	100%

The applicant's off-street parking analysis is attached. The applicant analyzed the parking for building for buildings I – VII (or phase 1-5), a combined parking analysis for a portion of the development. The output of the matrix is that a range of parking is identified based on the time of day and if the day is a weekday or weekend.

It should be noted that the applicant is assuming only one type of eating and drinking establishment and that all retail/commercial uses will exclude entertainment, public, & civic and recreation uses. The identification of specific uses within the buildings and the development will have an impact on the required parking. It should also be noted that the residential use proposed might not be approved based on the outcome of the Historic Resource review. Lastly, the analysis provided by the applicant includes only a portion of the development. The parking analysis does not extend beyond phase 4 (Buildings I-V).

Table 6 Required Off-Street Parking Comparison

REQUIRED OFF-STREET PARKING	Staff Analysis	Applicant Analysis
Residential	842	645 ¹
Office	100	83
Retail	154	100
Hotel (Building VIII excluded 150 spaces)	0	0
Eating and Drinking	212	212
Entertainment, Public & Civic, Recreation	0	0
Other	0	0
Total Without Hotel	1,308	1,040
Total With Hotel	1,458	1,190

Assessment of required off-street parking is complicated by several factors including phasing and unknown uses. This comparison includes only Buildings I-VII. The addition of the *hotel* use (Building VIII) in phase 5 will increase the parking demand in the development.

Table 7 Shared Parking Analysis

Required Parking – Staff Analysis		Weekday			Weekend	
Use Category	Peak Parking Demand	Night	Day	Evening	Day	Evening
Residential	Evening at 674	84	0	589	0	674
Office	Weekday Day at 46	5	100	10	10	5
Retail	Weekend Day at 154	8	108	139	154	108
Eating and Drinking	Weekend Evening at 212	0	106	212	106	212
	Peak Parking Demand at 996	97	314	950	270	998
Required Parking – Applicant Analysis		Weekday			Weekend	
Use Category	Peak Parking Demand	Night	Day	Evening	Day	Evening
Residential	Evening at 516	65	0	452	0	516
Office	Weekday Day at 83	4	83	8	8	4
Retail	Weekend Day at 100	5	70	90	100	70
Eating and Drinking	Weekend Evening at 212	0	106	212	106	212
	Peak Parking Demand at 802	74	259	762	214	802

Provided Parking

The preliminary development plan shows parking provided in the building III, IV, V, VI, and VII. The plan shows 308 surface parking spaces. Future parking includes 95 spaces. The hotel use is intended to be served by surface parking when the phase develops. The full development shows 725 spaces. Off-street parking for buildings I-VII require a minimum 998 off-street parking during the peak demand per staff's analysis. The applicant indicates that 802 parking spaces are needed at the peak demand.

Parking Summary

Required Parking – 809 Residential Spaces (Staff)

¹ Staff confirmed with the applicant that there was an error made in the shared calculation for required residential parking. Staff will provide an updated summary regarding off-street parking at the Planning Commission meeting.

Required Parking – 1,275 Spaces Phase I-4 only (Staff)
Required Parking – 645 Residential Spaces (Applicant)
Required Parking – 1,040 Spaces Phase 1-4 only (applicant)
Required Parking – 998 Shared Spaces Phase 1-4 only (Staff)
Required Parking – 802 Shared Spaces Phase 1-4 only (Applicant)

Proposed Parking 322 garage spaces
 308 surface parking
 630 spaces Initial Development

 95 future spaces
 725 total spaces garage and surface parking

Reduction in approvable residential use will result in a reduction in the required residential parking. The addition of the hotel will result in an additional 150 parking spaces being required based on the preliminary development plan. Off street parking for future buildings IX, X and XI are not provided by the applicant at this time. Based on the building size shown on the preliminary development plan these buildings could be assumed to have a similar requirement to buildings VI and VII. Staff estimates that these buildings would require an additional 1,077 spaces before applying any reductions or shared parking calculations. After applying the shared off-street parking matrix, the total required off-street parking is 2,003 spaces. Staff acknowledges that there are many unknown factors that will influence parking for this development. Regardless, it is staff's opinion that the parking is undercounted in the preliminary development plan. Staff recommends that parking be evaluated as each phase of the project is submitted for a final development review. The development must meet the minimum off-street parking based on the shared parking requirements of Section 20-909. A variance from the off-street parking requirement may be granted by City Commission. This may require that future final development plan applications require City Commission approval prior to final administrative action.

Sensitive Lands and Common Open Space

SENSITIVE LANDS

The subject property is encumbered by regulatory floodplain and is subject to a local floodplain development permit. There is insufficient information to process a flood plain permit at this time. Local floodplain permit will be required for phases that impact the levee and regulatory floodplain. Staff recommends a note be added to face of plan for reference.

COMMON OPEN SPACE

A planned development must provide common open space. Section 20-701(j) of the Land Development Code states that 20% of the site must be located within common open space and, if present, environmentally sensitive lands shall be protected and included within the common open space.

The development site includes 16.116 acres. To meet the required open space standard the site must include 3.22 acres of open space. The site summary indicates that 58% of the total site will be open space.

Table 8 Open Space Summary

Total Site Area 16.12 Acres		
Required Open space	3.22 Ares	140,403 square feet

Total Pervious Area	9.41 Acres	409,781 square feet
On site around Buildings I-VI	1.36 Acres	59,176 square feet
Levee open space	0.91 Acres	39,789 square feet
Future Development Site	7.65 Acres	333,059 square feet

The preliminary development plan relies on the levee and the area around the buildings to accommodate the majority of the required open space. The benefit of the project in proximity to the levee is a direct opportunity to provide and use existing open space. A significant portion of the site will remain undeveloped for the near future. The remaining 7.65 acres exceeds the required open space. As this portion of the development is refined, additional programmed open space will be added to the project inventory.

The Land Development Code also states, "50% of the common open space shall be developed as recreational open space unless environmentally sensitive lands are present, in which case the amount of recreational open space may be reduced to no less than 5% and no more than 10% of the common open space, with the intent being to preserve all or as much environmentally sensitive lands as possible in their natural state." The levee area provides access to an existing recreational open space. The project includes access and enhancement of the levee area. While technically off-site, the space is integral to the overall development. As later phases are developed open, space must be further incorporated into the development.

The CD district does not require a specific portion of open space per dwelling unit, which is common in other zoning districts. Open space in the CD district is typically located within public areas, parks, plazas, and similar civic spaces.

The existing grain elevator is located in the south-central portion of the site. The preliminary development plan shows this building surrounded by a green space with sidewalks. The structure and green space provide a focal point within the development that would otherwise be an unrelieved surface parking lot.



Figure 2: Focal Point - Open Space



Figure 3: Grain Elevator

Other common open space areas are highlighted in the following image.



Figure 4: Open Space

Design

The design of the project as it relates to the building facades, height, and orientation are deferred to the Historic Resources Commission. The CD permits a maximum building height of 90 feet. Building height, design, and building materials must be in context with the listed historic property, the Union Pacific Depot and with the buildings located along the west side of N. 2nd Street that contribute to the significance of the environs of the listed property. A future final development plan would be subject to compliance with the approvals granted by the Historic Resources Commission. Any specific conditions set by the Historic Resources Commission should be reflected as notes on the face of the preliminary development plan. Staff recommends that the preliminary development plan approval be conditioned upon final approval by the Historic Resources Commission with regard to building height and elevations, and noted on the face of the plan.

Density

The proposed preliminary development plan includes 444 units, as shown on the drawing. Residential uses are not currently identified in the future phase (Buildings IX-XI). The summary data indicates the number of units and total bedrooms within buildings but does not indicate how many 1, 2, 3, or more bedroom units are proposed. Off-street parking is based on the total number of bedrooms provided so the configuration of the units is not necessary information for this part of the review. The density as proposed is 27 dwelling units per acre. If the future area were developed in a comparable manner as the buildings in phase 4 and 5, an additional 270 units and 720 bedrooms would be added.

Table 9: Residential Unit Summary

16.11 ACRES	BUILDING	RESIDENTIAL UNITS	TOTAL BEDROOMS
Phase 1	I	0	0
	II	0	0
	III	24	24
	IV	90	200
Phase 2	V	114	224
Phase 3	VI	36	72
Phase 4	VII	90	240
Phase 5	VIII	90	240

Future Concept	IX	0	0
	X	0	0
	XI	0	0
Phases 1-5	Total	444	1,000
All Development	Total	714	1,720
Undeveloped area	Total		7.65 Acres
Density	Phase 1-5		27.5 dwellings per acre
Density	All Phases		44.3 dwelling per acre

Preliminary Development Plan Review

The proposed preliminary development plan has been evaluated based upon findings of fact and conclusions per Section 20-1304(d) (9) of the Land Development Code for the City of Lawrence, requiring consideration of the following nine items:

1) The Preliminary Development Plan's consistency with the Comprehensive Plan of the City.

The project will achieve a gross calculated density of 27.8 dwelling units per acre, which is considered high-density. Recommendations for high-density residential development are found in Chapter 5 of *Horizon 2020*.

While technically outside of the boundaries of the North Lawrence Neighborhood Improvement Association Boundary, the subject property is generally and commonly known as being a part of the neighborhood context. *Horizon 2020* describes the area as follows:

The North Lawrence neighborhood, located northeast of the Kansa River, is one of the least intensely developed residential areas. It includes a number of larger, vacant parcels potentially subject to infill development in the future.

While portion of this area maintain a more intense, or least mixed-density character, the area lying east of 8th Street/road includes larger, vacant sites. This area should be maintained for larger lot-single-family detached development.

Staff finds that the proposed project is consistent with the following Comprehensive Plan policies.

"Development proposals shall be reviewed for compatibility with existing land uses. The review should include use, building type, density, and intensity of use, architectural style, scale, access and its relationship to the neighborhood, and the amount and treatment of screening and open space." (Policy 1.1, page 5-23)

"Medium- and higher-density developments should be arranged in small clusters as transitions from more intensive land uses, or located at the intersection of major street/roads." (Policy 1.3, page 5-23)

"Ensure that medium-and higher-density development occurs in areas which can be adequate and efficiently served by infrastructure" (Policy 1.5), page 5.24)

"Encourage pedestrian use and neighborhood interaction through inclusion of pedestrian easements and sidewalks in subdivision design." (Policy 2.2 (a), page 5.25)

"Encourage the provision of usable open space on site by clustering buildings to minimize the creation of narrow, marginal-use areas in front of and between buildings (Policy 2.4 (a), page 5-25

Many of the policies regarding medium- and higher-density residential development speak to the importance of transitional methods when located near both more intense and less intense land uses, and ensuring compatibility with the surrounding area. The proposed infill redevelopment project is located on the western side of the neighborhood between the river and the Union Pacific Railroad. The subject property is uniquely situated within the community and within the neighborhood. There are no nearby residential uses to extrapolate a development pattern. The immediate land uses to the east are commercial. The proposed mixed-use development includes residential, commercial, and recreational uses that would enhance the riverfront. If approved, the development would be unique within the community.

Two residential policies are not specifically addressed in the proposed application regarding the recommendation to provide interspersed low- to moderate-income housing and to encourage a variety of housing types within neighborhood. The proposed development includes only one type of housing – *multi-dwelling residential* uses. In fact, forms of detached dwellings such as *attached, cluster dwellings, detached dwellings, duplex, and, zero lot line dwellings* are not permitted in the CD District.

Staff Finding – In staff's opinion, the proposed development complies with the land use goals and policies for medium- and higher-density residential development of the comprehensive plan.

2) Preliminary Development Plan's consistency with the Planned Development Standards of Section 20-701 including the statement of purpose.

a) Ensure development that is consistent with the comprehensive plan.

As discussed previously, staff finds that the development is consistent with the comprehensive plan.

b) Ensure that development can be conveniently, efficiently, and economically served by existing and planned utilities and services.

Existing water, sanitary sewer, and stormwater infrastructure with appropriate improvements subject to approval of public improvement plans can conveniently serve the subject property.

c) Allow design flexibility, which results in greater public benefits than could be achieved using conventional zoning district regulations.

The Planned Development was recommended by staff as a tool to ensure that the property developed in a manner that maintains public access to existing community recreational assets, and respects the historic context of the Union Pacific Depot, its contributing structures within the project boundary. The project benefits from this tool by accommodating common open space requirements off-site via the levee space.

d) Preserve environmental and historic resources.

The project includes regulatory floodplain that is subject to further review regardless of the zoning district. By conditioning the successive applications to comply with historic design review criteria as discussed in the body of the staff report, these resources are protected.

e) Promote attractive and functional residential, nonresidential, and mixed-use developments that are compatible with the character of the surrounding area.

The proposed development is unique to the community as a mixed-use development that features and enhances the community's connection to the river. The development is somewhat isolated from the larger neighborhood because of the physical barrier of the street and railroad facilities.

Staff Finding – The proposed preliminary development is consistent with the comprehensive plan and appropriately protects natural and historic resources, as conditioned. This proposed preliminary development plan is consistent with the Statement of Purpose of Planned Development.

3) The nature and extent of the common open space in the Planned Development.

Section 20-701(j) notes that 20% of the site must be located within common open space. This criterion was discussed earlier in the report. The open space requirement is met for phases 1-5 because the north portion of the site is undeveloped. A significant open space feature of this project is the levee and planned enhancements. The project will include levee side improvements such as patios and common areas that will connect and interface with the levee and recreation trail. While the levee is "off-site" its proximity to the project makes it a focal point of the development.

Staff Finding – The plan complies with the common open space standards within the development for phases 1-5. Additional review of open space must be incorporated into a review of the final development plan application.

4) The reliability of the proposals for maintenance and conservation of the common open space.

As discussed, common open space will continue to be evaluated throughout the entitlement process. The levee, as the key component of the common open space, is owned and maintained by the City of Lawrence. The developer will own common open spaces within the development. The project does not include any tracts of land specifically designated for open space, common or otherwise, at this time.

Staff Finding –Common open space will be privately owned and maintained by the owner/developer within the private property boundary of the project.

5) The adequacy or inadequacy of the amount and function of the common open space in terms of the densities and dwelling types proposed in the plan.

The Land Development Code does not require any outdoor area for residential uses in the CD district. Lots in the district may be fully developed with no building setback, interior, side, front or rear yards, or other open spaces. These uses are typically accounted for in public and civic areas.

Practically, the project does provide open space at the ground level and along the levee as described in the body of this report.

Staff Finding – The project does not require open space specific to residential uses within the development.

6) Whether the Preliminary Development Plan makes adequate provisions for public services, provides adequate control over vehicular traffic, and furthers the amenities of light and air, recreation and visual enjoyment.

Provisions for Fire/Medical access are provided with the preliminary development plan. The plan has also made adequate provisions for public services and provides adequate control over vehicular traffic. Some development is deferred until such time that a second full access can be provided. This development includes enhancement to the N. 2nd Street and to the river frontage. The development is subject to the submission, review, and approval of public improvement plans.

Staff Finding – Infrastructure and system capacity are conceptually available to the development area. These elements will be refined as part of the preliminary plat, final plat, and public improvement plan review and entitlements.

7) Whether the plan will measurably and adversely impact development or conservation of the neighborhood area by:

a) doubling or more the traffic generated by the neighborhood;

A traffic study was provided to City Staff for review (attached). The traffic study indicated that the street system could handle the proposed development, with the addition of certain street improvements. The street improvements recommended by the traffic study include:

- Provision of dedicated eastbound left-turn lane on Locust Street.
- Provision of a shared through/right-turn lane on Locust Street.
- Increasing the northbound left-turn lane on N. 2nd Street to the greatest extent possible.
- Modifying the signal timing.
- Adding a restricted right-out access drive near the south property line.
- Provision of additional access drives to N. 2nd street with the future phases.

Any street improvement implemented to accommodate the proposed development would be done so at the cost of the developer.

b) proposing housing types, building heights or building massings that are incompatible with the established neighborhood pattern; or

The North Lawrence neighborhood does not include any multi-story multi-dwelling residential uses. This housing form would be a new addition within the neighborhood. There are no nearby residential uses that set a neighborhood pattern for the area west of N. 2nd Street.

c) increasing the residential density 34% or more above the density of adjacent residential properties.

The proposed project is not adjacent to any residential properties. It does substantially increase the residential density within the larger neighborhood context by the nature and location of the project proposal.

Staff Finding – The proposed development is unique in its location within the North Lawrence Neighborhood and its proximity to the Kansas River. The proposed development represents a significant investment to the community in an area that is not ubiquitously accessible. The surrounding neighborhood character is largely isolated from the proposed development by the physical barrier of the railroad and N. 2nd Street. Staff has determined that the plan will not measurably or adversely impact the surrounding neighborhood.

8) Whether potential adverse impacts have been mitigated to the maximum practical extent.

Potential adverse impacts with a mixed-use development of this size can occur with lighting that extends onto adjacent properties, potential for increased noise levels, and traffic congestion along N. 2nd Street.

The most serious obstacle to full buildout will be the provision of a secondary access to the north. Existing buildings at the intersection of Locust and N. 2nd Street will remain and generally be expanded along the street frontage providing a “face” to the public street, along with applicable public improvements such as sidewalks, street trees, and green space. Parking for the development is located behind the commercial buildings that front N. 2nd Street. Buildings are similarly oriented to the river with parking in the rear, or the center section of the development.

Intersection improvements are required and will be subject to additional review as public improvement plans are prepared.

The phasing of the development with the single access at Locust Street will result in situations where full occupancy of buildings have been provided while others will be under construction. The mixing of construction traffic with daily traffic is known to be difficult to manage. The applicant is exploring opportunities with the Union Pacific Railroad to address this problem.

A photometric plan will be required prior to approval of the final development plan to ensure there are no negative impacts from the exterior lighting. Exterior lights shall have full cut-off features and will be further reviewed as part of the final development plan.

Section 20-701(h) provides regulation for balconies on the exterior sides of multi-dwelling building that are adjacent to the RS zoning or detached dwellings. In those situations, balconies are not permitted unless the building setback is increased to at least double the required minimum setback and landscaping is enhanced. This provision is only applicable to the exterior sides of the Planned Development that are adjacent to RS zoning or to detached dwelling units. The proposed development is not adjacent to any R zoning district.

Staff Finding – Any possible adverse impacts of exterior lighting will be addressed with a photometric plan to ensure there is no spillover light. Other potential adverse impacts of the proposed development will be mitigated through building design and the review of a final development plan.

9) The sufficiency of the terms and conditions proposed to protect the interest of the public and the residents of the Planned Unit Development in the case of a plan that proposes development over a period of years.

The proposed development includes multiple phases. Phasing is critical to the full development of the site, as a second point of access is required to meet fire prevention standards and accommodate vehicular access within the development. Phasing is also critical to the transfer of ownership of publicly owned land to the total developer. This preliminary development plan provides a detailed concept of the planned site redevelopment. Additional entitlements are required prior to development of the site.

The sanitary sewer design memo and the drainage study both references different phasing schedule than shown on the preliminary development plan. The phase included in these studies reflect the necessary public improvements that are required to support the overall development. These differing phasing schedules are not necessarily inconsistent.

Additional review of the project with future final development plan applications will ensure that the project will be properly managed.

- Undeveloped phases will need to be maintained and not permitted to become overgrown or exterior storage yards, expect as part of specific staging area for active construction projects.
- Adequate public facilities must be constructed during the initial phase. This aspect will be fully determined with the approval of a preliminary plat and submission of both a final plat and corresponding public improvement plans.

The development plan outlines a phasing schedule that is highly likely to be altered over time. The order of construction of buildings may or may not follow exactly as shown on the preliminary development plan. Regardless, as a final development plan is submitted, reasonable concurrence with the phasing must be met or the plan must be revised. Inadequate parking or access will be cause to deny a project in the future.

Staff Finding – A phased development is proposed. Phasing of development is dependent upon adequate access being available and construction of public infrastructure as controlling factors.

Staff Review and Conclusion

Review of the project is complicated by two unknown factors; final approval by the Historic Resources Commission establishing the maximum building height and thus some intensity, and detailed use information regarding off-street parking. The scope of the project challenges a reviewer to balance conceptual development with minimum code required compliance. This preliminary development plan should be considered as a detailed master plan for the redevelopment of the 16 acres. Phasing must be evaluated further with the public improvement plans and a final development plan for specific building applications.

While the CD district does not require off-street parking, the remote location does not provide an opportunity to benefit from publicly owned parking lots that could support the development. Without adequate parking, the project will be less successful than desired by the applicant and the community. Staff supports the proposed redevelopment but reserves the right for additional review of off-street parking in the total context of a final development plan as discussed in the body of the staff report.

The project represents a significant reinvestment in the area, and an opportunity to expand and enhance a community connection to the riverfront.

paulwerner
ARCHITECTS

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SUITE B2
LAWRENCE, KS 66044

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NOT TO SCALE



- 1.1 CURRENT ZONING: OS, CS, IG (CD ZONING WAS CONDITIONALLY APPROVED BY CITY COMMISSION)

1.2 PROPOSED ZONING: CD-PD

1.3 CURRENT USE: VACANT, BAR OR LOUNGE, MOBILE HOME, GENERAL RETAIL

1.4 ALLOWED USES:
P= PERMITTED,
S = SPECIAL USE,
A = ACCESSORY,
* = STANDARDS APPLY

HOUSEHOLD LIVING
MULTI-DWELLING STRUCTURE (P* S)
NON-GROUND FLOOR DWELLING (P*)
WORK/LIVE UNIT (P* S)
GROUP HOME, GENERAL (11 OR MORE) (S)
COLLEGE/UNIVERSITY
CULTURAL CENTER/LIBRARY
DAY CARE CENTER (S*)
DAY CARE HOME, CLASS A
DAY CARE HOME, CLASS B
EVENT CENTER, SMALL
EVENT CENTER, LARGE
LODGE, FRATERNAL & CIVIC ASSEMBLY
POSTAL, #PARCEL SERVICE
PUBLIC SAFETY
SCHOOL

FINANCIAL AND INTERMENT (*)
 TEMPORARY SHELTER (S*, A*)
 SOCIAL SERVICE AGENCY
 COMMUNITY MEAL PROGRAM (S, A*)
 UTILITIES, MINOR (P*, S*)
 UTILITIES AND SERVICE, MAJOR
 HEALTH CARE OFFICE, HEALTH CARE CLINIC
 OUTPATIENT CARE FACILITY (P*)
 ACTIVE RECREATION (S)
 SPECTATOR SPORTS, GENERAL
 ENTERTAINMENT & SPECTATOR SPORTS, LIMITED
 PARTICIPANT SPORTS & RECREATION, INDOOR
 PARTICIPANT SPORTS & RECREATION, INDOOR
 PASSIVE RECREATION
 NATURE PRESERVE/UNDEVELOPED
 PRIVATE RECREATION
 CAMPUS OR COMMUNITY INSTITUTION (P*)
 NEIGHBORHOOD INSTITUTION (P*)
 SALES AND GROOMING
 VETERINARY
 ACCESSORY BAR (A*)
 BAR OR LOUNGE (P*)

- REPHRASE (P*)
- FAST ORDER FOOD (P*)
- NIGHTCLUB (P*)
- PRIVATE DINING ESTABLISHMENTS (P*)
- RESTAURANT, QUALITY (P*)
- ADMINISTRATIVE AND PROFESSIONAL OFFICE
- FINANCIAL, INSURANCE & REAL ESTATE OFFICE
- PAYMENT ADVANCE, CAR TITLE LOAN BUSINESS
- OFFICE, OTHER
- ACCESSORY PARKING FACILITIES (A*)
- COMMERCIAL PARKING FACILITIES (S)
- BUILDING MAINTENANCE
- BUSINESS EQUIPMENT
- BUSINESS SUPPORT
- FOOD AND BEVERAGE (P*)
- MIXED MEDIA STORE (P*)
- PERSONAL CONVENIENCE SERVICES
- PERSON IMPROVEMENT SERVICES
- REPAIR SERVICE, CONSUMER (P*)
- RETAIL SALES, GENERAL (P*)
- RETAIL ESTABLISHMENT, MEDIUM (P*)

- RETAIL ESTABLISHMENT, SPECIALTY (*)
- HOTEL, MOTEL, EXTENDED STAY
- LIGHT EQUIPMENT REPAIR (S)
- LIGHT EQUIPMENTS SALES/RENTAL (S)
- MAKER SPACE, LIMITED
- MAKER SPACE, INTENSIVE
- MANUFACTURING AND PRODUCTION, LIMITED (S)
- MANUFACTURING AND PRODUCTION, TECH (S)
- RESEARCH SERVICE (S)
- DESIGNATED HISTORIC PROPERTY (S*)
- AMATEUR AND RECEIVER-ONLY ANTENNAS (A*)
- BROADCASTING TOWER (S)
- COMMUNICATIONS SERVICE ESTABLISHMENT
- TELECOMMUNICATIONS ANTENNA (A*)
- TELECOMMUNICATIONS TOWER (S*)
- SATELLITE DISH (A*)
- SMALL COLLECTION RECYCLING FACILITIES

2.1 ALL GROUND MOUNTED MECHANICAL UNITS SHALL BE SCREENED IN ACCORDANCE WITH SECTION 20-1006(b).

2.2 ALL ACCESSIBLE SIDEWALK RAMPS BY A.D.A. STANDARDS.

2.3 SITE PLAN HAS BEEN DESIGNED TO COMPLY WITH THE PROVISIONS OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) FOR BUILDINGS AND FACILITIES, APPENDIX A TO 28CFR PART 36.

2.4 SITE PLAN HAS BEEN DESIGNED TO COMPLY WITH THE MINIMUM PROVISIONS OF THE FINAL FAIR HOUSING ACCESSIBILITY GUIDELINES, 24 CFR, CHAPTER I, SUBCHAPTER A, APPENDIX II, OF THE FAIR HOUSING ACT OF 1968, AS AMENDED.

2.5 THE CITY OF LAWRENCE WILL NOT BE HELD RESPONSIBLE FOR DAMAGE CAUSED BY TRASH TRUCKS.

2.6 PLAN FOR CITY APPROVAL ONLY! CONSTRUCTION DOCUMENTS TO BE FURNISHED AT THE REQUEST OF OWNER.

2.7 INFORMATION TAKEN FROM AERIAL PHOTOS, AVAILABLE DOCUMENTS AND ON SITE INVESTIGATIONS.

2.9 ALL TRAFFIC CONTROL SIGNS PLACED ON PRIVATE PROPERTY OPEN TO THE GENERAL PUBLIC SHALL COMPLY WITH THE "MANUAL ON UNIFORMS TRAFFIC CONTROL DEVICES" AND "STANDARD HIGHWAY SIGNS, PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION, WITH RESPECT TO SIZE, SHAPE, COLOR, RETROFLECTIVITY, AND POSITION.

2.10 ENTIRE COMPLEX TO BE CONSTRUCTED IN PHASES. OCCUPANCY PERMITS TO BE ISSUED PER BUILDING WHEN NECESSARY IMPROVEMENTS HAVE BEEN COMPLETED FOR EACH STRUCTURE.

2.11 ANY PROPOSED EASEMENTS SHALL BE DEDICATED WITH THE FINAL PLAT AND/OR BY SEPARATE INSTRUMENT PRIOR TO THEIR CONSTRUCTION

2.12 WE HEREBY DEDICATE TO THE CITY OF LAWRENCE THE RIGHT TO REGULATE ANY CONSTRUCTION OVER THE AREA DESIGNATED AS COMMON OPEN SPACE, OPEN AIR RECREATION AREA, AND NON-ENCROACHABLE AREA AND TO PROHIBIT ANY CONSTRUCTION WITHIN SAID AREAS AND SPACES INCONSISTENT WITH THE APPROVED USE OR ENJOYMENT OF RESIDENTS, LESSEES AND OWNERS OF THE PLANNED UNIT DEVELOPMENT.

2.13 PER CITY CODE SECTION 4-902, THE PATIO AREA WILL BE MANAGED TO PREVENT STORMWATER POLLUTION. FOOD WASTE, TRASH, CIGARETTES AND OTHER SOLID WASTES WILL BE COLLECTED AND DISPOSED OF PROPERLY. COLLECTION MUST BE FREQUENT ENOUGH TO PREVENT WASTES CARRIED OFF BY WIND OR STORMWATER RUNOFF. PAVEMENT AND FURNISHINGS MUST BE CLEANED FREQUENTLY ENOUGH TO PREVENT CONTAMINATION OF STORMWATER RUNOFF. FLUID WASTE, INCLUDING WASTEWATER FROM PAVEMENT OR FURNITURE CLEANING, WILL BE COLLECTED AND DISCHARGED TO THE SANITARY SEWER SYSTEM.

2.14 TYPICAL DIMENSIONS: REGULAR SPACES - 9' X 18'
ADA SPACES - 9' X 18' (9' AISLE)

2.14 PAVEMENT: APPROACHES:
PARKING AND DRIVES:
CURB AND GUTTER:
SIDEWALKS:

7" - 4000 PSI CONCRETE W/ #5 BARS 12" O.C.B.W.
MIN. 6" ASPHALT ON 4" GRAVEL OR 5" CONCRETE MIN. 5" ASPHALT ON 4" GRAVEL OR 4" CONCRETE
TYPE I CURB AND GUTTER THROUGHOUT SITE UNLESS SPECIFIED DIFFERENTLY.
4" CONCRETE WITH COMPACTED SUBGRADE UNLESS OTHERWISE NOTED

A TRACT OF LAND LOCATED IN NORTH LAWRENCE AND ADDITIONS, SUBDIVISIONS IN LAWRENCE, DOUGLAS COUNTY, KANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF THE NORTHEAST QUARTER OF SECTION 30, TOWNSHIP 12 SOUTH, RANGE 20 EAST OF THE 6TH PRINCIPAL MERIDIAN, THENCE SOUTH 21°35'25" EAST, ALONG THE EAST LINE OF SAID NORTHEAST QUARTER, 2644.36 FEET TO THE SOUTHEAST CORNER OF SAID NORTHEAST QUARTER, THENCE S88°12'41"N, 10.00 FEET; THENCE N01°47'13"E, 95.46 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD, THENCE SAID RAILROAD RIGHT-OF-WAY LINE ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 1402.45 FEET, AN ARC LENGTH OF 146.76 FEET, AND A CHORD BEARING OF N89°52'47"E, 147.02 FEET; THENCE S10°12'14"E, 63.61 FEET; THENCE S89°52'47"E, 147.02 FEET TO THE RIGHT HAVING A RADIUS OF 1402.45 FEET, AN ARC LENGTH OF 146.76 FEET, AND A CHORD BEARING OF N89°52'47"E, 147.02 FEET; THENCE ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 2750.00 FEET, AND ARC LENGTH OF 236.78 FEET, AND A CHORD BEARING OF N02°55'23"E, 236.63 FEET TO THE INTERSECTION OF THE WEST RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD, 100 FEET WIDE WITH THE SOUTH RIGHT-OF-WAY LINE OF LYON STREET, 60 FEET WIDE; THENCE ALONG SAID SOUTH RIGHT-OF-WAY LINE S68°21'42"E, 68.51 FEET TO THE EAST RIGHT-OF-WAY LINE OF THE PROPOSED LEVEE TRACT; THENCE SOUTH 21°24'55" WEST, 83.11 FEET; THENCE ALONG A CURVE TO THE LEFT, HAVING A RADIUS OF 100.00 FEET, WITH A CHORD BEARING OF SOUTH 16°50'11" WEST AND A CHORD LENGTH OF 258.41 FEET; FOR A DISTANCE OF 258.44 FEET; THENCE SOUTH 61°54'43" WEST, 512.73 FEET; THENCE ALONG A CURVE TO THE LEFT, HAVING A RADIUS OF 150.00 FEET, WITH A CHORD BEARING OF SOUTH 12°50'11" WEST AND A CHORD LENGTH OF 147.17 FEET; FOR A DISTANCE OF 208.10 FEET; THENCE SOUTH 21°24'01" EAST, 45.04 FEET; THENCE SOUTH 11°26'58" EAST, 214.95 FEET; THENCE ALONG A CURVE TO THE LEFT, HAVING A RADIUS OF 5535.00 FEET, WITH A CHORD BEARING OF SOUTH 25°53'02" EAST AND A CHORD DISTANCE OF 124.05 FEET, FOR A DISTANCE OF 124.32 FEET; THENCE SOUTH 32°19'05" EAST, 28.61 FEET; THENCE SOUTH 45°54'23" EAST, 75.24 FEET; THENCE ALONG A CURVE TO THE LEFT, HAVING A RADIUS OF 35.00 FEET, WITH A CHORD BEARING OF SOUTH 53°59'45" EAST AND A CHORD LENGTH OF 102.54 FEET, FOR A DISTANCE OF 102.41 FEET; THENCE SOUTH 62°25'08" EAST, 75.94 FEET; THENCE SOUTH 33°04'11" EAST, 63.06 FEET TO THE WEST RIGHT-OF-WAY LINE OF NORTH 2ND STREET, VARIABLE WIDTH; THENCE ALONG SAID WEST RIGHT-OF-WAY LINE, ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 1854.20 FEET, AN ARC LENGTH OF 120.75 FEET, AND A CHORD BEARING OF N06°26'18"E, 106.51 FEET; THENCE ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 1670.50 FEET, AN ARC LENGTH OF 106.34 FEET, AND A CHORD BEARING OF N06°26'18"E, 106.51 FEET; THENCE N03°29'13"E, 50.03 FEET; THENCE N03°29'13"E, 282.34 FEET; THENCE N01°08'04"E, 27.41 FEET TO THE POINT OF BEGINNING AND CONTAINING 102.03 SQUARE FEET OR 1616 ACRES MORE OR LESS.

PDP-100 COVER AND NOTES

PDP-101 NOTES
PDP-102 DEMO PLAN
PDP-103 OVERALL LAYOUT
PDP-104 LAYOUT
PDP-105 LAYOUT
PDP-106 LAYOUT
PDP-107 UTILITIES/GRADING
PDP-108 UTILITIES/GRADING
PDP-109 UTILITIES/GRADING

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NORTH LAWRENCE RIVERFRONT PRELIMINARY DEVELOPMENT PLAN

PROJECT # 217-350
AUGUST 2018

RELEASE:	DATE:
1.0	10.15.18
1.1	12.5.18

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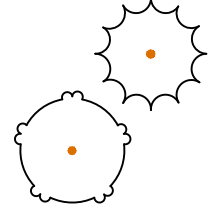
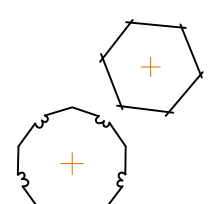

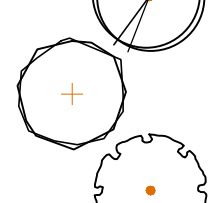
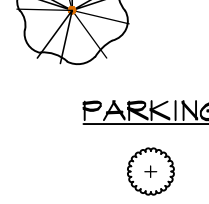
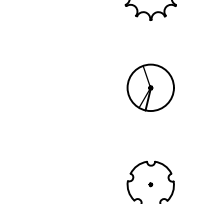










PARKING:

- 3.1 REFER TO SUPPLEMENT PARKING DOCUMENT.
- 3.2 THE DEVELOPMENT USES A SHARED PARKING TABLE ATTACHED TO THE PARKING DOCUMENTS.
- 3.3 PARKING TO BE EVALUATED BETWEEN OWNER AND CITY PLANNING OFFICE PRIOR TO ISSUANCE OF BUILDING PERMIT FOR BUILDING VII TO DETERMINE IF APPROPRIATE PARKING IS AVAILABLE.

FIRE/MEDICAL ACCESS:

- 4.1 SINCE THE SECOND ENTRANCE (EMERGENCY ENTRANCE AND RIGHT-OUT SOUTHBOUND EXIT AS DEPICTED ON THE PRELIMINARY PLAT), IS CLOSER TO THE MAIN ENTRANCE THAN IS PERMITTED BY THE 2015 IFC, THE OWNERS OF THE GROUND HAVE AGREED TO THE FOLLOWING:
- A) THE FOOTPRINT OF ALL HABITABLE STRUCTURES SHALL NOT EXCEED 124,000 SF, EXCLUDING BUILDINGS LABELED I, II, III, & IV.
- B) ALL STRUCTURES SHALL BE PROTECTED BY AN AUTOMATIC SPRINKLER SYSTEM MEETING NFPA 13, EXCLUDING BUILDING II.
- C) THERE SHALL BE NO OTHER LIMITS ON ALLOWED OCCUPANCIES OTHER THAN THE 124,000 SF FOOTPRINT.
- D) STAND ALONE PARKING STRUCTURES SHALL NOT COUNT TOWARDS THE 124,000 SF LIMIT.
- E) THE ENTIRE AREA OF THIS PRELIMINARY PLAT IS AVAILABLE FOR DEVELOPMENT.
- 4.2 IF AN ALTERNATIVE ACCESS POINT BECOMES AVAILABLE, THESE STIPULATIONS MAY BE AMENDED OR ELIMINATED BASED ON THE 2015 IFC, OR CURRENT ADOPTED FIRE CODE.

LANDSCAPING LEGEND:

EXISTING TREES TO REMAIN	QTY	COMMON NAME / BOTANICAL NAME	SIZE	CONTAINER
	3	SCOTCH PINE / PINUS SYLVESTRIS	---	
	4	WHITE OAK / QUERCUS ALBA	---	
PARKING LOT TREES	QTY	COMMON NAME / BOTANICAL NAME	SIZE	CONTAINER
	5	OKLAHOMA REDBUD / CERCIS CANADENSIS 'OKLAHOMA'	1.75" CAL.	POT
	5	SUNCOLE HONEYLOCUST / GLEDITSIA TRIACANTHOS 'SUNCOLE' TM	2.5" CAL.	POT
	4	AMERICAN LINDEN / TILIA AMERICANA 'SENTRY'	2.5" CAL.	POT
	12	SAWLEAF ZELKOVA / ZELKOVA SERRATA 'VILLAGE GREEN'	2.5" CAL.	POT
STREET TREES	QTY	COMMON NAME / BOTANICAL NAME	SIZE	CONTAINER
	4	PACIFIC SUNSET MAPLE / ACER TRUNCATUM 'PACIFIC SUNSET' TM	2.5" CAL.	POT
	3	MAIDENHAIR TREE / GINKGO BILOBA 'AUTUMN GOLD' TM	2.5" CAL.	POT
	3	SWAMP WHITE OAK / QUERCUS BICOLOR	2.5" CAL.	POT
	3	FRONTIER ELM / ULMUS CARPINIFOLIA X PARVIFOLIA 'FRONTIER'	2.5" CAL.	POT
PARKING LOT SHRUBS	QTY	COMMON NAME / BOTANICAL NAME	SIZE	CONTAINER
	56	GLENCOE BOXWOOD / BUXUS X 'CHICAGOLAND GREEN' TM	2 GAL.	POT
	47	SARGENT JUNIPER / JUNIPERUS CHINENSIS SARGENTII	5 GAL.	POT
	3	SUMMER WINE NINEBARK / PHYSOCARPUS OPULIFOLIUS 'SUMMER WINE'	2 GAL.	POT
	26	BIRCHLEAF SPIREA / SPIRAEA BETULIFOLIA 'TOR'	2 GAL.	POT
	3	DWARF KOREAN LILAC / SYRINGA MEYERI 'PALIBIN'	2 GAL.	POT
SHRUB AREAS	QTY	COMMON NAME / BOTANICAL NAME	SIZE	CONTAINER
	445	DWARF FOUNTAIN GRASS / PENNISETUM ALOPECUROIDES 'HAMELN'	1 GAL.	POT

LANDSCAPING NOTES:

- 5.1 ALL TURF AREA TO BE SEEDED WITH DROUGHT TOLERANT GRASS.
- 5.2 CARE SHALL BE TAKEN TO PRESERVE ALL TREES OUTSIDE OF THE IMMEDIATE CONSTRUCTION AREA.
- 5.3 LANDSCAPING SHALL BE IRRIGATED. OWNER SHALL COORDINATE WITH CITY UTILITIES DEPT. IF THEY DESIRE TO METER IT SEPARATELY.
- 5.4 REQUIRED STREET TREES (1 PER 40 L.F. OF STREET FRONTAGE)
- 5.5 REQUIRED INTERIOR PARKING LOT LANDSCAPING (40 SQ.FT. OF LANDSCAPED AREA PER PARKING SPACE & 1 SHADE TREE AND 3 SHRUBS PER 10 PARKING SPACES)
- 5.6 REQUIRED PARKING LOT PERIMETER LANDSCAPING (1 SHADE TREE PER 25 LINEAR FEET OF PARKING LOT FRONTAGE) AND CONTINUOUS ROW OF EVERGREEN SHRUBS

LANDSCAPING CALCULATIONS:

PARKING STALLS PROVIDED	248
INTERIOR PARKING LOT GREENSPACE PROVIDED (SF)	24,944
INTERIOR PARKING LOT GREENSPACE REQUIRED (SF)	11,920
INTERIOR PARKING LOT SHRUBS PROVIDED	135
INTERIOR PARKING LOT SHRUBS REQUIRED	90
INTERIOR PARKING LOT TREES PROVIDED	31
INTERIOR PARKING LOT TREES REQUIRED	30
PERIMETER PARKING LOT FRONTAGE (FT)	96
PERIMETER PARKING LOT TREES PROVIDED	4
PERIMETER PARKING LOT TREES REQUIRED	4
NORTH 2ND STREET TREES PROVIDED	25
NORTH 2ND STREET TREES REQUIRED	958 L.F. / 40 =24
COMMON OPEN SPACE PROVIDED ON SITE (SF)	54,176 AROUND BLDG. I-VII
COMMON OPEN SPACE PROVIDED ON LEVEE (SF)	39,189 AROUND BLDG. I-VII
COMMON OPEN SPACE PROVIDED ON UNDEVELOPED AREA OF SITE (SF)	333,059 SF.
COMMON OPEN SPACE REQUIRED (SF) (20% OF SITE AREA)	140,403 SF.

IMPERVIOUS SURFACE SUMMARY:

PROJECT SITE 102,013 S.F. (16.116 ACRES):			
EXISTING CONDITIONS:		PROPOSED CONDITIONS:	
LAND AREA:	<u>50 FT</u> 102,013	LAND AREA:	<u>50 FT</u> 102,013
BUILDING FOOTPRINT:	36,064 5%	BUILDING FOOTPRINT:	45,850 14%
PAVEMENT AREAS:	3,421 1%	PAVEMENT AREAS:	146,382 28%
TOTAL IMPERVIOUS:	39,485 6%	TOTAL IMPERVIOUS:	242,232 42%
TOTAL PERVIOUS:	66,2523 94%	TOTAL PERVIOUS:	404,181 58%

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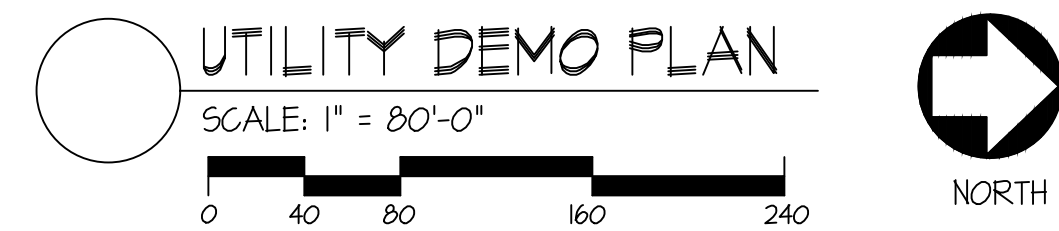
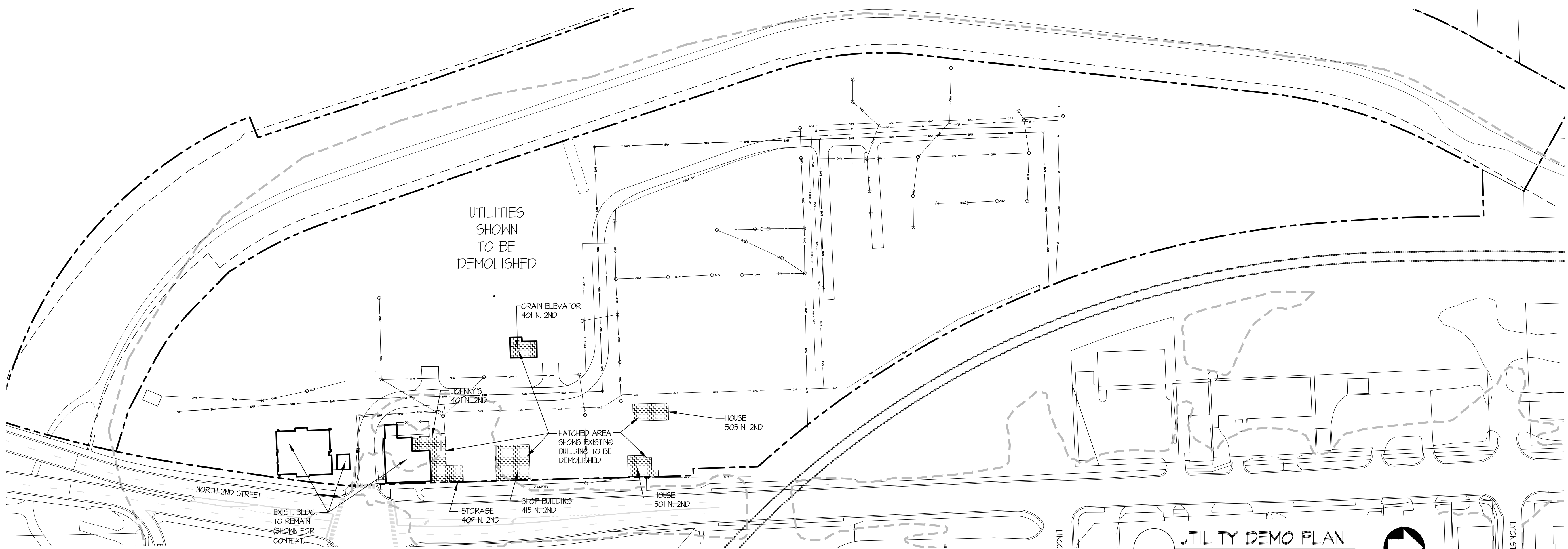
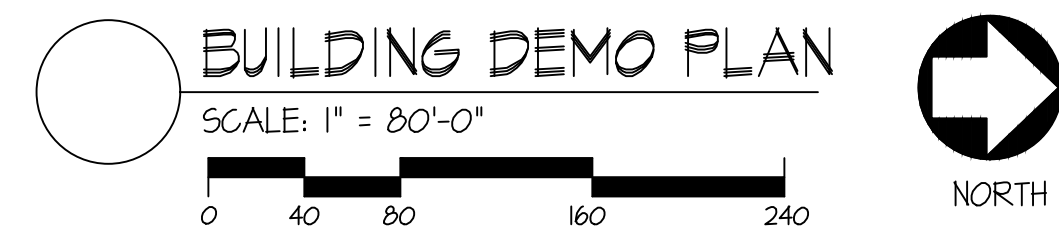
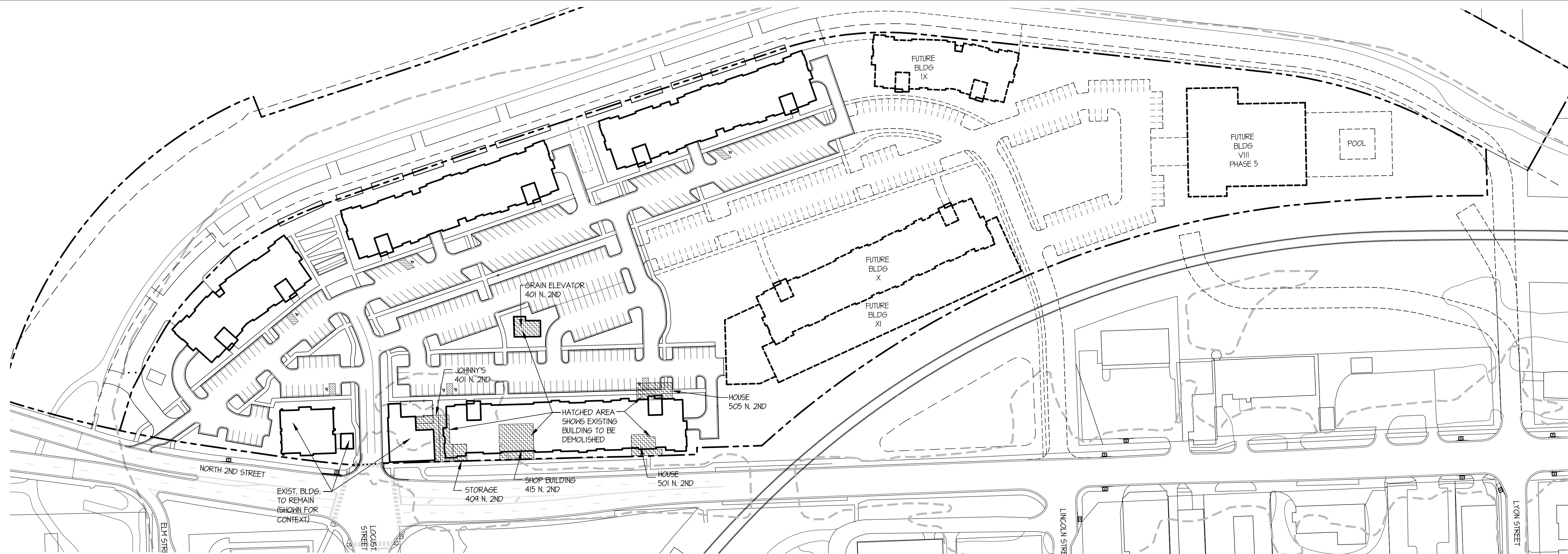
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PROJECT # 217-350
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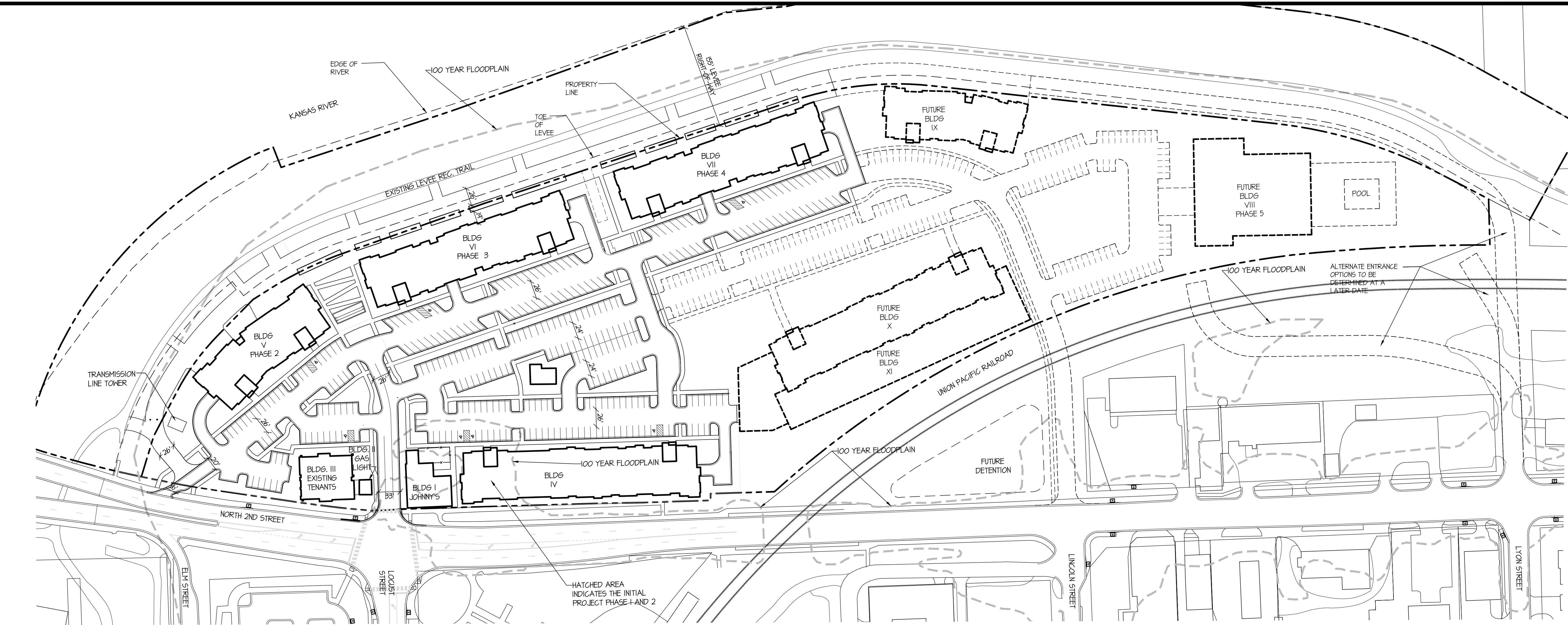
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PROJECT # 217-350
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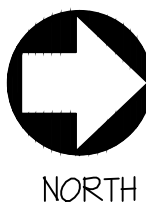
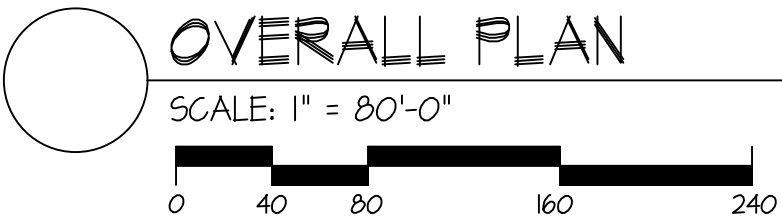
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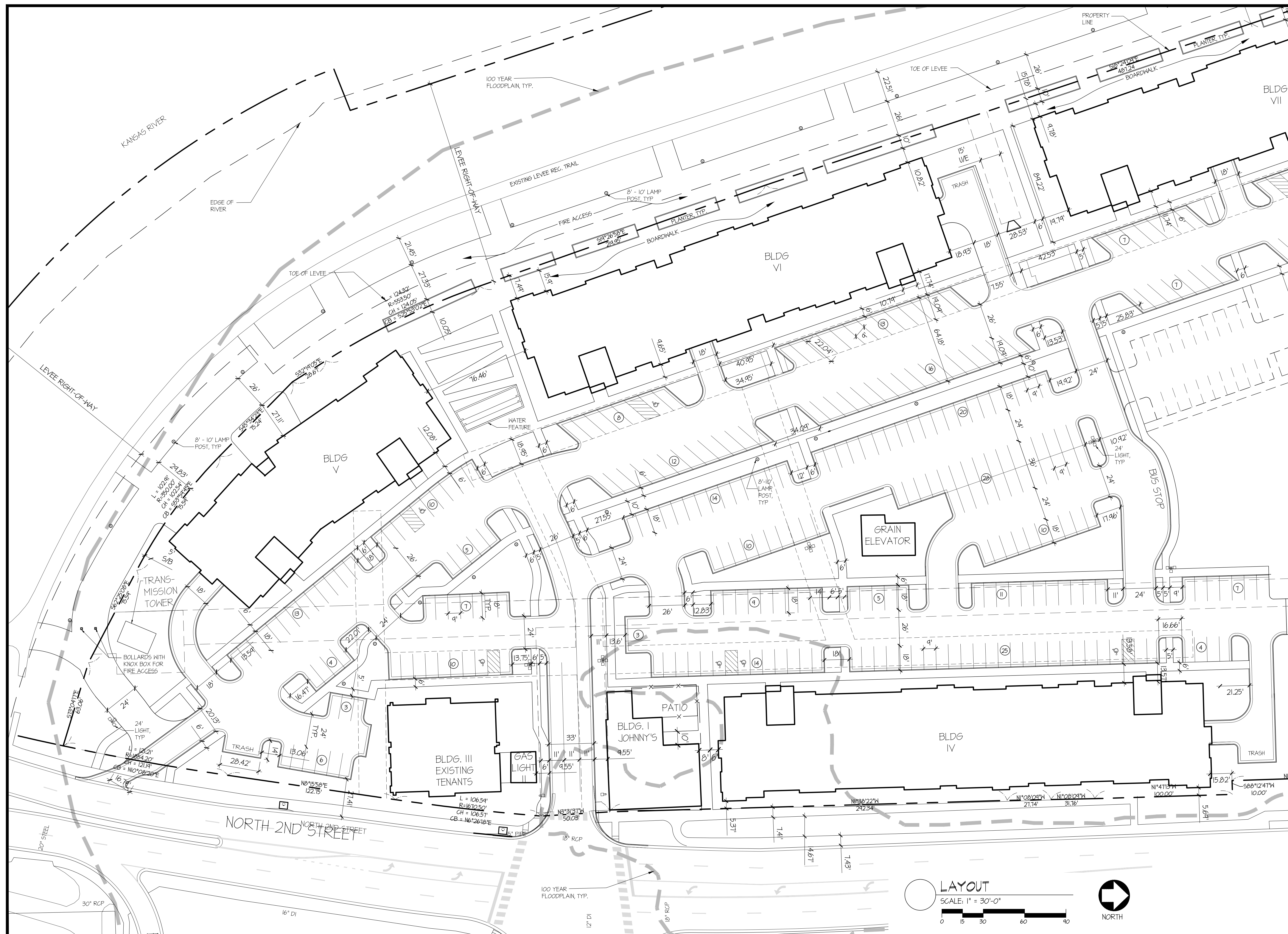


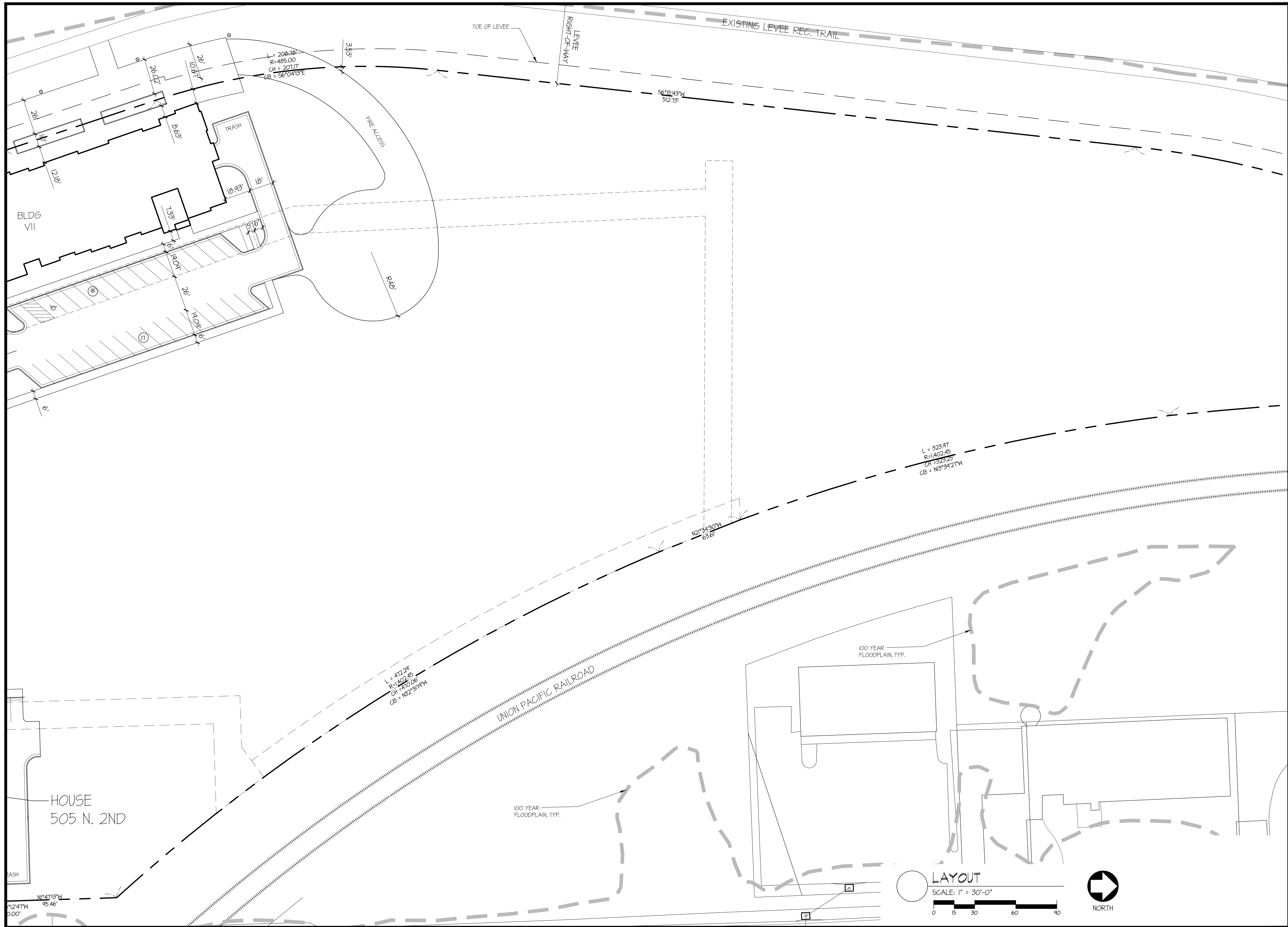
USE TABLE:

PHASE	TIME LINE	BUILDING	DESCRIPTION	PLANNED USES (SEE I.1)	FOOTPRINT (SQ. FT.)	BUILDING (G.S.F.)	BUILDING HEIGHT*	EATING AND DRINKING ESTABLISHMENT		OFFICE G.S.F.	RETAIL G.S.F.	HOTEL ROOMS	RESIDENTIAL		
								CUSTOMER SERVICE AREA	PATIO				# MULTI-FAMILY UNITS	# CONDOS	# BEDS
I	2019-2020	I	EXISTING JOHNNY'S 401 N 2ND STREET	RESTAURANT/BAR	4,000	8,000	+/- 29'-5 1/8"	3,200	400	1,000	-	-	-	-	-
I	2019-2020	II	GASLIGHT TAVERN 317 N 2ND STREET	BAR	900	900	+/- 13'-3 1/2"	600	300	-	-	-	-	-	-
I	2019-2020	III	THIRD EYE SUSAN 311 N 2ND STREET	RETAIL/OFFICE	5,600	22,400	52'-4"	-	-	2,000	3,000	-	24	0	24
I	2019-2020	IV	MULTI-USE, COMMERCIAL BUILDING, NO MORE THAN 6 STORIES PLUS 1 BASEMENT LEVEL	BASEMENT: PARKING, 1ST FLOOR: PARKING/RETAIL/OFFICE, 2ND-5TH FLOOR: MULTI-FAMILY RESIDENTIAL	28,000	168,000	51'-6"	1,000	200	3,000	2,000	-	40	0	200
		SUBTOTAL			38,500	199,300		4,800	1,400	6,000	5,000	-	114	0	224
2	2020-2021	V	6 STORIES (INCLUDES 1 LEVEL OF PARKING)	PARKING, EATING, DRINKING, OFFICE, MULTI-FAMILY	18,000	108,000	70'	4,000	1,000	18,000	11,000	-	36	0	12
3	2021-2022	VI	4 STORIES (INCLUDES 2 LEVELS OF PARKING) (SEE I.2)	PARKING, EATING, DRINKING, OFFICE, MULTI-FAMILY	25,000	225,000	40'	4,000	1,000	3,000	15,000	-	40	30	240
4	2022-2023	VII	4 STORIES (INCLUDES 2 LEVELS OF PARKING) (SEE I.2)	PARKING, EATING, DRINKING, OFFICE, MULTI-FAMILY	25,000	225,000	40'	4,000	1,000	3,000	15,000	-	40	30	240
5	2020-2021	VIII	HOTEL UP TO 7 OCCUPIED STORIES PLUS PARKING LEVEL TO BE DETERMINED (SEE I.2)	HOTEL	30,000	210,000 & PARKING LEVELS	40'	3,000	800	-	2,500	150	-	-	-
FUTURE CONCEPT	FUTURE	IX - XI	FUTURE CONCEPT												

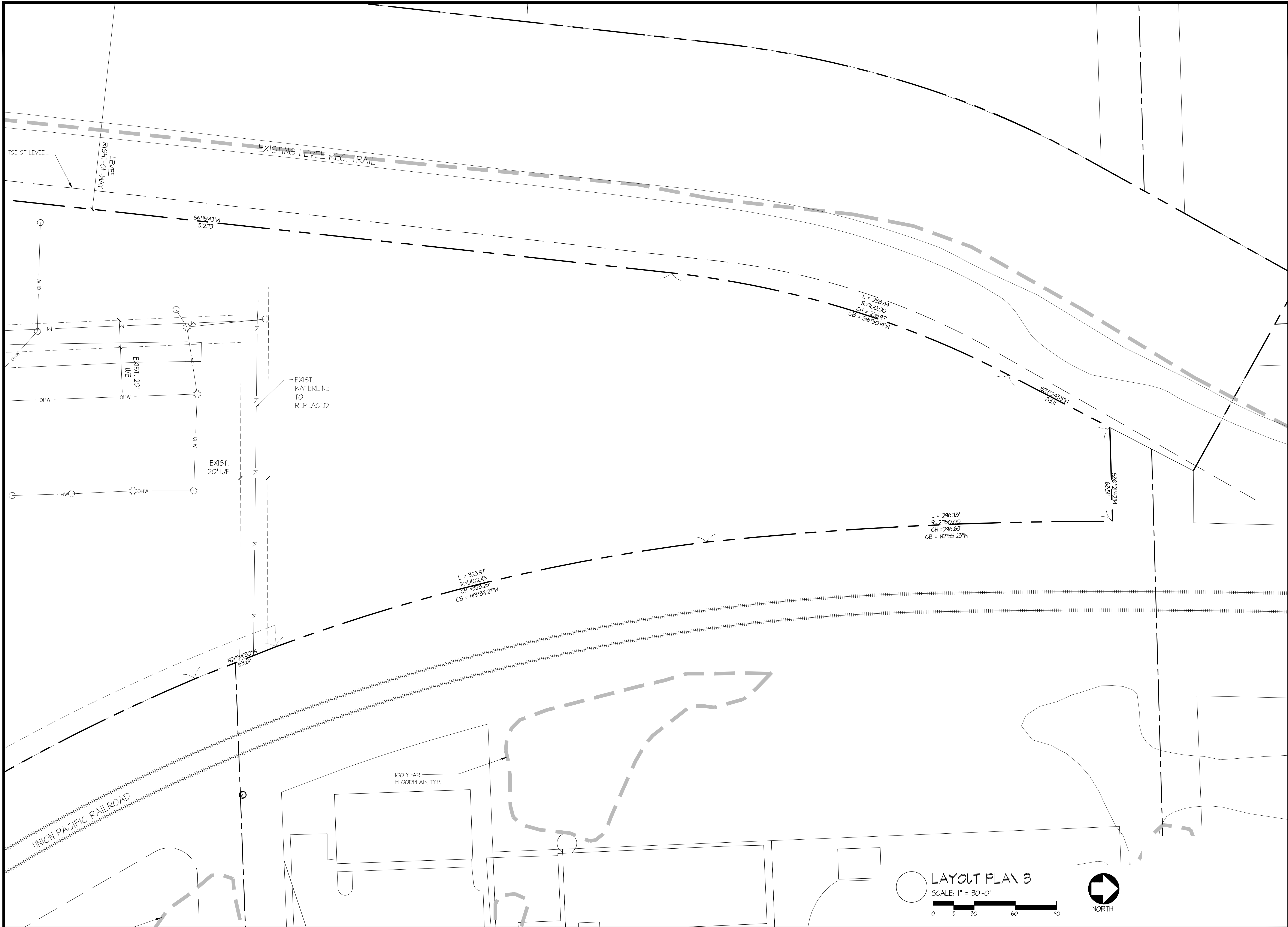
*FLUCTUATION FROM THE SHOWN HEIGHT OF BUILDINGS I-IV TO BE REVIEWED BY THE HISTORIC RESOURCES ADMINISTRATOR







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PROJECT # 217-350
AUGUST 2018

RELEASE: 1.0
1.1

DATE:
10.15.18
12.5.18

PDP-106

**NORTH LAWRENCE RIVERFRONT
PRELIMINARY
DEVELOPMENT PLAN**
LAURENCE, KANSAS

PROJECT # 211-350
AUGUST 2018

RELEASE: 1.0 DATE: 10.15.18
1.1 12.5.18

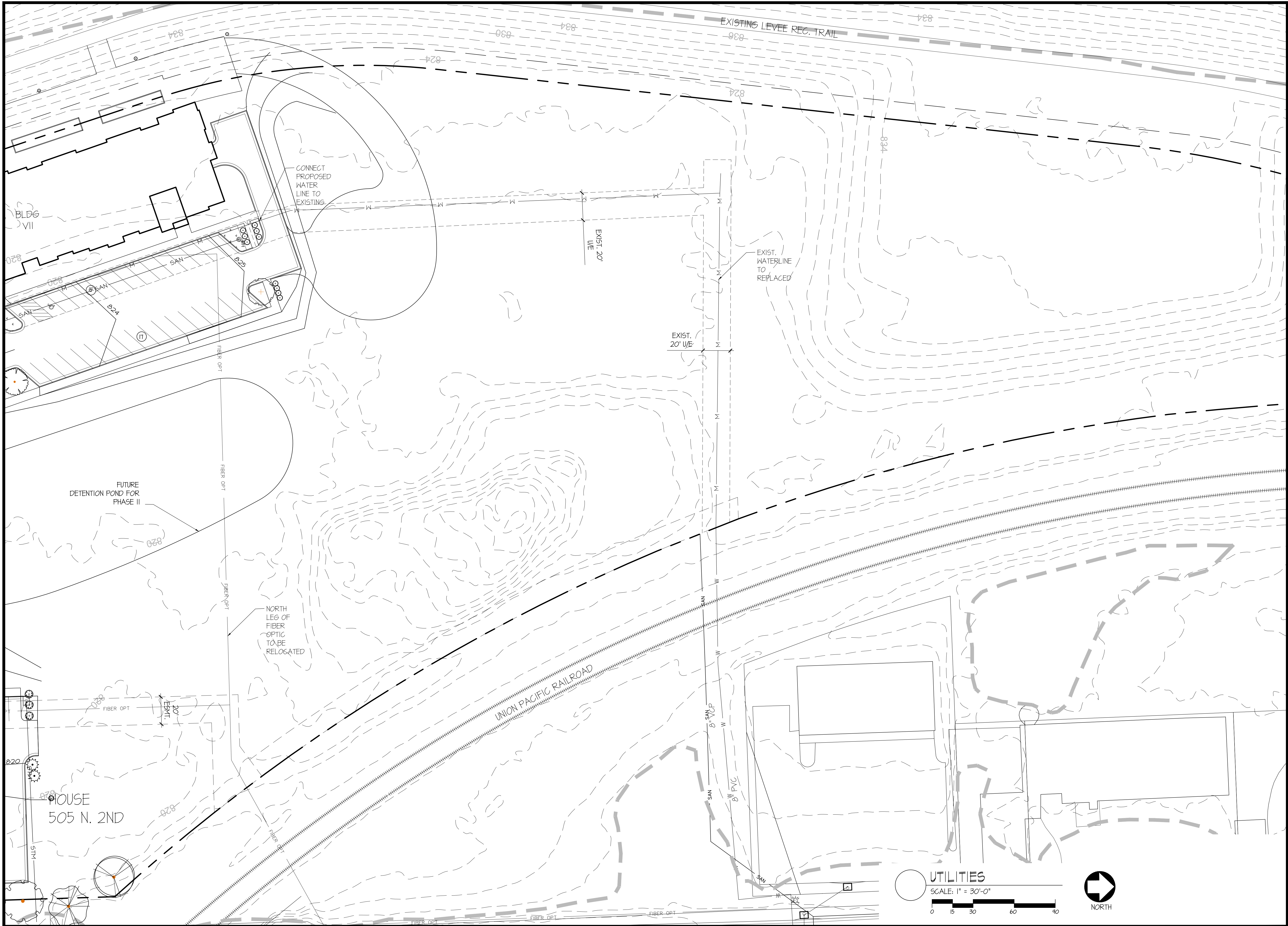
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UTILITIES
SCALE: 1" = 30'-0"



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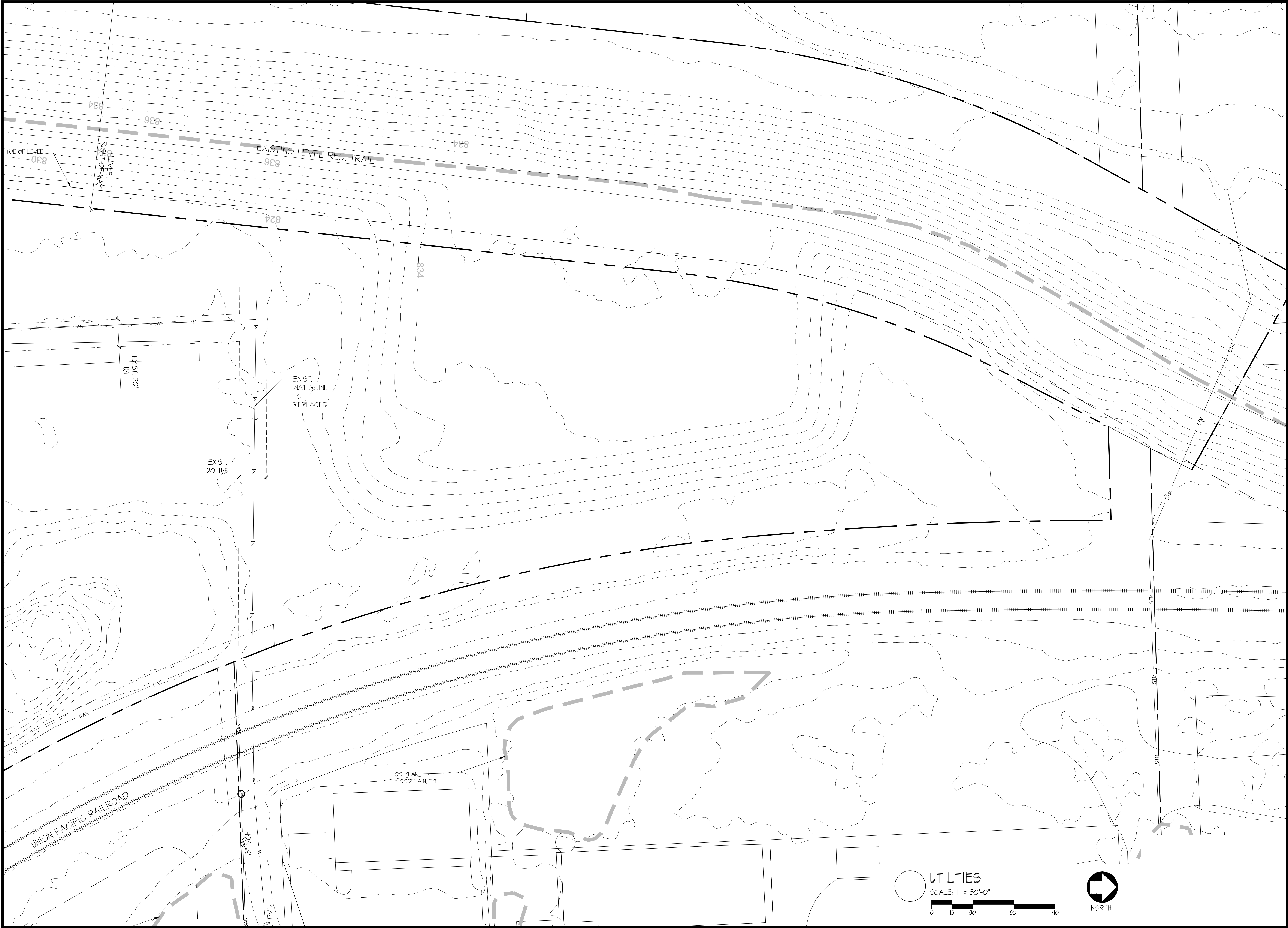
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AUGUST 2018

RELEASE: 1.0 DATE: 10.15.18
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Updated Traffic Impact Study for North Lawrence Riverfront Addition

NWC of N. 2nd Street (US 40/59) and Elm Street
City of Lawrence, Kansas

Prepared
for
Paul Werner Architects

Prepared
By



Serving Communities Through Excellence
Missouri | Kansas | Michigan | California



Mehrdad Givechi, P.E., PTOE
October 2018

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Appendix III	Results of Trip Generation Analysis Using <i>ITE Trip Generation Manual, 10th Edition</i>
Appendix IV	Results of Internal Capture Trips Using <i>NCHRP Report 684 Estimator (ITE Trip Generation Handbook, 3rd Edition)</i>
Appendix V	Summary of Traffic Counts
Appendix VI	Current Transit Route and Schedule
Appendix VII	Current and Future Bikeways

Introduction

Proposed Redevelopment

The proposed “North Lawrence Riverfront Addition” is an infill, mixed-use redevelopment located along the Kaw River in North Lawrence, Kansas within KDOT’s District 1, Area 4. The site is bounded by N. 2nd Street (US 40/59) and Union Pacific railroad to the east and by the river levee on all other sides (See Location Map, Figure 1 of Appendix I). Currently the site is occupied by a few businesses, a couple of residential homes and a small mobile home park. Under the proposed redevelopment plan, the existing businesses and residential dwelling units will be replaced with number of mixed uses as shown in the Concept Plan, Appendix I and described below:

- 30,000 gross square feet of General Office Buildings (ITE Land Use Code 710);
- 74,300 gross square feet of retail/service consisting of:
 - Two High-Turnover (Sit-Down) Restaurants (ITE Land Use Code 932) – each with gross floor area of approximately 6,000 sq. ft. including outdoor patios;
 - A Quality Restaurant (ITE Land Use Code 931) with gross floor area of approximately 6,500 square feet including an outdoor patio;
 - A Coffee Shop Without Drive Thru Lane (ITE Land Use Code 936) with gross floor area of approximately 1,200 sq. ft.;
 - Three Drinking Places (ITE Land Use Code 925) – two with gross floor area of approximately 2,600 sq. ft.; and one with gross floor area of approximately 900 sq. ft.;
 - 48,500 gross square feet of general retail space. At the time this study was prepared, specific uses for the individual retail spaces were unknown; therefore, ITE Land Use Code 820 (Shopping Center) is selected with gross leasable area (GLA) as the independent variable. For this analysis, gross leasable area is assumed to be the same as gross floor area.
- 390 dwelling units of Multifamily (Mid-Rise) residential (ITE Land Use Code 221) including 330 apartments and 60 condominiums; and
- A 150-room hotel (ITE Land Use 310).

Existing Developments and Zoning

The area in the vicinity of the project site is currently fully developed consisting of downtown Lawrence just south of the bridge over the Kaw River, residential neighborhood east of N. 2nd Street (US 40/59), and commercial and industrial uses along N. 2nd Street (US 40/59) farther to the north.

According to the City's Interactive Map, the site is currently zoned *IG (General Industrial)* with a small area on the southeast corner zoned as *CS (Commercial Strip District)*. The approved zoning for the entire site is *CD (Commercial District)* subject to the recording of the final plat.

Access

Currently, access to the site is provided at one point onto N. 2nd Street (US 40/59) at its intersection with Locust Street. Under the proposed redevelopment, a second access drive will be added onto N. 2nd Street (US 40/59), near the southernmost property line. This access drive will be restricted to "right-out" only and will also be used by fire/medical vehicles to enter the site in case of an emergency (See Concept Plan, Figure 2 of Appendix I).

Purpose

The purpose of this study is to:

1. Evaluate the existing operating conditions of traffic at the intersections of N. 2nd Street (US 40/59) with Locust, Lincoln and Lyon Streets.
2. Identify existing operational and/or safety deficiency(s), if any, at the above-mentioned intersections and recommend mitigation measures as needed.
3. Assess impact of trips generated by the proposed redevelopment on the subject intersections.
4. Recommend off-site improvements needed as the result of this redevelopment.
5. Evaluate the future operating conditions of traffic on the street network surrounding the site for target year 2040.

Data Collection and Summary

The following paragraphs summarize the results of data collection tasks and field observations for this project.

Roadway Characteristics

In the vicinity of the development sites (See Figure 3 of Appendix I for details):

- N. 2nd Street runs north/south along east side of the project site and is designated as a “*Principal Arterial*” on the City’s T2040 Thoroughfare Map. It is a four-lane undivided concrete roadway with curb/gutter sections and posted speed limit of 40 mph. North of Lincoln Street, the posted speed limit changes to 45 mph.
- Elm Street runs between N. 2nd Street (US 40/59) and N. 9th Street approximately 50 ft. north of the south property line and is designated as a “*Local Street*” with one-way operation in the eastbound direction, except for a 250 ft. segment just west of N. 3rd Street that operates as a two-way street to provide access to local businesses on the north side of Elm Street. The only posted speed limit sign on Elm Street is a “20 mph when flashing” sign in the Woodlawn school zone. The statutory speed limit is 30 mph during all other times.
- Locust Street runs east/west at the entrance to the project site approximately 400 ft. north of the south property line and is designated as a “*Collector*” on the City’s T2040 Thoroughfare Map. It has one through lane in each direction and a two-way left-turn lane between N. 2nd Street (US 40/59) and N. 3rd Street for a distance of approximately 560 ft. The posted speed limit is 30 mph.
- Lincoln Street runs east/west to the east of N. 2nd Street (US 40/59) approximately 1,000 ft. north of Locust Street and is designated as a “*Local Street*” on the City’s T2040 Thoroughfare Map. It is a two-way two-lane roadway with posted speed limit of 25 mph.
- Lyon Street runs east/west to the east of N. 2nd Street (US 40/59) and is located near the northern edge of the project site approximately 1,650 ft. north of Locust Street. It is designated as a “*Collector*” on the City’s T2040 Thoroughfare Map. It is a two-way two-lane roadway with posted speed limit of 30 mph.

- The intersection of N. 2nd Street (US 40/59) and Locust Street is controlled by a fully-actuated signal with camera detection, “*protected/permissive*” left-turn phase for north/south approaches and “*permissive only*” for east/west approaches. The lane configuration consists of:
 - North and south approach, each has one dedicated left-turn lane with approximate storage lengths of 255 ft. and 145 ft., respectively; and two through lanes with the outside lane shared by right-turn movement.
 - East approach has a dedicated left-turn lane with storage lengths of 225 ft. and one shared through- and right-turn lane.
 - West approach has one shared lane and no dedicated turn lane.
- The intersection of N. 2nd Street (US 40/59) and Lyon Street is controlled by a fully actuated signal with “*permissive only*” left-turn phase for all approaches. The lane configuration consists of:
 - North and south approach, each has two through lanes with the outside lane shared by right-turn movement and the inside lane shared by left-turn movement.
 - East and west approach, each has one shared lane and no dedicated turn lane.
 - The west leg of the intersection is a private commercial drive.
- The intersection of N. 2nd (US 40/59) and Lincoln Street is a “T” intersection controlled by a stop sign on Lincoln Street. The lane configuration consists of:
 - North and south approach, each has two through lanes with the outside lane (in the northbound direction) shared by right-turn movement and the inside lane (in the southbound direction) shared by left-turn movement.
 - East approach has a de-facto left-turn lane and a very short dedicated right-turn lane with storage for one vehicle only.

Manual Traffic Counts

Most recent peak-hour vehicular turning movement counts for the signalized intersections in the study area were obtained from the city files. These counts were taken during morning and afternoon peak-hours of typical weekdays in April 2015 and

January 2016. In addition, existing vehicular turning movement counts (dated July 2013) at the intersections of N. 2nd Street (US 40/59) with Elm Street and Lincoln Street were used for this analysis. Results, as summarized in Appendix IV and illustrated in Figure 4 of Appendix I, indicate that peak characteristics of traffic in the study area are as follows:

- On a typical weekday, morning peak occurs sometime between 7:15 and 8:30 with
 - N. 2nd Street (US 40/59), south of Locust Street, carrying peak-hour volumes of approximately 1,975 vph with directional distribution of approximately 60% - 40% (southbound - northbound). North of Locust Street, peak-hour volumes for the same time period are approximately 1,750 vph with directional distribution of approximately 55% - 45% (southbound – northbound).
 - Elm Street, east of N. 2nd Street (US 40/59), carrying peak-hour volumes of approximately 200 vph in the eastbound direction.
 - Locust Street, east of N. 2nd Street (US 40/59), carrying peak-hour volumes of approximately 320 vph with directional distribution of approximately 85% - 15% (westbound – eastbound).
 - Lyon Street, east of N. 2nd Street (US 40/59), carrying peak-hour volumes of approximately 180 vph with directional distribution of approximately 75% - 25% (westbound – eastbound).
- On a typical weekday, afternoon peak occurs sometime between 4:15 and 6:00 with
 - N. 2nd Street (US 24/40), south of Locust Street, carrying peak-hour volumes of approximately 2,250 vph with directional distribution of approximately 52% - 48% (southbound - northbound). North of Locust Street, peak-hour volumes for the same time period are approximately 2,050 vph with directional distribution of approximately 50% - 50% (southbound – northbound).
 - Elm Street, east of N. 2nd Street (US 24/40), carrying peak-hour volumes of approximately 300 vph in the eastbound direction.

- Locust Street, east of N. 2nd Street (US 24/40), carrying peak-hour volumes of approximately 350 vph with directional distribution of approximately 70% - 30% (westbound – eastbound).
- Lyon Street, east of N. 2nd Street (US 24/40), carrying peak-hour volumes of approximately 245 vph with directional distribution of approximately 50% - 50% (westbound – eastbound).
- The intersection of N. 2nd Street (US 24/40) and Locust Street carries approximately 2,015 vph and 2,325 vph during morning and afternoon peak-hours, respectively.
- The intersection of N. 2nd Street (US 24/40) and Lyon Street carries approximately 1,590 vph and 1,940 vph during morning and afternoon peak-hours, respectively.

Transit Services

N. Lawrence is currently served by the City's transit system (Line #4) with a number of stops along Locust Street, N. 7th Street, Lyon Street and N. 2nd Street (US 40/59). As shown in Appendix V, the closest stop to the proposed redevelopment site is at N. 2nd Street (US 24/40) and Locust (across the street from the site) with services every hour from 6:30 a.m. to 8:00 p.m. Monday - Saturday. Under the proposed redevelopment plan, on-site bus stop(s) will be provided to promote active transportation in the vicinity of the project site.

Existing and Planned Bikeways

According to the City's T2040 Bicycle System Map (See Appendix VI for details):

- The levee on the north side of the Kaw River that runs adjacent to the project site has a recreational trail that is designated as existing shared use path.
- The bridge over the Kaw River, connecting downtown to North Lawrence, is designated as existing bike route.
- Elm Street and N. 3rd Street are both designated as planned (future) bike route.
- Lyon Street, between N. 3rd Street and N. 5th Street is designated as existing bike route. East of N. 5th Street, it is designated as planned (future) bike route.

Under the proposed redevelopment plan, on-site pedestrian and bicycle amenities with connections to the existing adjacent bikeways and sidewalks will be provided to promote active transportation.

Evaluation of the Existing Operating Conditions

Volume/Capacity Analysis

A volume/capacity analysis (using Synchro 10 Software and methodologies outlined in the 6th Edition of the Highway Capacity Manual (HCM) published by TRB) was conducted to determine level-of-service (LOS) for all movements at the intersections under study during both morning and afternoon peak-hours of a typical weekday.

Level-of-service, as defined in the HCM, describes the quality of traffic operating condition and ranges from “A” to “F”, with LOS “A” representing the best (most desirable with minimum delay) conditions and LOS “F” the worst (severely congested with excessive delays). The following chart outlines level-of-service criteria for unsignalized and signalized intersections.

Level-Of-Service	Control Delay for Unsignalized Intersections (seconds/vehicle)	Control Delay for Signalized Intersections (seconds/vehicle)
A	0 – 10	0 – 10
B	> 10 – 15	> 10 – 20
C	> 15 – 25	> 20 – 35
D	> 25 – 35	> 35 – 55
E	> 35 – 50	> 55 – 80
F	> 50	> 80

Results of the analysis, as summarized in Appendix II and illustrated in Figure 5 of Appendix I, indicate that during the peak-hours of a typical weekday, under the existing lane geometry and phasing scheme with optimum signal timing:

- The intersection of N. 2nd and Locust operates at LOS “B” during both peak-hours with individual movements operating at LOS “B” and higher, except for the westbound left-turn movement that operates at LOS “C”.
- The intersection of N. 2nd and Lyon operates at LOS “A” during both peak-hours with individual movements operating at LOS “B” and higher.
- At the intersection of N. 2nd and Lincoln, westbound left-turn movement operates at LOS “E” and “F” during morning and afternoon peak-hours, respectively with $v/c < 0.18$ and 95th percentile stacking of one (1) vehicle. However, it is to be noted that it is not uncommon for minor stop-controlled streets along arterials to experience above average delays with low LOS.

Sight Distance

A sight distance analysis and field investigations indicate that intersection sight distance (ISD) for westbound movement on Lincoln Street at its intersection with N. 2nd Street (US 24/40) is restricted to the south by the east retaining wall of the railroad overpass.

Reasoning (Using KDOT Access Management Policy, January 2013, Tables 4-12 and 4-14)

Posted speed limit on N. 2nd Street (US 24/40) = 40 mph

Grade on N. 2nd Street (US-40/59) = +3% (NB approaching Lincoln); -3% (SB Approaching Lincoln)

Req. ISD for westbound left-turn (from Lincoln onto N. 2nd) = 475 ft. vs. 90 ft. (measured) **RESTRICTED**

Req. ISD for westbound right-turn (from Lincoln onto N. 2nd) = 385 ft. vs. 90 ft. (measured) **RESTRICTED**

Req. SSD for Northbound movement on N. 2nd Street (US-40/59) = 289 ft. vs. >1000 ft. (measured) **OK**

Req. SSD for Southbound movement on N. 2nd Street (US-40/59) = 315 ft. vs. > 1000 ft. (measured) **OK**

Dedicated Turn Lane Analysis

Using the guidelines presented in the KDOT's Access Management Policy (dated January 2013), under the existing conditions, the requirements for provision of a dedicated southbound left-turn lane and northbound right-turn lane on N. 2nd Street (US-40/59) at the intersection of Lincoln Street are met.

Reasoning:Southbound Left-Turn Lane at Lincoln Street (Table 4-28)

Operating Speed = 45 mph (assume 5 mph over the posted speed limit of 40 mph)

Existing Advance Peak-Hour Volumes = 902 vph (AM) > 400 vph; 918 vph (PM) > 400 vph

Existing Opposing Peak-Hour Volumes = 770 vph (AM); 1022 vph (PM)

Existing Left-Turn Volumes = 3 vph (AM) < 7 vph (Req. Min.)

= 15 vph (PM) > 5 vph (Req. Min.)

LT Lane Requirement Met

Northbound Right-Turn Lane at Lincoln Street (Table 4-26)

Operating Speed = 45 mph (assume 5 mph over the posted speed limit of 40 mph)

Existing Advance Peak-Hour Volumes = 770 vph (AM); 1022 vph (PM)

Existing Right-Turn Volumes = 22 vph (AM) < 41 (Req. Min.);

= 76 vph (PM) > 28 vph (Req. Min.)

RT Lane Requirement Met

Trip Generation Analysis

Trip generation of a proposed land development project is typically estimated using trip generation rates suggested by the Institute of Transportation Engineers (ITE), Trip Generation Manual (currently the 10th Edition). As mentioned earlier, for this analysis, the ITE land use codes 221, 310, 710, 820, 925, 931, 932, and 936 with their respective independent variables were selected. Results, as shown in Table 1 and Appendix III, are described in the following paragraphs.

Total Unadjusted Trips (External + Internal + Pass-By + New)

- On average, 677 trip-ends (364 inbound and 313 outbound) during morning peak-hour of a typical weekday;
- On average, 887 trip-ends (480 inbound and 407 outbound) during afternoon peak-hour of a typical weekday; and
- On average, 9,977 (+/-) trip-ends (two-way volumes) during 24-hour period of a typical weekday.

Internal Capture Trips

The above-mentioned trip numbers represent sum of the trips for single-use, free-standing sites for each proposed land use in a suburb setting. At mixed-use development sites, with two or more complementary land uses, however, there is potential for interaction among the uses that are referred to as “internal capture trips”. As a result, the total external trip numbers may be less than the simple sum of the trips generated by each use separately.

The proposed redevelopment under study is a mixed-use redevelopment consisting of “office”, “residential”, “retail” and “lodging” components with significant potential for internal capture trips. Using the guidelines suggested in the ITE Trip Generation Handbook, 3rd Edition in conjunction with the NCHRP Report 684 “Internal Capture Trip Estimation Tool”, the internal capture rate for the proposed redevelopment is 20% during the morning peak-hour and 47% during the afternoon peak-hour. For this analysis, a 20% value is assumed for both peak periods. Results, as summarized in Table 1 and shown in Appendix IV, indicate that the internal capture trips for this redevelopment are:

- On average, 135 trip-ends (69 inbound and 69 outbound) during morning peak-hour of a typical weekday; and
- On average, 177 trip-ends (96 inbound and 81 outbound) during afternoon peak-hour of a typical weekday.

External Trips

The estimated external trips for the proposed redevelopment, as summarized in Table 1, are:

- On average, 542 trip-ends (295 inbound and 244 outbound) during morning peak-hour of a typical weekday;
- On average, 710 trip-ends (384 inbound and 326 outbound) during afternoon peak-hour of a typical weekday; and
- On average, 7,982 (+/-) trip-ends (two-way volumes) during 24-hour period of a typical weekday.

Pass-By Trips

The resulting number of trips after adjustment for “internal capture trips” represents total vehicles entering and exiting the site at its proposed driveway(s). Because one of the components of the proposed uses for this site is retail-oriented, it attracts a portion of its trips from traffic passing the site on the way from origin to an ultimate destination. These retail trips are called “pass-by” trips and do not add new traffic to the adjacent street network. These trips are typically estimated using values suggested by the *ITE Trip Generation Handbook, 3rd Edition*. Table 1 shows a summary of applicable “pass-by” trips for this redevelopment; however, for this analysis a zero value is assumed.

Multimodal Concept

As mentioned earlier, this redevelopment site is located in a developed urbanized area about a block north of downtown Lawrence where walking, bicycling, and transit are viable modes of transportation. Trip generation numbers presented in the previous paragraphs do not reflect those for urban infill sites such as this site. These types of redevelopment sites often result in fewer vehicle trips due to modal shifts:

- More walking because of close proximity of complementary uses;
- More transit ridership because of convenient, frequent transit services; and
- More bicycling because of bicycle facilities and amenities available in and adjacent to the site.

Therefore, it is reasonable to imply that the trips used in this analysis may be subject to additional discounts due to the multimodal nature of this redevelopment.

Table 1

Summary of Trip Generation Calculations^{1, 2} for North Lawrence Riverfront Development (Peak-Hours of Adjacent Street Network)

Land Use (ITE CODE)	Setting/ Location	Size	Typical Weekday						
			24-hr, 2-Way Volume (vpd)	AM Peak-Hour ³ (vph)			PM Peak-Hour ³ (vph)		
				Enter	Exit	Total	Enter	Exit	Total
Mid-Rise Multifamily Housing (221)*	Gen. Urban/Suburban	390 units	2,124	34	96	130	100	64	164
Hotel (310)*	Gen. Urban/Suburban	150 rooms	1,254	41	29	70	44	42	86
General Office (710)*	Gen. Urban/Suburban	30,000 sq. ft.	330	47	8	55	6	30	36
Shopping Center (820)*	Gen. Urban/Suburban	48,500 GFA ⁴ 48,500 GLA ⁵	3,676	109	67	176	153	165	318
Drinking Place (925)	Gen. Urban/Suburban	2,600 sq. ft.					20	10	30
Drinking Place (925)	Gen. Urban/Suburban	2,600 sq. ft.					20	10	30
Drinking Place (925)	Gen. Urban/Suburban	900 sq. ft.					7	3	10
Quality Restaurant (931)	Gen. Urban/Suburban	6,500 sq. ft.	545	5	0	5	34	17	51
High-Turnover Restaurant (932)	Gen. Urban/Suburban	6,000 sq. ft.	673	33	27	60	37	22	59
High-Turnover Restaurant (932)	Gen. Urban/Suburban	6,000 sq. ft.	673	33	27	60	37	22	59
Coffee Shop without D.T. Lane (936)	Gen. Urban/Suburban	1,200 sq. ft.	702	62	59	121	22	22	44
TOTAL UNADJUSTED TRIPS (Buildings I thru VII and Hotel)			9,977	364	313	677	480	407	887
Internal Capture Trips [AM Peak = 20%; PM Peak = 47% (Assume 20%)] ⁶			-1995	-69	-69	-135	-96	-81	-177
UNADJUSTED EXTERNAL TRIPS (Buildings I thru VII and Hotel)			7982	295	244	542	384	326	710
Other Applicable Adjustments :									
- Multi-Modal Use for Retail Component (Assume 0%)				0	0	0	0	0	0
- Pass-By Trips for Shopping Center (AM = 0%; PM = 34%) ⁷							-42	-45	-86
- Pass-By Trips for Restaurants (AM = 0%; PM = 43%) ⁷							-37	-21	-58
ADJUSTED EXTERNAL TRIPS ADDED TO NETWORK (Building I thru VII and Hotel)			7,982	295	244	542	305	260	565

NOTES:

- 1) Trip generation numbers in this table are calculated using the rates suggested in the *ITE Trip Generation Manual, 10th Edition*.
- 2) Number of trips are determined by both Weighted Average Rate Method and the Regression Equation Method and the method that meets the criteria suggested in the *ITE Trip Generation Handbook, 3rd Edition* with statistical significance is selected for analysis (* denotes use of Reg. Eq.)
- 3) Peak-hour of adjacent street network.
- 4) GFA = Gross Floor Area
- 5) GLA = Gross Leasable Area (for this study, GLA is assumed to be equal to GFA)
- 6) Calculated using the *NCHRP 684, Internal Trip Capture Estimation Tool*. However, for this study, a 20% rate is assumed for both AM and PM peak hours.
- 7) Calculated using suggested values in the *ITE Trip Generation Handbook, 3rd Edition*.

Trip Distribution and Assignment Analysis

For the purpose of this study, it is assumed that individual components of the proposed redevelopment site have their own unique trip distribution patterns based on a combination of several factors such as:

- The existing traffic distribution patterns;
- Most direct and short route vs. a desirable alternative route that is either less congested (path of least resistance) or is more convenient;
- Employment-based trips;
- Home-based trips; and
- Retail-based trips.

“Hotel” Component (Figure 6 of Appendix I)

- 95% to/from north using N. 2nd Street; and
- 5% to/from south using the bridge over the Kaw River.

“Residential” and “Office” Components (Figure 7 of Appendix I)

- 40% to/from north using N. 2nd Street; and
- 60% to/from south using the bridge over the Kaw River.

“Retail” Component (Figure 8 of Appendix I)

- 41% to/from north using N. 2nd Street. Of which 2% to/from Lincoln Street and 10% to/from Lyon Street;
- 11% to/from east using Locust Street; and
- 48% to/from south using the bridge over the Kaw River.

Using these trip distribution patterns, site-generated trips are assigned to individual movements in the study area as illustrated in Figure 9 of Appendix I.

Analysis Time Period

An overview of the existing traffic volumes in the study area and their peak characteristics, in conjunction with estimated trips generated by the proposed redevelopment, indicate that the most critical peak period will likely occur during the afternoon peak-hour of a typical weekday. For this study, however, both peak-hours are analyzed.

Impact Assessment

Volume/Capacity Analysis

For the purpose of this analysis, the unadjusted external trip generation numbers were used to assess traffic impact of this redevelopment under the “worst case scenario” with no consideration for “pass-by” trips and multimodal characteristics of the project site. Results of a volume/capacity analysis indicate that during the peak-hours of a typical weekday, with certain site access improvements (i.e. a dedicated left-turn lane and a shared through/right turn lane for eastbound approach on Locust Street at its intersection with N. 2nd Street (US 40/59); and a new restricted “right/out” access drive near the south property line):

- The intersection of N. 2nd Street (US 40/59) and Locust Street will likely operate at LOS “B” and “C” during morning and afternoon peak-hours, respectively with individual movements likely operating at LOS “C” and higher, except for the westbound left-turn movement that will likely operate at LOS “D” during morning peak-hour. Moreover, the 95th percentile stacking length for the northbound left-turn movement (higher of the morning and afternoon peak-hour) will be 156’, which utilizes the entire available storage length of this lane. The extent of which this lane can be lengthened is limited with some design constraints due to proximity of the bridge over the Kaw River.
- The intersection of N. 2nd Street (US 40/59) and Lyon Street will likely operate at LOS “A” during both peak-hours with individual movements likely operating at

LOS “B” and higher.

- At the intersection of N. 2nd Street (US 40/59) and Lincoln, westbound left-turn movement will still operate at LOS “E” and “F” during morning and afternoon peak-hours, respectively. As mentioned earlier, this movement carries very little traffic with v/c of < 0.27 with 95th percentile stacking of one (1) vehicle.

Target Year 2040

According to the City’s T2040 Plan, the most recent Travel Demand Model (TDM) projects the following LOS for the street network surrounding this development site for Target Year 2040 (Refer to T2040, Figure 7.6, LOS Map, TDM, scenario #13).

- N. 2nd Street, north of Lyon Street will likely operate at uncongested level with LOS “C” or higher. Between Lyon Street and Locust Street, it will likely experience some congestion with LOS “D”. South of Locust Street (the bridge on Kaw River extending onto downtown) will likely operate at congested level (at or above capacity).
- Lyon Street, Locust Street and Elm Street all will likely operate at uncongested level with LOS “C” or higher.

Summary and Recommendations

This study evaluates the existing operating conditions of traffic at selected intersections within the study area (See Location Map, Figure 1 of Appendix I). It also assesses the impact of traffic generated by the proposed North Lawrence Riverfront Addition on the adjacent street network. In addition, a cursory evaluation of traffic for target year 2040 is presented as well.

Existing Conditions (See Figures 3, 4 & 5 of Appendix I)

- 1) Under the existing geometric and operating conditions, the signalized intersections in the study area operate at LOS “B” and higher with ample reserve capacity. The only movement that operates at LOS “C” is the westbound left-turn movement on Locust Street at its intersection with N. 2nd Street (US40/59) with some reserve capacity. Results of the analysis also indicate that the 95th percentile stacking (queue length) for dedicated turn lanes at the intersection of N. 2nd Street (US 40/59) and Locust Street are as follows:
 - 150’ for westbound left-turn lane (available length = 225’, OK);
 - <25’ for southbound left-turn lane (available length = 255’, OK); and
 - <25’ for northbound left-turn lane (available length = 145’, OK).

- 2) At the intersection of N. 2nd Street (US 40/59) and Lincoln Street, westbound left-turn movement experiences above the average delays during both, morning and afternoon peak-hours. As mentioned earlier, this movement carries very little traffic with $v/c < 0.18$ and 95th percentile stacking of one (1) vehicle. It should be noted that it is not uncommon for minor stop-controlled streets along arterials to experience excessive delays.

Furthermore, field observation indicates that the existing railroad overpass retaining wall (south of this intersection) restricts sight distance for traffic exiting Lincoln Street. Given the availability of several other east/west alternative routes, motorist will likely avoid using this intersection.

Following KDOT's Access Management Policy guidelines (using traffic volumes as the criteria), requirements for provision of a dedicated southbound left-turn lane and northbound right-turn lane on N. 2nd Street (US 40/59) at its intersection with Lincoln Street are met. However, because there is very little traffic (about 15 vph) negotiating this maneuver during the peak-hour, a southbound left lane should be considered if crash history indicates that there is a safety concern at this location.

Existing + Proposed Redevelopment (See Figures 10 & 11 of Appendix I)

As mentioned earlier, for this analysis, the scenario representing the unadjusted external trip generation numbers prior to any discounts for “pass-by” trips and multimodal characteristics was selected.

Results of the analysis indicate that with certain site access improvements including:

- A dedicated left-turn lane and a shared through/right turn lane for eastbound approach on Locust Street at its intersection with N. 2nd Street (US 40/59); and
- A new restricted “right-out” access drive near the south property line

the signalized intersections in the study area will likely operate at LOS “C” and higher with some reserve capacity. The only movement that will likely operate at LOS “D” is the westbound left-turn movement on Locust Street at its intersection with N. 2nd Street (US 40/59).

Results of the analysis also indicate that the 95th percentile stacking for the dedicated turn lanes at the intersection of N. 2nd Street (US 40/59) and Locust Street are as follows:

- 201' for westbound left-turn lane (available length = 225', OK);
- 29' for southbound left-turn lane (available length = 255', OK); and
- 156' for northbound left-turn lane (available length = 145', Marginal). The extent of which this lane can be lengthened is limited with some design constraints due to proximity of the bridge over the Kaw River.

Recommended Improvements

1. At the intersection of N. 2nd Street (US-40/59) and Locust Street:
 - a) Provide a dedicated eastbound left-turn lane on Locust Street with minimum storage length of 100 ft.
 - b) Provide a shared through/right turn lane on Locust Street.
 - c) Increase the storage length of the existing northbound left-turn lane as far as possible.
 - d) Consider modifying signal timing and phasing scheme as needed.
2. Provide a restricted “right-out” access drive near the south property line across from Elm Street.
3. Provide additional access drives onto N. 2nd Street (US 40/59) with the future phases of the redevelopment and/or when access at Locust Street cannot meet the demand.

APPENDIX I










Figures



Figure 1
Location Map

SITE PLAN

LEGEND

-  PROPOSED DEVELOPMENT
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  SHARED LANE
-  DEDICATED LANE
-  35 MPH SPEED LIMIT
-  North

Not to Scale

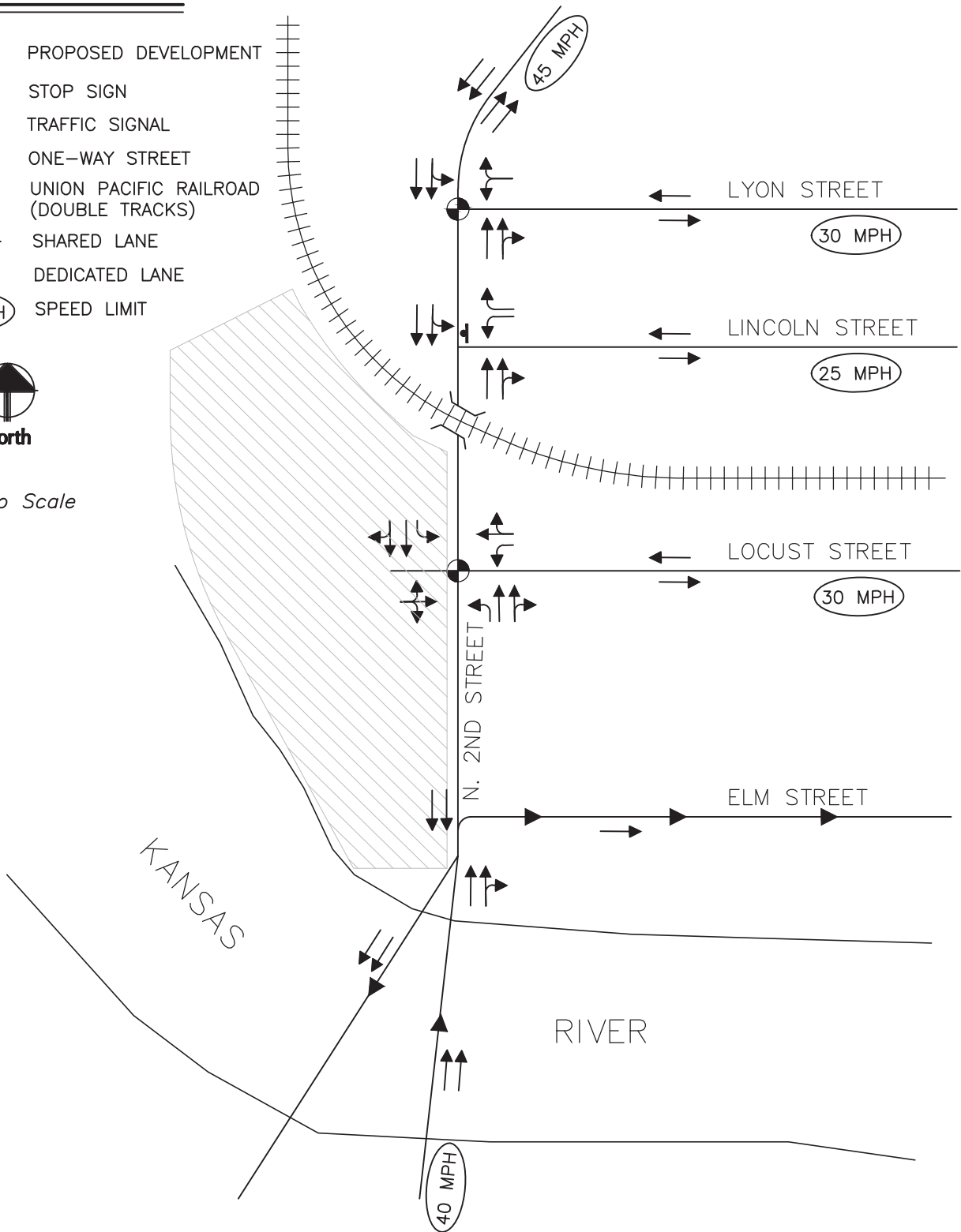







FIGURE 3
EXISTING LANE CONFIGURATIONS,
TRAFFIC CONTROL DEVICES, AND POSTED SPEED LIMITS
(2018)

LEGEND

-  PROPOSED DEVELOPMENT
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
- XX(XX) A.M. (P.M.)



North

Not to Scale

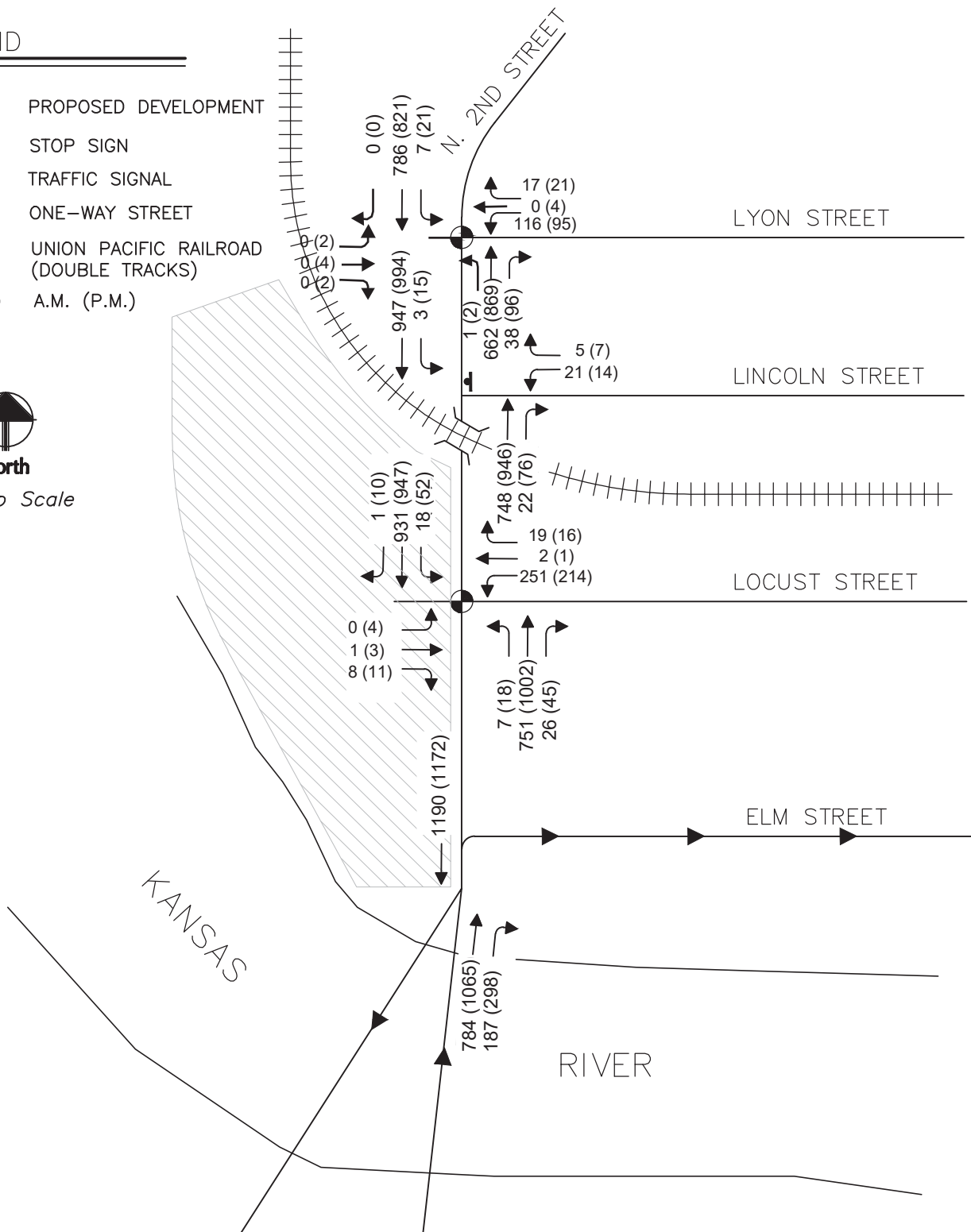








FIGURE 4
EXISTING PEAK-HOUR TRAFFIC VOLUMES
(TYPICAL WEEKDAY, 2015-2016)

LEGEND

-  PROPOSED DEVELOPMENT
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  MOVEMENT GROUP
- XX(XX) A.M. (P.M.)
- B LEVEL OF SERVICE
- * FREE FLOW
- [XXX] 95TH PERCENTILE STACKING (FT) MAXIMUM A.M./P.M.



North

Not to Scale

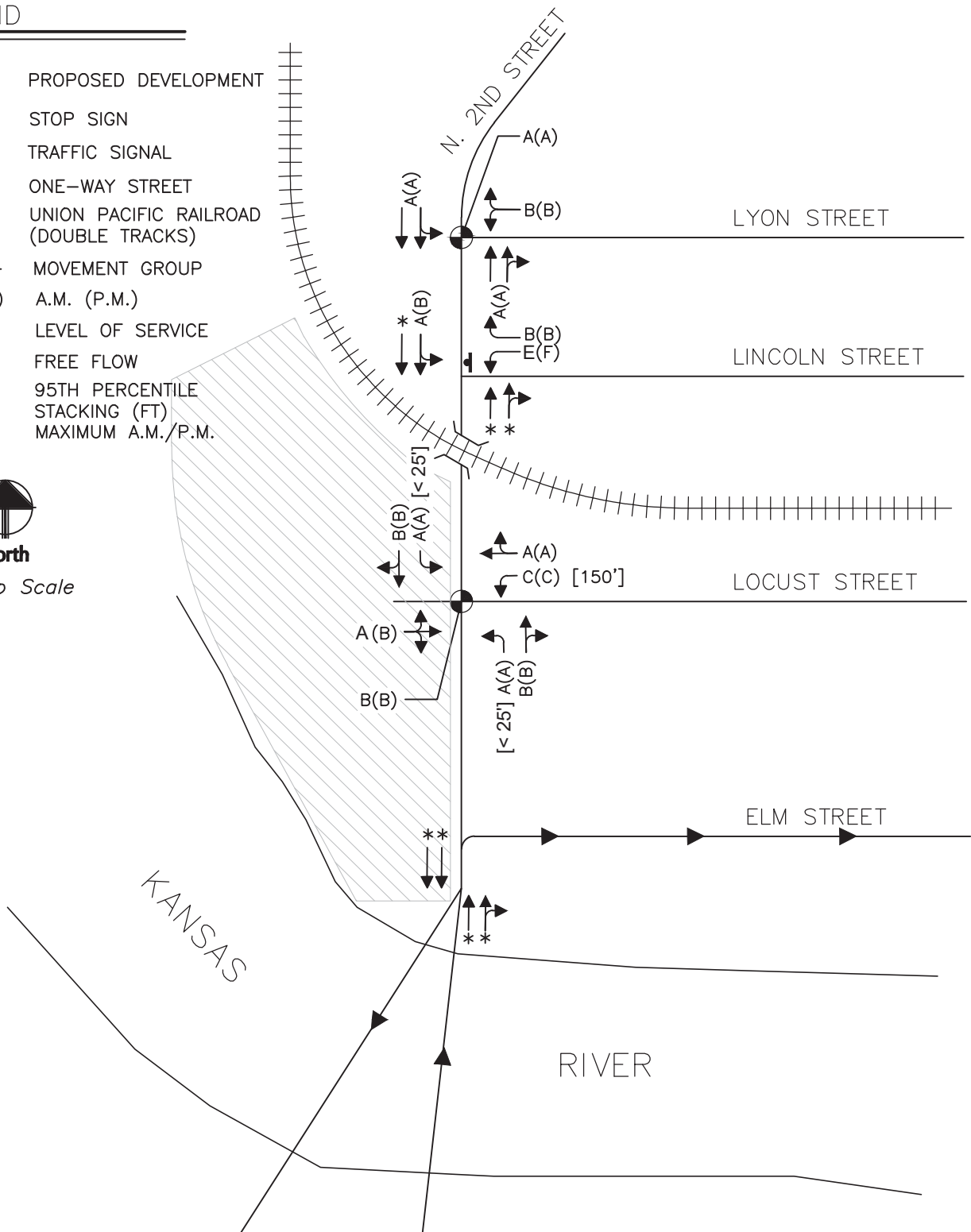










FIGURE 5
SUMMARY OF LEVEL OF SERVICE
FOR EXISTING TRAFFIC CONDITIONS
(PEAK-HOURS OF A TYPICAL WEEKDAY, 2015-2016)

LEGEND

-  PROPOSED DEVELOPMENT
-  PROPOSED ACCESS
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  OUTBOUND TRIPS
-  INBOUND TRIPS



North

Not to Scale

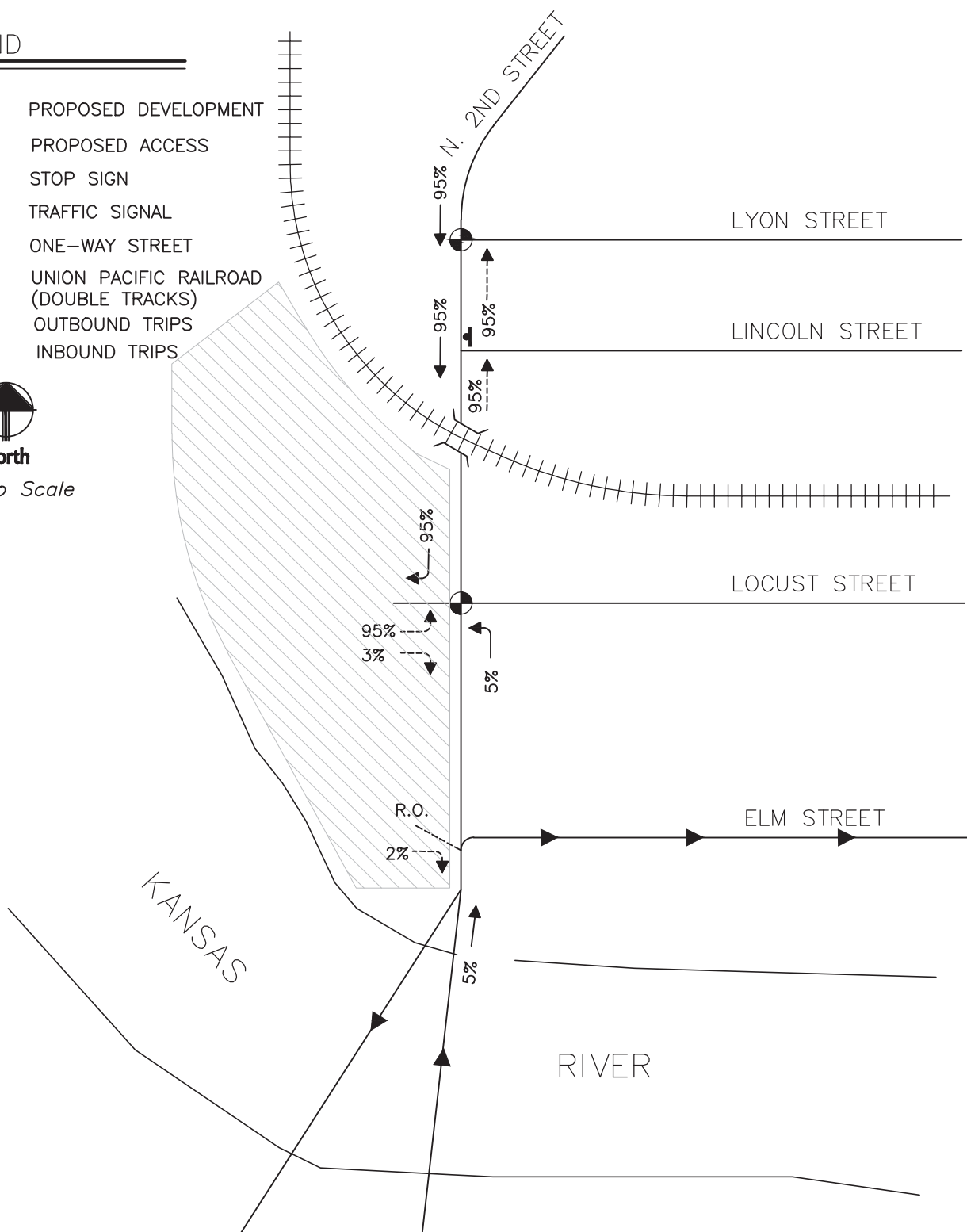










FIGURE 6
TRIP DISTRIBUTION PATTERNS FOR
"HOTEL" COMPONENT OF NORTH LAWRENCE
RIVERFRONT ADDITION
(PEAK-HOUR OF A TYPICAL WEEKDAY)

LEGEND

-  PROPOSED DEVELOPMENT
-  PROPOSED ACCESS
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  OUTBOUND TRIPS
-  INBOUND TRIPS



North

Not to Scale

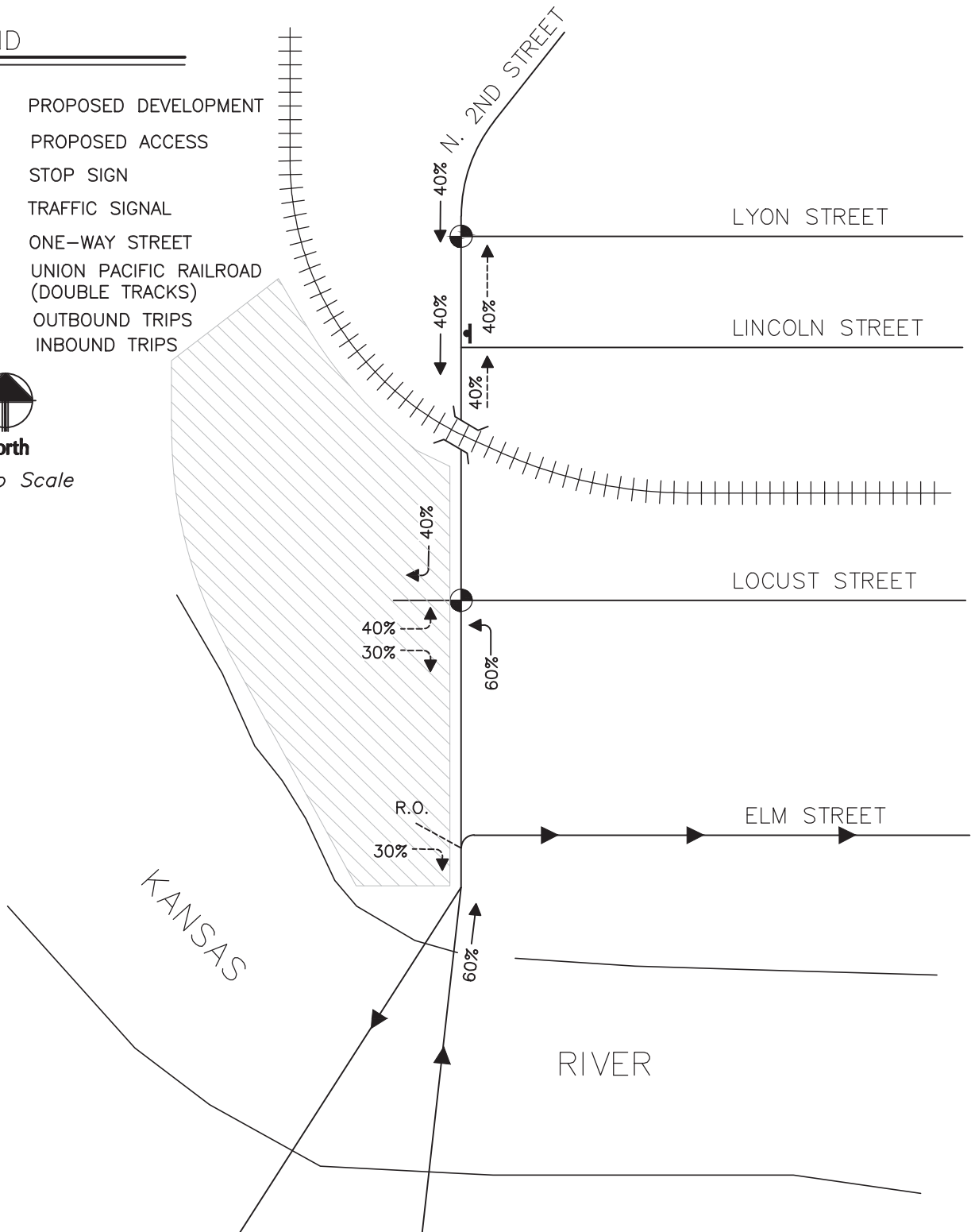










FIGURE 7
TRIP DISTRIBUTION PATTERNS FOR
"RESIDENTIAL" AND "OFFICE" COMPONENTS OF NORTH
LAWRENCE RIVERFRONT ADDITION
(PEAK-HOUR OF A TYPICAL WEEKDAY)

LEGEND

-  PROPOSED DEVELOPMENT
-  PROPOSED ACCESS
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  OUTBOUND TRIPS
-  INBOUND TRIPS



North

Not to Scale

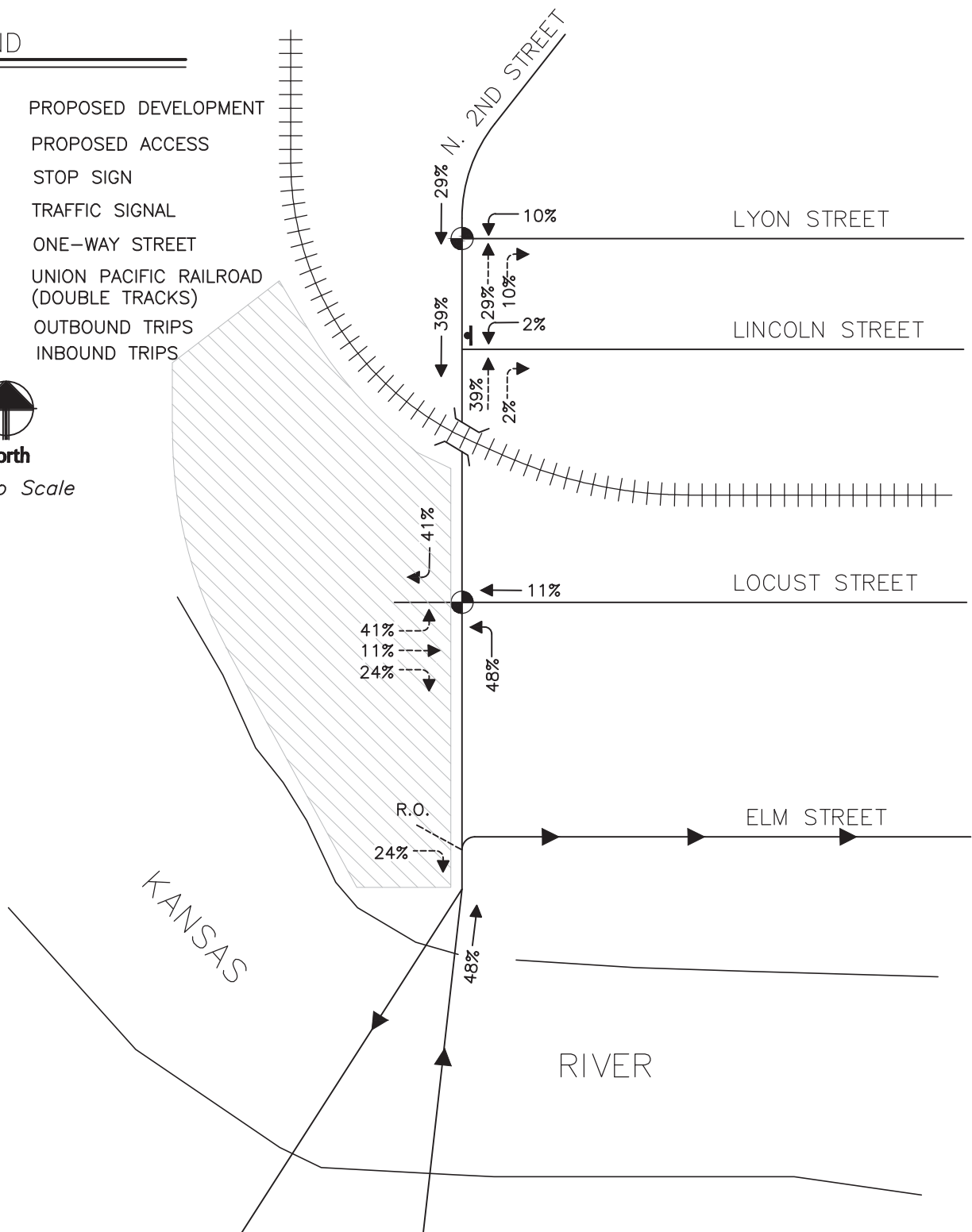








FIGURE 8
TRIP DISTRIBUTION PATTERNS FOR
"RETAIL" COMPONENT OF NORTH LAWRENCE
RIVERFRONT ADDITION
(PEAK-HOUR OF A TYPICAL WEEKDAY)

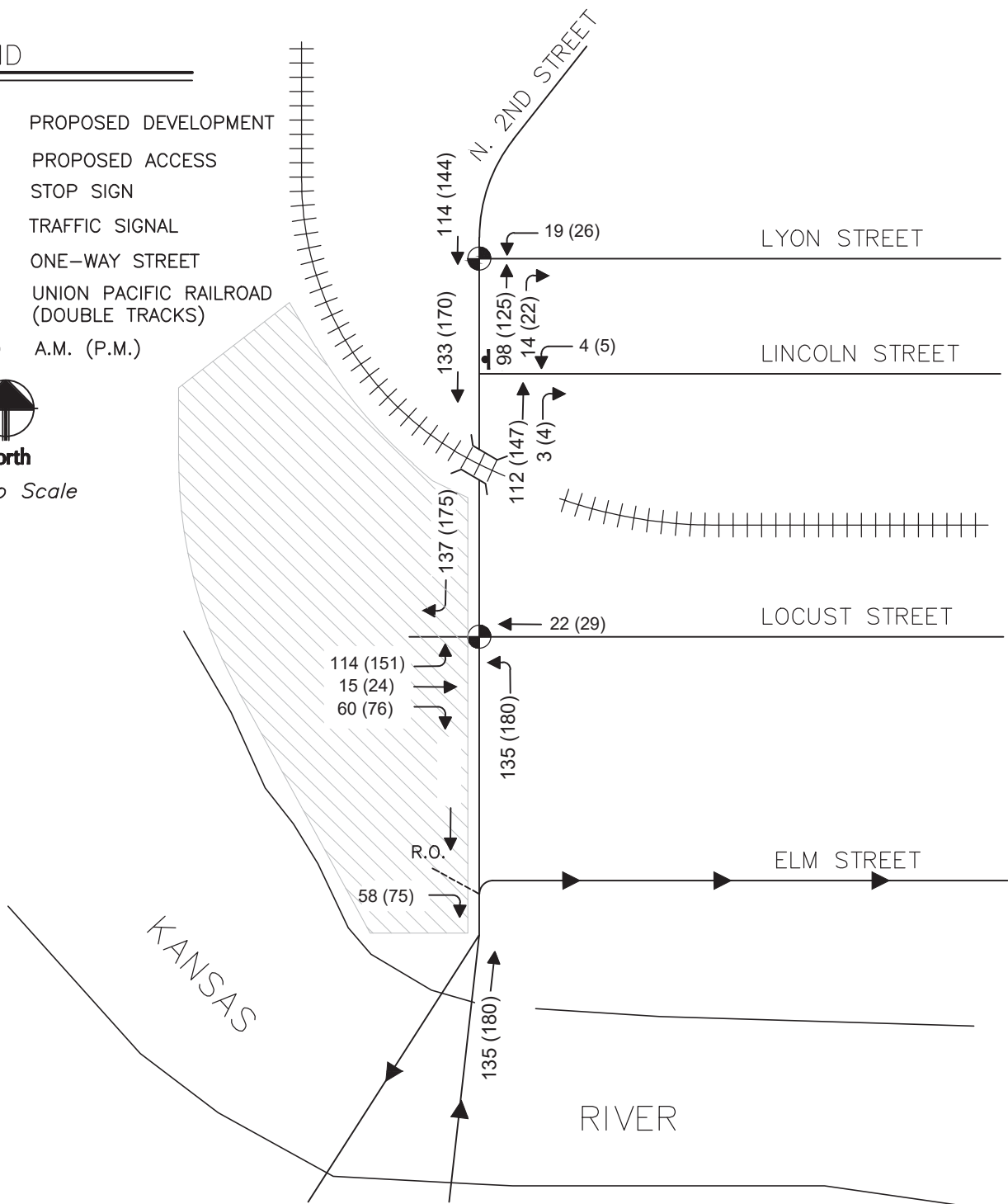
LEGEND

-  PROPOSED DEVELOPMENT
-  PROPOSED ACCESS
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
- XX(XX) A.M. (P.M.)



North






Not to Scale



TRIPS (VPH)	UNADJUSTED		INTERNAL CAPTURE		DRIVEWAY	
	AM	PM	AM	PM	AM	PM
INBOUND	364	480	-69	-96	295	384
OUTBOUND	313	407	-69	-81	244	326
TOTAL 2-WAY	677	887	-135	-177	542	710

FIGURE 9
SITE-GENERATED EXTERNAL TRIPS
NORTH LAWRENCE RIVERFRONT ADDITION BUILDINGS I - VII AND HOTEL
(PEAK-HOURS OF A TYPICAL WEEKDAY)

LEGEND

-  PROPOSED DEVELOPMENT
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
- XX(XX) A.M. (P.M.)



North

Not to Scale

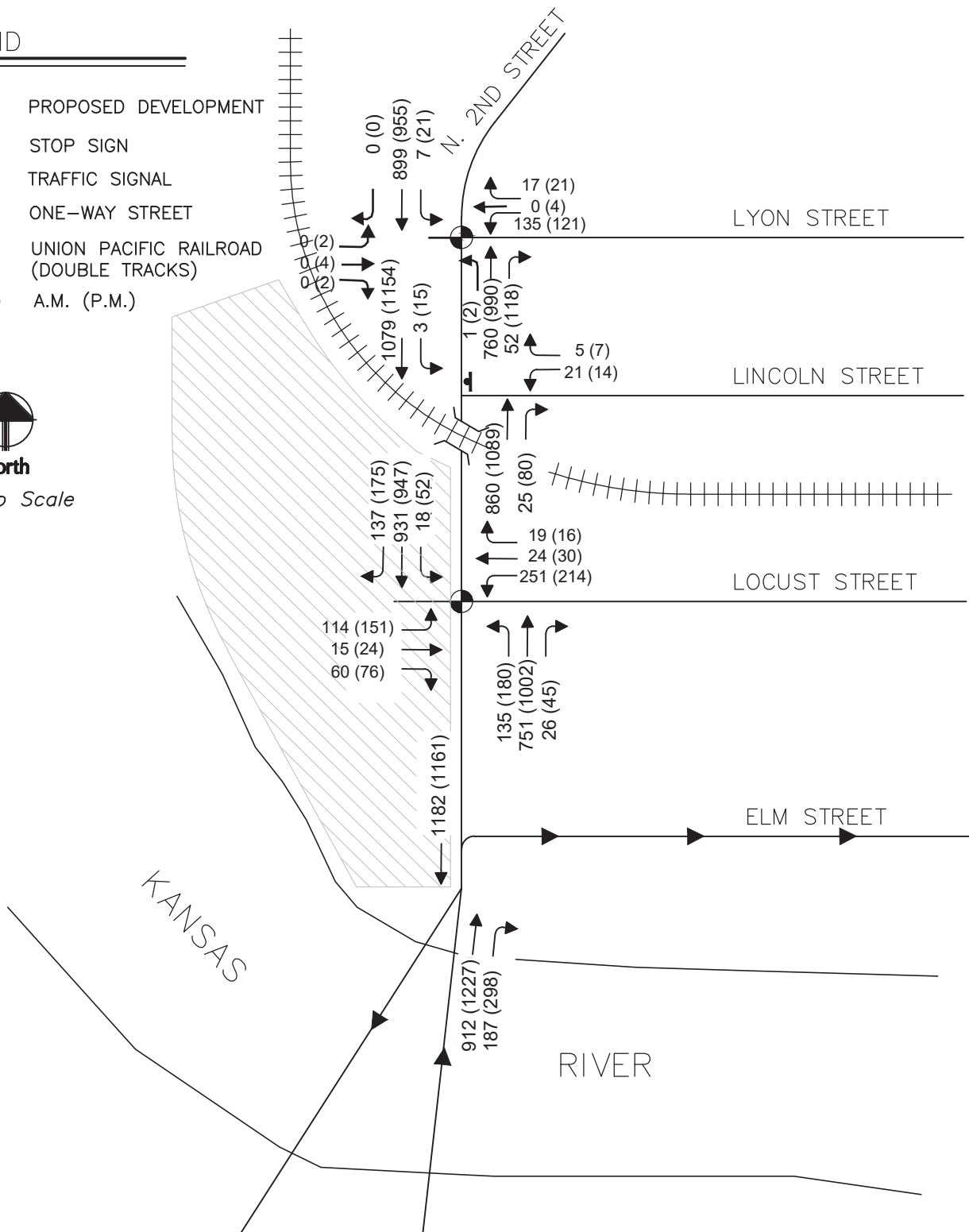









FIGURE 10

"ADJUSTED EXISTING + NORTH LAWRENCE RIVERFRONT ADDITION (BUILDINGS I -VII AND HOTEL)"
TRAFFIC VOLUMES (PEAK-HOURS OF A TYPICAL WEEKDAY)

LEGEND

-  PROPOSED DEVELOPMENT
-  PROPOSED ACCESS
-  STOP SIGN
-  TRAFFIC SIGNAL
-  ONE-WAY STREET
-  UNION PACIFIC RAILROAD (DOUBLE TRACKS)
-  MOVEMENT GROUP
- XX(XX) A.M. (P.M.)
- * FREE FLOW
- B LEVEL OF SERVICE
- [XXX] 95TH PERCENTILE STACKING (FT)
- MAXIMUM A.M./P.M.



North

Not to Scale

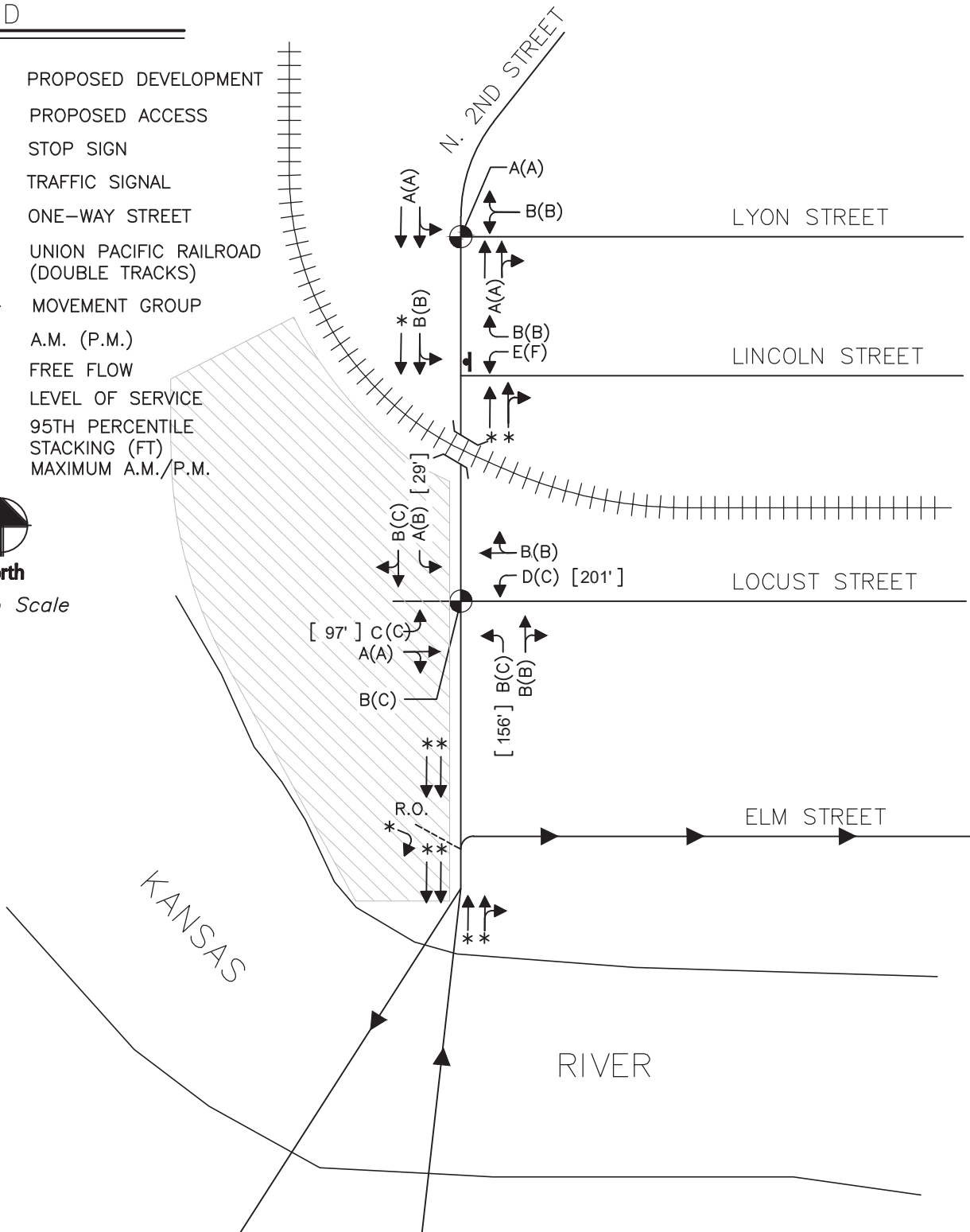


FIGURE 11
SUMMARY OF LEVEL OF SERVICE FOR
"ADJUSTED EXISTING + NORTH LAWRENCE RIVERFRONT ADDITION (BUILDINGS I -VII AND HOTEL)"
TRAFFIC VOLUMES (PEAK-HOURS OF A TYPICAL WEEKDAY)

APPENDIX II




















Results of Volume/Capacity Analysis
Using
Synchro 10 Software
(HCM 6th Edition Methodology)













**“EXISTING”
CASE SCENARIO**

N. 2nd (US 40/59) & Locust

Existing Conditions

Morning Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	1	8	251	2	19	7	751	26	18	931	1
Future Volume (vph)	1	1	8	251	2	19	7	751	26	18	931	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	130		0	140		0	250		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.890			0.863			0.995				
Flt Protected		0.995		0.950			0.950			0.950		
Satd. Flow (prot)	0	1650	0	1770	1608	0	1770	3522	0	1770	3539	0
Flt Permitted		0.986		0.750			0.188			0.266		
Satd. Flow (perm)	0	1635	0	1397	1608	0	350	3522	0	495	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			21			6				
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		200			252			310			922	
Travel Time (s)		4.5			5.7			7.0			14.0	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	1	1	9	273	2	21	8	816	28	20	1012	1
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	11	0	273	23	0	8	844	0	20	1013	0
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.8	20.8		20.8	20.8		9.3	21.3		9.3	21.3	
Total Split (s)	24.0	24.0		24.0	24.0		9.4	26.6		9.4	26.6	
Total Split (%)	40.0%	40.0%		40.0%	40.0%		15.7%	44.3%		15.7%	44.3%	
Yellow Time (s)	3.2	3.2		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	1.6	1.6		1.6	1.6		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		4.8		4.8	4.8		5.3	5.3		5.3	5.3	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None		None	None		None	Min		None	Min	
Act Effct Green (s)		13.9		13.9	13.9		22.5	21.8		22.5	21.8	
Actuated g/C Ratio		0.29		0.29	0.29		0.47	0.46		0.47	0.46	
v/c Ratio		0.02		0.67	0.05		0.03	0.52		0.06	0.62	
Control Delay		8.6		23.9	7.3		7.6	12.0		7.7	13.8	
Queue Delay		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Delay		8.6		23.9	7.3		7.6	12.0		7.7	13.8	
LOS		A		C	A		A	B		A	B	
Approach Delay		8.6			22.6			12.0			13.7	
Approach LOS		A			C			B			B	
Queue Length 50th (ft)		0		58	0		1	71		3	92	
Queue Length 95th (ft)		10		153	14		7	188		12	#244	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Internal Link Dist (ft)		120			172			230			842	
Turn Bay Length (ft)				130			140			250		
Base Capacity (vph)		678		574	674		290	1678		345	1683	
Starvation Cap Reductn		0		0	0		0	0		0	0	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.02		0.48	0.03		0.03	0.50		0.06	0.60	

Intersection Summary

Area Type: Other

Cycle Length: 60

Actuated Cycle Length: 47.5

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.67

Intersection Signal Delay: 14.2

Intersection LOS: B

Intersection Capacity Utilization 54.8%


ICU Level of Service A




















Analysis Period (min) 15













95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 7: Locust Street & N. 2nd Street

 Ø1	 Ø2	 Ø4
9.4 s	26.6 s	24 s
 Ø5	 Ø6	 Ø8
9.4 s	26.6 s	24 s

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	4	3	11	214	1	16	18	1002	45	52	947	10
Future Volume (vph)	4	3	11	214	1	16	18	1002	45	52	947	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		0	130		0	140		0	250		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.915			0.858			0.994			0.998	
Flt Protected		0.990		0.950			0.950			0.950		
Satd. Flow (prot)	0	1687	0	1770	1598	0	1770	3518	0	1770	3532	0
Flt Permitted		0.963		0.745			0.214			0.150		
Satd. Flow (perm)	0	1641	0	1388	1598	0	399	3518	0	279	3532	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		12			17			6			1	
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		200			252			310			922	
Travel Time (s)		4.5			5.7			7.0			14.0	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	4	3	12	233	1	17	20	1089	49	57	1029	11
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	19	0	233	18	0	20	1138	0	57	1040	0
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	34.8	34.8		34.1	34.1		9.3	27.4		9.3	21.3	
Total Split (s)	34.8	34.8		34.8	34.8		9.3	30.8		9.4	30.9	
Total Split (%)	46.4%	46.4%		46.4%	46.4%		12.4%	41.1%		12.5%	41.2%	
Yellow Time (s)	3.2	3.2		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	1.6	1.6		1.6	1.6		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		4.8		4.8	4.8		5.3	5.3		5.3	5.3	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None		None	None		None	Min		None	Min	
Act Effct Green (s)		15.0		15.0	15.0		30.2	28.0		32.3	31.6	
Actuated g/C Ratio		0.26		0.26	0.26		0.52	0.48		0.55	0.54	
v/c Ratio		0.04		0.65	0.04		0.07	0.67		0.22	0.54	
Control Delay		11.1		28.8	8.6		7.8	17.1		9.2	12.0	
Queue Delay		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Delay		11.1		28.8	8.6		7.8	17.1		9.2	12.0	
LOS		B		C	A		A	B		A	B	
Approach Delay		11.1			27.3			16.9			11.8	
Approach LOS		B			C			B			B	
Queue Length 50th (ft)		2		76	0		3	174		8	102	
Queue Length 95th (ft)		15		139	13		13	#337		27	266	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Internal Link Dist (ft)		120			172			230			842	
Turn Bay Length (ft)				130			140			250		
Base Capacity (vph)		860		723	840		301	1687		260	1909	
Starvation Cap Reductn		0		0	0		0	0		0	0	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.02		0.32	0.02		0.07	0.67		0.22	0.54	

Intersection Summary

Area Type: Other

Cycle Length: 75

Actuated Cycle Length: 58.4

Natural Cycle: 75

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.67

Intersection Signal Delay: 15.7

Intersection LOS: B

Intersection Capacity Utilization 63.8%







ICU Level of Service B

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer.





Queue shown is maximum after two cycles.

Splits and Phases: 7: Locust Street & N. 2nd Street

 Ø1	 Ø2	 Ø4
9.4 s	30.8 s	34.8 s
 Ø5	 Ø6	 Ø8
9.3 s	30.9 s	34.8 s

Intersection

Int Delay, s/veh 0.5

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	21	5	748	22	3	947
Future Vol, veh/h	21	5	748	22	3	947
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	25	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	23	5	813	24	3	1029





Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1346	419	0	0	837
Stage 1	825	-	-	-	-
Stage 2	521	-	-	-	-
Critical Hdwy	6.84	6.94	-	-	4.14
Critical Hdwy Stg 1	5.84	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-
Follow-up Hdwy	3.52	3.32	-	-	2.22
Pot Cap-1 Maneuver	143	583	-	-	793
Stage 1	391	-	-	-	-
Stage 2	561	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	142	583	-	-	793
Mov Cap-2 Maneuver	142	-	-	-	-
Stage 1	387	-	-	-	-
Stage 2	561	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	30.5	0	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1WBLn2	SBL	SBT
Capacity (veh/h)	-	- 142 583	793	-
HCM Lane V/C Ratio	-	- 0.161 0.009	0.004	-
HCM Control Delay (s)	-	- 35.1 11.2	9.6	0
HCM Lane LOS	-	- E B	A	A
HCM 95th %tile Q(veh)	-	- 0.6 0	0	-

Intersection

Int Delay, s/veh 0.7

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	14	7	946	76	15	994
Future Vol, veh/h	14	7	946	76	15	994
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	25	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	15	8	1028	83	16	1080

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1642	556	0	0	1111
Stage 1	1070	-	-	-	-
Stage 2	572	-	-	-	-
Critical Hdwy	6.84	6.94	-	-	4.14
Critical Hdwy Stg 1	5.84	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-
Follow-up Hdwy	3.52	3.32	-	-	2.22
Pot Cap-1 Maneuver	91	475	-	-	624
Stage 1	291	-	-	-	-
Stage 2	528	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	85	475	-	-	624
Mov Cap-2 Maneuver	85	-	-	-	-
Stage 1	272	-	-	-	-
Stage 2	528	-	-	-	-

















Approach	WB	NB	SB
HCM Control Delay, s	41.8	0	0.5
HCM LOS	E		













Minor Lane/Major Mvmt	NBT	NBRWBLn1WBLn2	SBL	SBT
Capacity (veh/h)	-	-	85	475
HCM Lane V/C Ratio	-	-	0.179	0.016
HCM Control Delay (s)	-	-	56.4	12.7
HCM Lane LOS	-	-	F	B
HCM 95th %tile Q(veh)	-	-	0.6	0.1

N. 2nd (US 40/59) & Lyon

Existing Conditions

Morning Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	1	1	116	1	17	1	662	38	7	786	1
Future Volume (vph)	1	1	1	116	1	17	1	662	38	7	786	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.955			0.983			0.992				
Flt Protected		0.984			0.958							
Satd. Flow (prot)	0	1750	0	0	1754	0	0	3511	0	0	3539	0
Flt Permitted		0.907			0.752			0.954			0.948	
Satd. Flow (perm)	0	1613	0	0	1377	0	0	3349	0	0	3355	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			17			14				
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		126			252			660			472	
Travel Time (s)		2.9			5.7			15.0			7.2	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	1	1	1	126	1	18	1	720	41	8	854	1
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	3	0	0	145	0	0	762	0	0	863	0
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2		6	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.0	20.0		24.8	24.8		21.3	21.3		21.3	21.3	
Total Split (s)	20.0	20.0		24.8	24.8		25.2	25.2		25.2	25.2	
Total Split (%)	40.0%	40.0%		49.6%	49.6%		50.4%	50.4%		50.4%	50.4%	
Yellow Time (s)	3.5	3.5		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	0.5	0.5		1.2	1.2		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		4.0			4.4			5.3			5.3	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		Min	Min		Min	Min	
Act Effct Green (s)		9.4			9.1			23.8			23.8	
Actuated g/C Ratio		0.24			0.24			0.62			0.62	
v/c Ratio		0.01			0.43			0.37			0.42	
Control Delay		9.7			15.8			6.4			6.9	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		9.7			15.8			6.4			6.9	
LOS		A			B			A			A	
Approach Delay		9.7			15.8			6.4			6.9	
Approach LOS		A			B			A			A	
Queue Length 50th (ft)		0			24			45			55	
Queue Length 95th (ft)		4			57			92			109	
Internal Link Dist (ft)		46			172			580			392	
Turn Bay Length (ft)												
Base Capacity (vph)		894			756			2086			2084	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Starvation Cap Reductn		0			0			0			0	
Spillback Cap Reductn		0			0			0			0	
Storage Cap Reductn		0			0			0			0	
Reduced v/c Ratio		0.00			0.19			0.37			0.41	

Intersection Summary

Area Type: Other

Cycle Length: 50

Actuated Cycle Length: 38.6

Natural Cycle: 50

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.43

Intersection Signal Delay: 7.4





Intersection LOS: A

Intersection Capacity Utilization 48.9%

ICU Level of Service A

Analysis Period (min) 15

















Splits and Phases: 1: Lyon Street & N. 2nd Street













 Ø2	 Ø4
25.2 s	20 s
 Ø6	 Ø8
25.2 s	24.8 s

N. 2nd (US 40/59) & Lyon

Existing Conditions

Afternoon Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	2	4	2	95	4	21	2	869	96	21	821	1
Future Volume (vph)	2	4	2	95	4	21	2	869	96	21	821	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.966			0.976			0.985				
Flt Protected		0.988			0.962						0.999	
Satd. Flow (prot)	0	1778	0	0	1749	0	0	3486	0	0	3536	0
Flt Permitted		0.923			0.765			0.954			0.917	
Satd. Flow (perm)	0	1661	0	0	1391	0	0	3326	0	0	3245	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			23			28				
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		140			252			660			472	
Travel Time (s)		3.2			5.7			15.0			7.2	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	2	4	2	103	4	23	2	945	104	23	892	1
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	8	0	0	130	0	0	1051	0	0	916	0
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2		6	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.0	20.0		24.8	24.8		21.3	21.3		21.3	21.3	
Total Split (s)	20.0	20.0		24.8	24.8		25.2	25.2		25.2	25.2	
Total Split (%)	40.0%	40.0%		49.6%	49.6%		50.4%	50.4%		50.4%	50.4%	
Yellow Time (s)	3.5	3.5		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	0.5	0.5		1.2	1.2		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		4.0			4.4			5.3			5.3	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		Min	Min		Min	Min	
Act Effct Green (s)		8.9			8.6			26.9			26.9	
Actuated g/C Ratio		0.21			0.21			0.65			0.65	
v/c Ratio		0.02			0.42			0.48			0.43	
Control Delay		10.4			16.4			6.7			6.5	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		10.4			16.4			6.7			6.5	
LOS		B			B			A			A	
Approach Delay		10.4			16.4			6.7			6.5	
Approach LOS		B			B			A			A	
Queue Length 50th (ft)		1			25			67			57	
Queue Length 95th (ft)		7			51			131			113	
Internal Link Dist (ft)		60			172			580			392	
Turn Bay Length (ft)												
Base Capacity (vph)		858			715			2174			2111	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Starvation Cap Reductn		0			0			0			0	
Spillback Cap Reductn		0			0			0			0	
Storage Cap Reductn		0			0			0			0	
Reduced v/c Ratio		0.01			0.18			0.48			0.43	

Intersection Summary

Area Type: Other

Cycle Length: 50

Actuated Cycle Length: 41.4

Natural Cycle: 55

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.48

Intersection Signal Delay: 7.2





Intersection LOS: A

Intersection Capacity Utilization 59.3%

ICU Level of Service B

Analysis Period (min) 15





















Splits and Phases: 1: Lyon Street & N. 2nd Street

 Ø2	 Ø4
25.2 s	20 s
 Ø6	 Ø8
25.2 s	24.8 s

**“Existing + Proposed Redevelopment (Building I-VII & Hotel)”
Traffic Conditions**













N. 2nd (US 40/59) & Locust "Existing + Project (Buildings I-VII and Hotel)" Traffic Conditions

Morning Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	114	15	60	251	24	19	135	751	26	18	931	137
Future Volume (vph)	114	15	60	251	24	19	135	751	26	18	931	137
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	125		0	130		0	140		0	250		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.880			0.933			0.995			0.981	
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	1639	0	1770	1738	0	1770	3522	0	1770	3472	0
Flt Permitted	0.726			0.704			0.135			0.306		
Satd. Flow (perm)	1352	1639	0	1311	1738	0	251	3522	0	570	3472	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		65			21			7			32	
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		200			252			310			922	
Travel Time (s)		4.5			5.7			7.0			14.0	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	124	16	65	273	26	21	147	816	28	20	1012	149
Shared Lane Traffic (%)												
Lane Group Flow (vph)	124	81	0	273	47	0	147	844	0	20	1161	0
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.8	20.8		20.8	20.8		9.3	21.3		9.3	21.3	
Total Split (s)	21.1	21.1		21.1	21.1		9.6	29.6		9.3	29.3	
Total Split (%)	35.2%	35.2%		35.2%	35.2%		16.0%	49.3%		15.5%	48.8%	
Yellow Time (s)	3.2	3.2		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	1.6	1.6		1.6	1.6		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	4.8	4.8		4.8	4.8		5.3	5.3		5.3	5.3	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None		None	None		None	Min		None	Min	
Act Effect Green (s)	14.8	14.8		14.8	14.8		30.5	29.7		27.2	24.2	
Actuated g/C Ratio	0.26	0.26		0.26	0.26		0.54	0.53		0.48	0.43	
v/c Ratio	0.35	0.17		0.80	0.10		0.58	0.46		0.06	0.77	
Control Delay	21.1	7.8		40.1	11.7		19.7	10.2		6.5	19.4	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Delay	21.1	7.8		40.1	11.7		19.7	10.2		6.5	19.4	
LOS	C	A		D	B		B	B		A	B	
Approach Delay		15.8			35.9			11.6			19.1	
Approach LOS		B			D			B			B	
Queue Length 50th (ft)	36	4		90	7		23	82		3	184	
Queue Length 95th (ft)	76	31		#201	28		#81	170		10	#272	

N. 2nd (US 40/59) & Locust "Existing + Project (Buildings I-VII and Hotel)" Traffic Conditions

Morning Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Internal Link Dist (ft)	120			172			230			842		
Turn Bay Length (ft)	125			130			140			250		
Base Capacity (vph)	396	526		384	524		253	1873		361	1518	
Starvation Cap Reductn	0	0		0	0		0	0		0	0	
Spillback Cap Reductn	0	0		0	0		0	0		0	0	
Storage Cap Reductn	0	0		0	0		0	0		0	0	
Reduced v/c Ratio	0.31	0.15		0.71	0.09		0.58	0.45		0.06	0.76	

Intersection Summary

Area Type: Other

Cycle Length: 60

Actuated Cycle Length: 56.5

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.80

Intersection Signal Delay: 18.1

Intersection LOS: B

Intersection Capacity Utilization 71.0%




ICU Level of Service C

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer.





















Queue shown is maximum after two cycles.

Splits and Phases: 7: Locust Street & N. 2nd Street

 Ø1	 Ø2	 Ø4
9.3 s	29.6 s	21.1 s
 Ø5	 Ø6	 Ø8
9.6 s	29.3 s	21.1 s













N. 2nd (US 40/59) & Locust "Existing + Project (Buildings I - VII and Hotel)" Traffic Conditions

Afternoon Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	151	24	76	214	30	16	180	1002	45	52	947	175
Future Volume (vph)	151	24	76	214	30	16	180	1002	45	52	947	175
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	125		0	130		0	140		0	250		0
Storage Lanes	1		0	1		0	1		0	1		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt		0.886			0.949			0.994			0.977	
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	1650	0	1770	1768	0	1770	3518	0	1770	3458	0
Flt Permitted	0.724			0.687			0.134			0.169		
Satd. Flow (perm)	1349	1650	0	1280	1768	0	250	3518	0	315	3458	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		83			17			6			30	
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		200			252			310			922	
Travel Time (s)		4.5			5.7			7.0			14.0	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	164	26	83	233	33	17	196	1089	49	57	1029	190
Shared Lane Traffic (%)												
Lane Group Flow (vph)	164	109	0	233	50	0	196	1138	0	57	1219	0
Turn Type	Perm	NA		Perm	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	34.8	34.8		34.1	34.1		9.3	27.4		9.3	21.3	
Total Split (s)	34.8	34.8		34.8	34.8		10.6	30.8		9.4	29.6	
Total Split (%)	46.4%	46.4%		46.4%	46.4%		14.1%	41.1%		12.5%	39.5%	
Yellow Time (s)	3.2	3.2		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	1.6	1.6		1.6	1.6		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	4.8	4.8		4.8	4.8		5.3	5.3		5.3	5.3	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?								Yes		Yes		
Recall Mode	None	None		None	None		None	Min		None	Min	
Act Effct Green (s)	16.3	16.3		16.3	16.3		32.9	29.8		28.6	24.5	
Actuated g/C Ratio	0.26	0.26		0.26	0.26		0.53	0.48		0.46	0.40	
v/c Ratio	0.46	0.22		0.69	0.10		0.74	0.67		0.23	0.88	
Control Delay	22.8	7.5		31.2	12.2		31.7	17.9		10.5	27.7	
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Delay	22.8	7.5		31.2	12.2		31.7	17.9		10.5	27.7	
LOS	C	A		C	B		C	B		B	C	
Approach Delay		16.7			27.8			20.0			26.9	
Approach LOS		B			C			B			C	
Queue Length 50th (ft)	51	7		77	9		31	180		8	205	
Queue Length 95th (ft)	97	37		142	30		#156	#363		29	#418	

N. 2nd (US 40/59) & Locust "Existing + Project (Buildings I - VII and Hotel)" Traffic Conditions

Afternoon Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Internal Link Dist (ft)	120			172			230			842		
Turn Bay Length (ft)	125			130			140			250		
Base Capacity (vph)	661	851		627	875		264	1704		243	1391	
Starvation Cap Reductn	0	0		0	0		0	0		0	0	
Spillback Cap Reductn	0	0		0	0		0	0		0	0	
Storage Cap Reductn	0	0		0	0		0	0		0	0	
Reduced v/c Ratio	0.25	0.13		0.37	0.06		0.74	0.67		0.23	0.88	

Intersection Summary

Area Type: Other

Cycle Length: 75

Actuated Cycle Length: 61.7

Natural Cycle: 90

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.88

Intersection Signal Delay: 23.2

Intersection LOS: C

Intersection Capacity Utilization 73.1%







ICU Level of Service D

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer.





Queue shown is maximum after two cycles.

Splits and Phases: 7: Locust Street & N. 2nd Street

 Ø1	 Ø2	 Ø4
9.4 s	30.8 s	34.8 s
 Ø5	 Ø6	 Ø8
10.6 s	29.6 s	34.8 s





N. 2nd (US 40/59) & Lincoln "Existing + Project (Buildings I-VII and Hotel)" Traffic Conditions

















Morning Peak-Hour













Intersection						
Int Delay, s/veh	0.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	21	5	860	25	3	1079
Future Vol, veh/h	21	5	860	25	3	1079
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	25	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	23	5	935	27	3	1173
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	1542	481	0	0	962	0
Stage 1	949	-	-	-	-	-
Stage 2	593	-	-	-	-	-
Critical Hdwy	6.84	6.94	-	-	4.14	-
Critical Hdwy Stg 1	5.84	-	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-	-
Follow-up Hdwy	3.52	3.32	-	-	2.22	-
Pot Cap-1 Maneuver	106	531	-	-	711	-
Stage 1	337	-	-	-	-	-
Stage 2	515	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	105	531	-	-	711	-
Mov Cap-2 Maneuver	105	-	-	-	-	-
Stage 1	333	-	-	-	-	-
Stage 2	515	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	41.5	0		0.1		
HCM LOS	E					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	WBLn2	SBL	SBT	
Capacity (veh/h)	-	-	105	531	711	-
HCM Lane V/C Ratio	-	-	0.217	0.01	0.005	-
HCM Control Delay (s)	-	-	48.6	11.9	10.1	0.1
HCM Lane LOS	-	-	E	B	B	A
HCM 95th %tile Q(veh)	-	-	0.8	0	0	-

N. 2nd (US 40/59) & Lincoln "Existing + Project (Buildings I - VII and Hotel)" Traffic Conditions

Afternoon Peak-Hour

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	14	7	1089	80	15	1154
Future Vol, veh/h	14	7	1089	80	15	1154
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	25	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	15	8	1184	87	16	1254
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1887	636	0	0	1271	0
Stage 1	1228	-	-	-	-	-
Stage 2	659	-	-	-	-	-
Critical Hdwy	6.84	6.94	-	-	4.14	-
Critical Hdwy Stg 1	5.84	-	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-	-
Follow-up Hdwy	3.52	3.32	-	-	2.22	-
Pot Cap-1 Maneuver	62	421	-	-	542	-
Stage 1	240	-	-	-	-	-
Stage 2	476	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	56	421	-	-	542	-
Mov Cap-2 Maneuver	56	-	-	-	-	-
Stage 1	217	-	-	-	-	-
Stage 2	476	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	65.7	0	0.7			
HCM LOS	F					
Minor Lane/Major Mvmt	NBT	NBRWBLn1WBLn2	SBL	SBT		
Capacity (veh/h)	-	- 56 421	542	-		
HCM Lane V/C Ratio	-	- 0.272 0.018	0.03	-		
HCM Control Delay (s)	-	- 91.7 13.7	11.8	0.6		
HCM Lane LOS	-	- F B	B	A		
HCM 95th %tile Q(veh)	-	- 0.9 0.1	0.1	-		

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	1	1	1	135	1	17	1	760	52	7	899	1
Future Volume (vph)	1	1	1	135	1	17	1	760	52	7	899	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.955			0.985			0.990				
Flt Protected		0.984			0.958							
Satd. Flow (prot)	0	1750	0	0	1758	0	0	3504	0	0	3539	0
Flt Permitted		0.910			0.748			0.954			0.947	
Satd. Flow (perm)	0	1619	0	0	1372	0	0	3343	0	0	3352	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			15			17				
Link Speed (mph)		30			30			30			45	
Link Distance (ft)		126			252			660			472	
Travel Time (s)		2.9			5.7			15.0			7.2	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	1	1	1	147	1	18	1	826	57	8	977	1
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	3	0	0	166	0	0	884	0	0	986	0
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2		6	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.0	20.0		24.8	24.8		21.3	21.3		21.3	21.3	
Total Split (s)	20.0	20.0		24.8	24.8		25.2	25.2		25.2	25.2	
Total Split (%)	40.0%	40.0%		49.6%	49.6%		50.4%	50.4%		50.4%	50.4%	
Yellow Time (s)	3.5	3.5		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	0.5	0.5		1.2	1.2		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		4.0			4.4			5.3			5.3	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		Min	Min		Min	Min	
Act Effect Green (s)		10.0			9.8			24.3			24.3	
Actuated g/C Ratio		0.25			0.25			0.61			0.61	
v/c Ratio		0.01			0.48			0.43			0.48	
Control Delay		9.3			16.8			7.3			7.8	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		9.3			16.8			7.3			7.8	
LOS		A			B			A			A	
Approach Delay		9.3			16.8			7.3			7.8	
Approach LOS		A			B			A			A	
Queue Length 50th (ft)		0			30			58			70	
Queue Length 95th (ft)		4			65			117			139	
Internal Link Dist (ft)		46			172			580			392	
Turn Bay Length (ft)												
Base Capacity (vph)		868			729			2050			2048	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Starvation Cap Reductn		0			0			0			0	
Spillback Cap Reductn		0			0			0			0	
Storage Cap Reductn		0			0			0			0	
Reduced v/c Ratio		0.00			0.23			0.43			0.48	

Intersection Summary

Area Type: Other

Cycle Length: 50

Actuated Cycle Length: 39.8

Natural Cycle: 50

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.48

Intersection Signal Delay: 8.3





Intersection LOS: A

















Intersection Capacity Utilization 53.1%

ICU Level of Service A

Analysis Period (min) 15













Splits and Phases: 1: Lyon Street & N. 2nd Street

 Ø2	 Ø4
25.2 s	20 s
 Ø6	 Ø8
25.2 s	24.8 s

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	2	4	2	121	4	21	2	990	118	21	955	1
Future Volume (vph)	2	4	2	121	4	21	2	990	118	21	955	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	0.95	0.95	0.95	0.95
Frt		0.966			0.980			0.984				
Flt Protected		0.988			0.960						0.999	
Satd. Flow (prot)	0	1778	0	0	1752	0	0	3483	0	0	3536	0
Flt Permitted		0.925			0.757			0.954			0.915	
Satd. Flow (perm)	0	1664	0	0	1382	0	0	3322	0	0	3238	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			21			30				
Link Speed (mph)		30			30			30				45
Link Distance (ft)		140			252			660				472
Travel Time (s)		3.2			5.7			15.0				7.2
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	2	4	2	132	4	23	2	1076	128	23	1038	1
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	8	0	0	159	0	0	1206	0	0	1062	0
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Detector Phase	4	4		8	8		2	2		6	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0		4.0	4.0		4.0	4.0		4.0	4.0	
Minimum Split (s)	20.0	20.0		24.8	24.8		21.3	21.3		21.3	21.3	
Total Split (s)	20.0	20.0		24.8	24.8		25.2	25.2		25.2	25.2	
Total Split (%)	40.0%	40.0%		49.6%	49.6%		50.4%	50.4%		50.4%	50.4%	
Yellow Time (s)	3.5	3.5		3.2	3.2		4.3	4.3		4.3	4.3	
All-Red Time (s)	0.5	0.5		1.2	1.2		1.0	1.0		1.0	1.0	
Lost Time Adjust (s)		0.0			0.0			0.0			0.0	
Total Lost Time (s)		4.0			4.4			5.3			5.3	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		None	None		Min	Min		Min	Min	
Act Effect Green (s)		9.8			9.5			26.7			26.7	
Actuated g/C Ratio		0.23			0.23			0.63			0.63	
v/c Ratio		0.02			0.49			0.57			0.52	
Control Delay		9.7			17.5			8.6			7.9	
Queue Delay		0.0			0.0			0.0			0.0	
Total Delay		9.7			17.5			8.6			7.9	
LOS		A			B			A			A	
Approach Delay		9.7			17.5			8.6			7.9	
Approach LOS		A			B			A			A	
Queue Length 50th (ft)		1			30			87			75	
Queue Length 95th (ft)		7			61			#179			154	
Internal Link Dist (ft)		60			172			580			392	
Turn Bay Length (ft)												
Base Capacity (vph)		837			691			2112			2048	

N. 2nd (US 40/59) & Lyon "Existing + Project (Buildings I - VII and Hotel)" Traffic Conditions

Afternoon Peak-Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Starvation Cap Reductn		0			0			0			0	
Spillback Cap Reductn		0			0			0			0	
Storage Cap Reductn		0			0			0			0	
Reduced v/c Ratio		0.01			0.23			0.57			0.52	

Intersection Summary

Area Type: Other

Cycle Length: 50

Actuated Cycle Length: 42.2

Natural Cycle: 60

Control Type: Actuated-Uncoordinated

Maximum v/c Ratio: 0.57

Intersection Signal Delay: 8.9

Intersection LOS: A

Intersection Capacity Utilization 64.4%

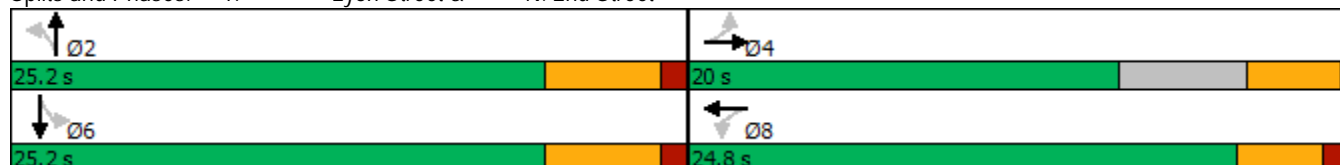
ICU Level of Service C

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 1: Lyon Street & N. 2nd Street



APPENDIX III

Results of Trip Generation Analysis
Using
ITE Trip Generation Manual, 10th Edition

Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

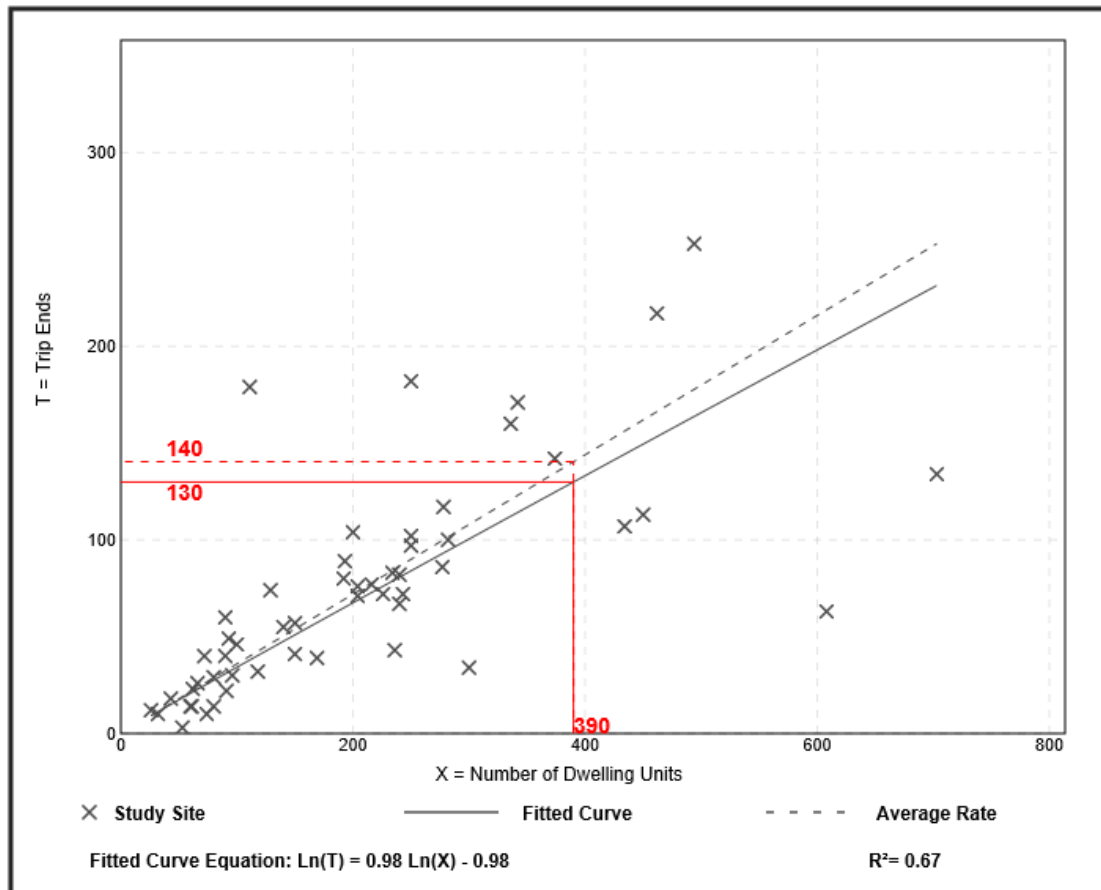
Avg. Num. of Dwelling Units: 207

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.06 - 1.61	0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 60

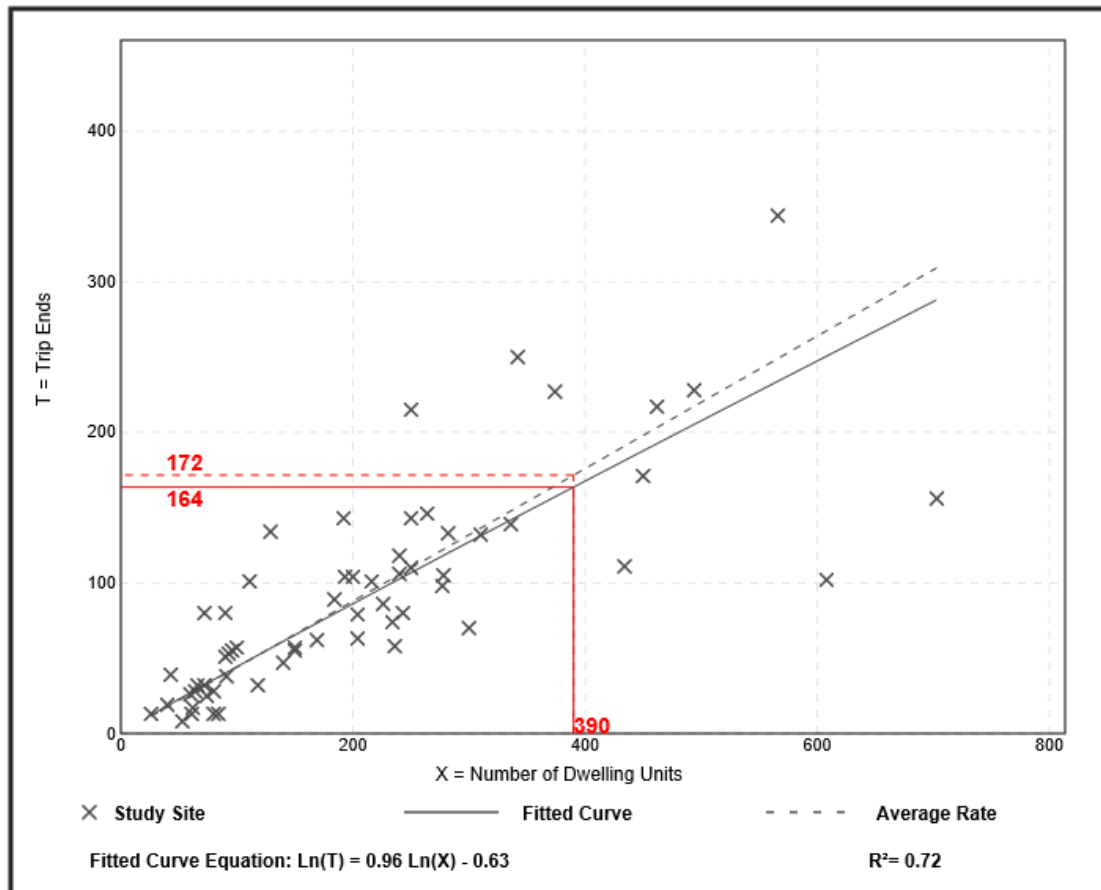
Avg. Num. of Dwelling Units: 208

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

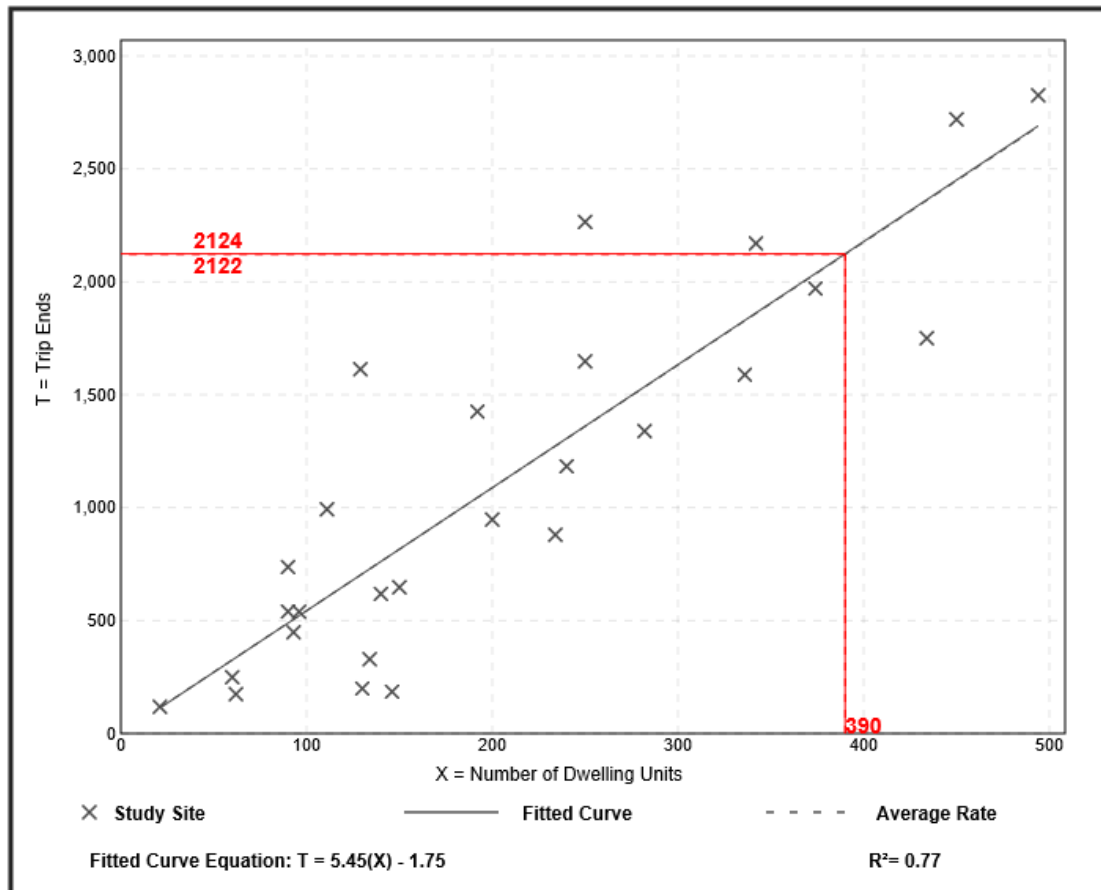
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 27
Avg. Num. of Dwelling Units: 205
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

Data Plot and Equation



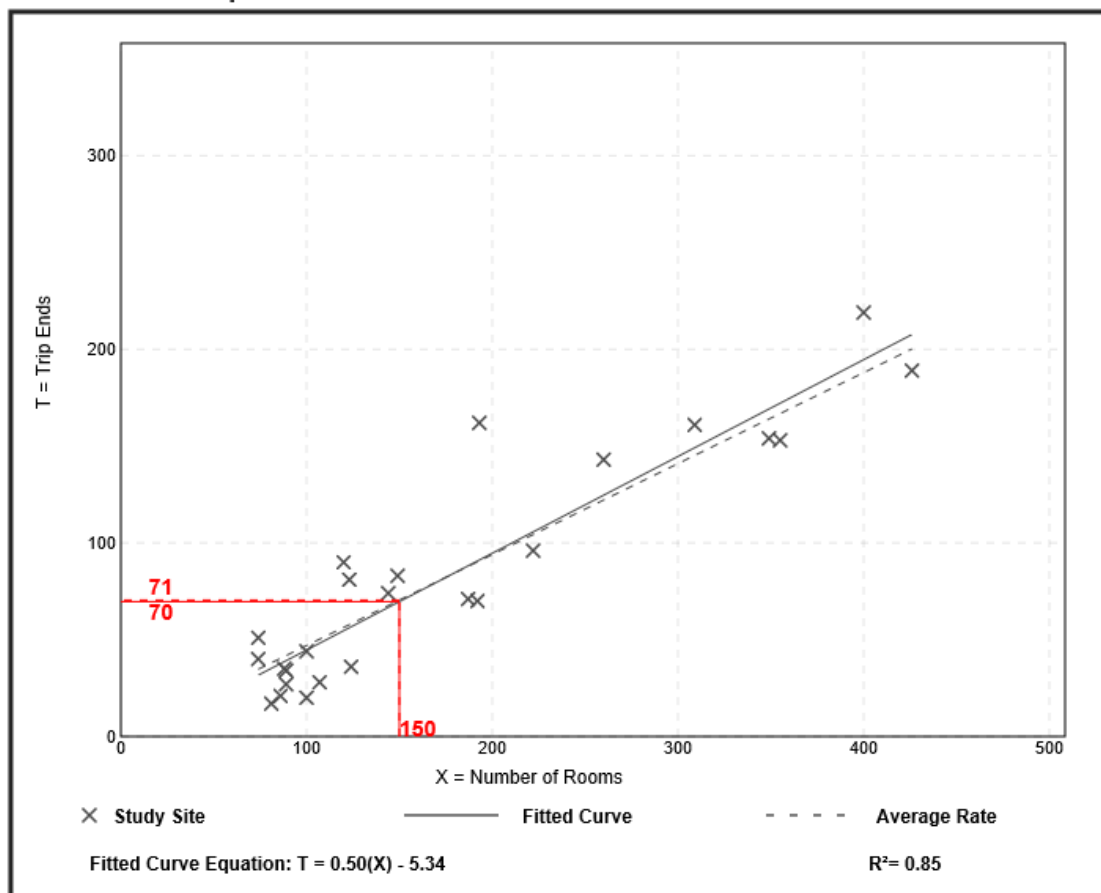
Hotel (310)

Vehicle Trip Ends vs: Rooms
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 25
 Avg. Num. of Rooms: 178
 Directional Distribution: 59% entering, 41% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.47	0.20 - 0.84	0.14

Data Plot and Equation



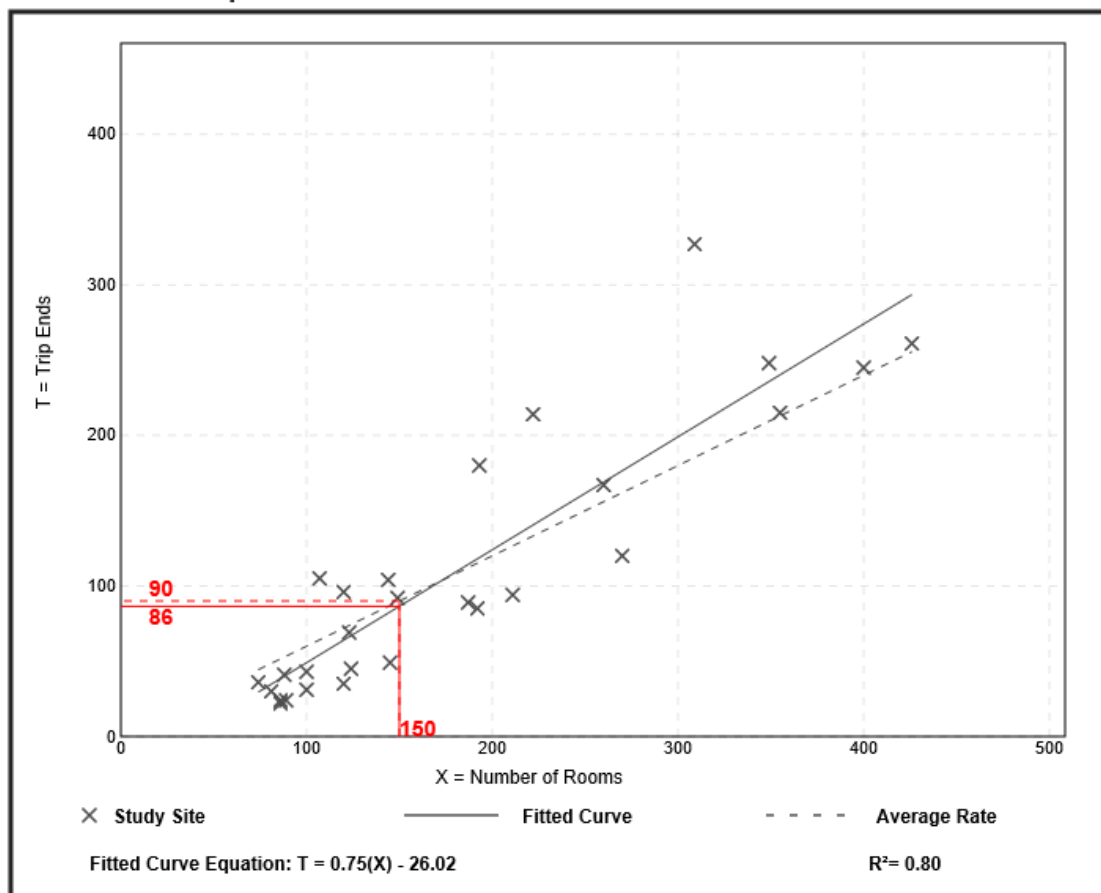
Hotel (310)

Vehicle Trip Ends vs: Rooms
On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 28
 Avg. Num. of Rooms: 183
 Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.60	0.26 - 1.06	0.22

Data Plot and Equation



Hotel (310)

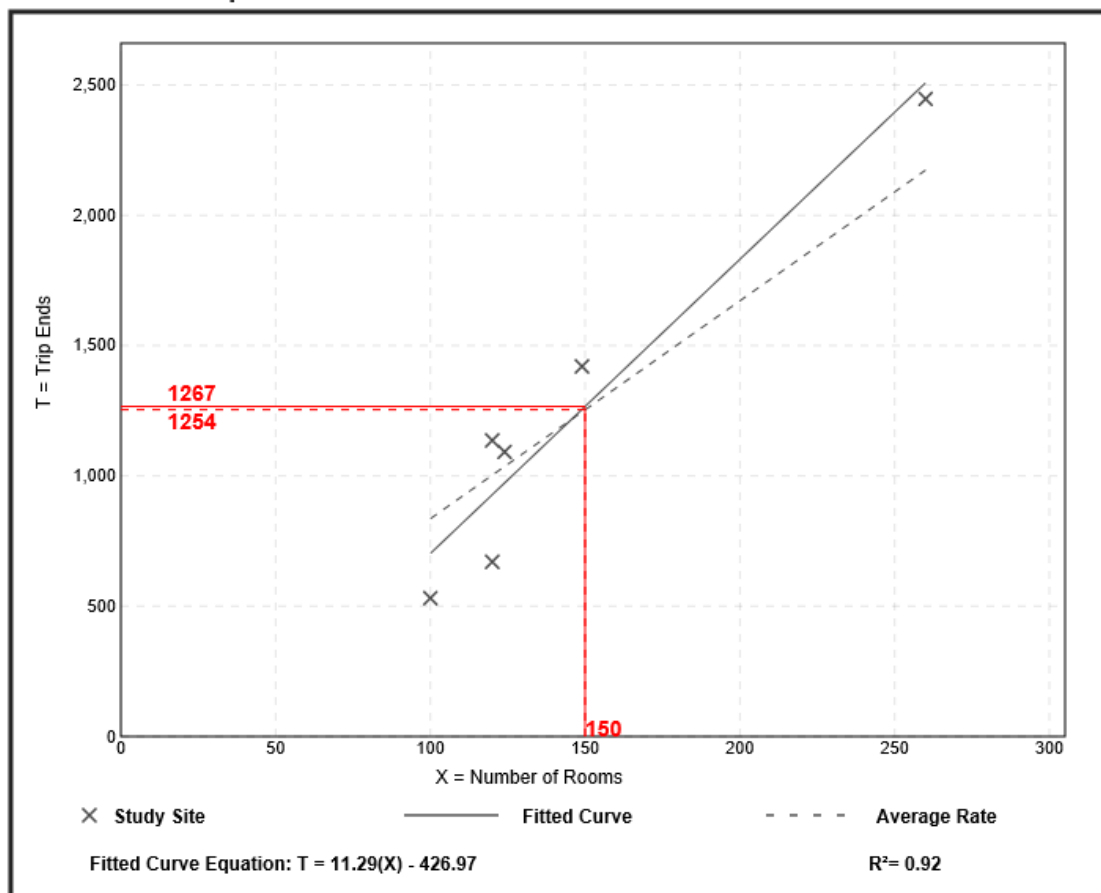
Vehicle Trip Ends vs: Rooms
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 6
Avg. Num. of Rooms: 146
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
8.36	5.31 - 9.53	1.86

Data Plot and Equation



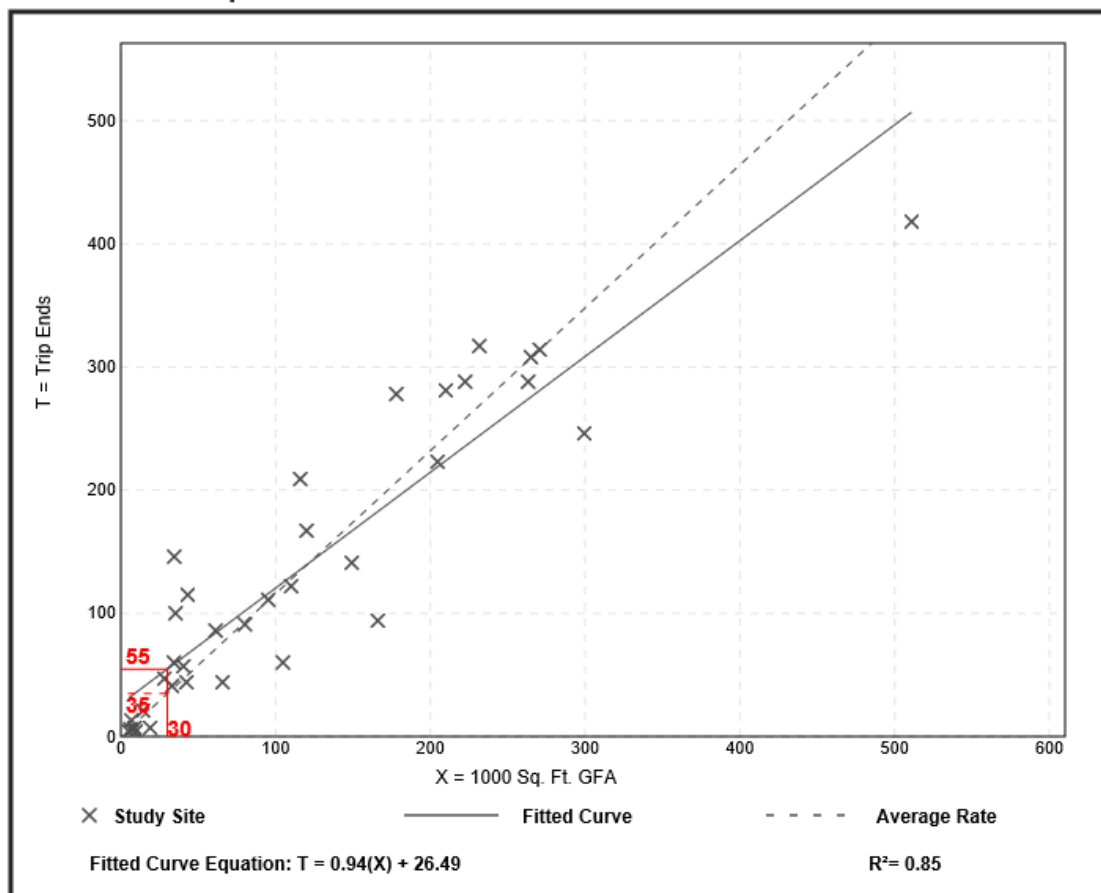
General Office Building (710)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 35
 Avg. 1000 Sq. Ft. GFA: 117
 Directional Distribution: 86% entering, 14% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.16	0.37 - 4.23	0.47

Data Plot and Equation



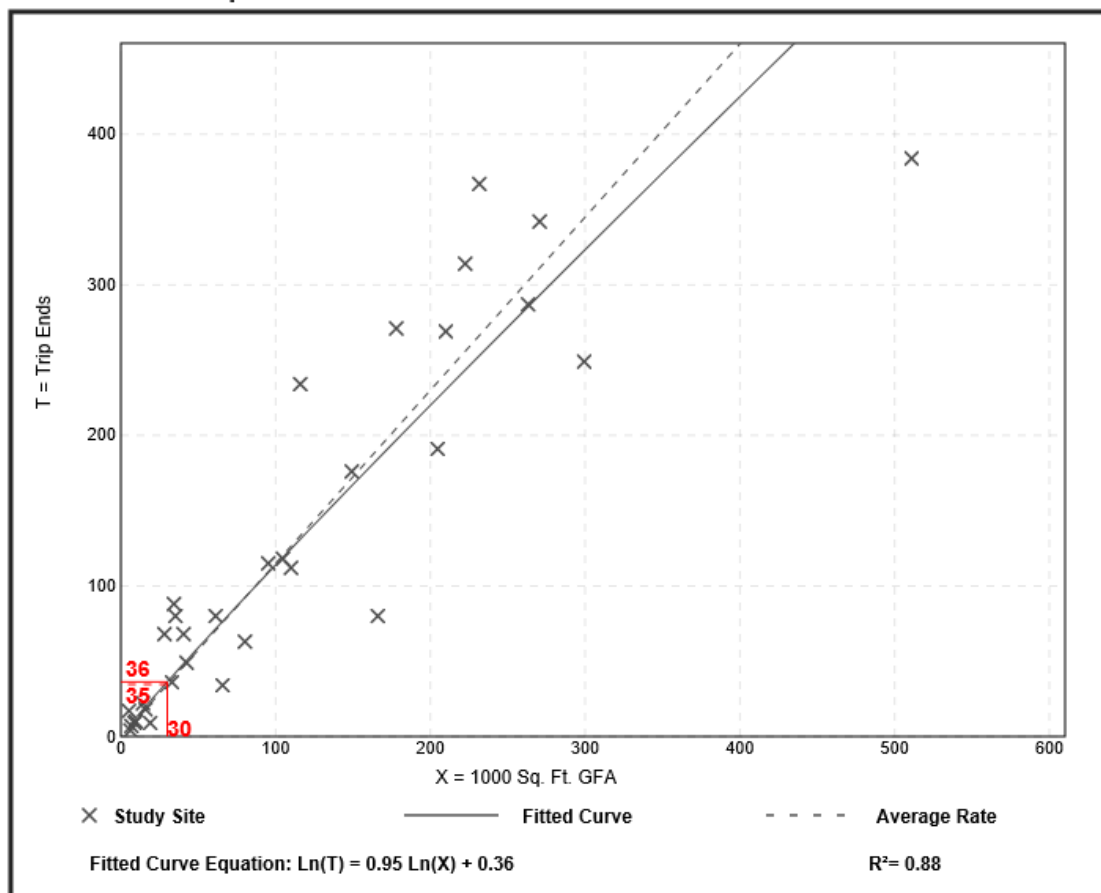
General Office Building (710)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 32
 Avg. 1000 Sq. Ft. GFA: 114
 Directional Distribution: 16% entering, 84% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.15	0.47 - 3.23	0.42

Data Plot and Equation



General Office Building (710)

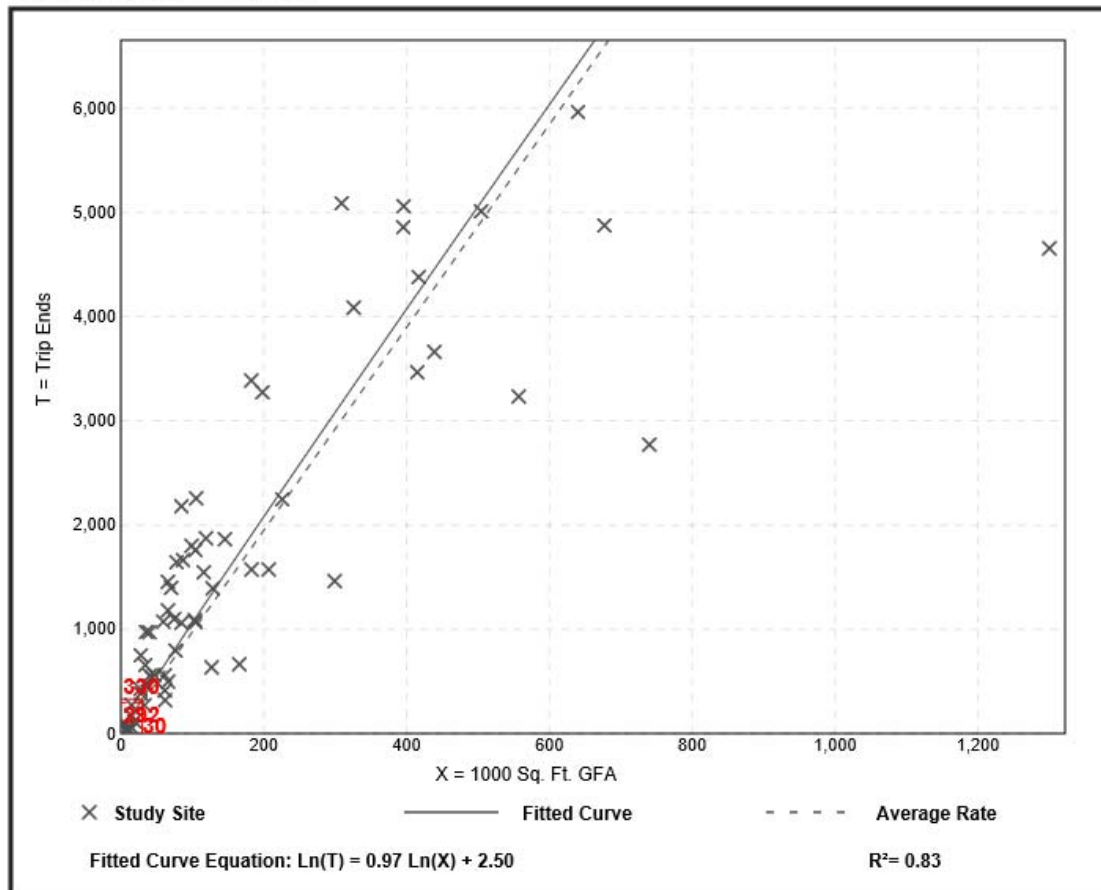
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 66
Avg. 1000 Sq. Ft. GFA: 171
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.74	2.71 - 27.56	5.15

Data Plot and Equation



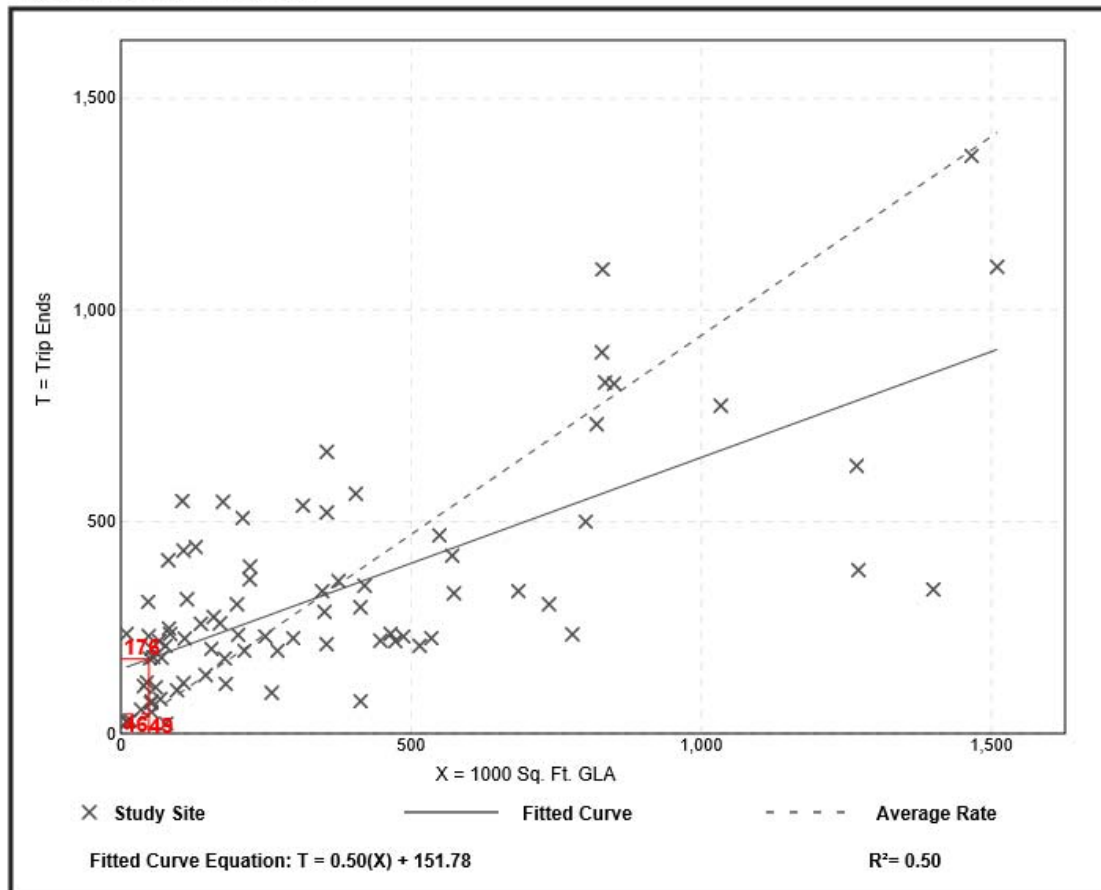
Shopping Center (820)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 84
 Avg. 1000 Sq. Ft. GLA: 351
 Directional Distribution: 62% entering, 38% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
0.94	0.18 - 23.74	0.87

Data Plot and Equation



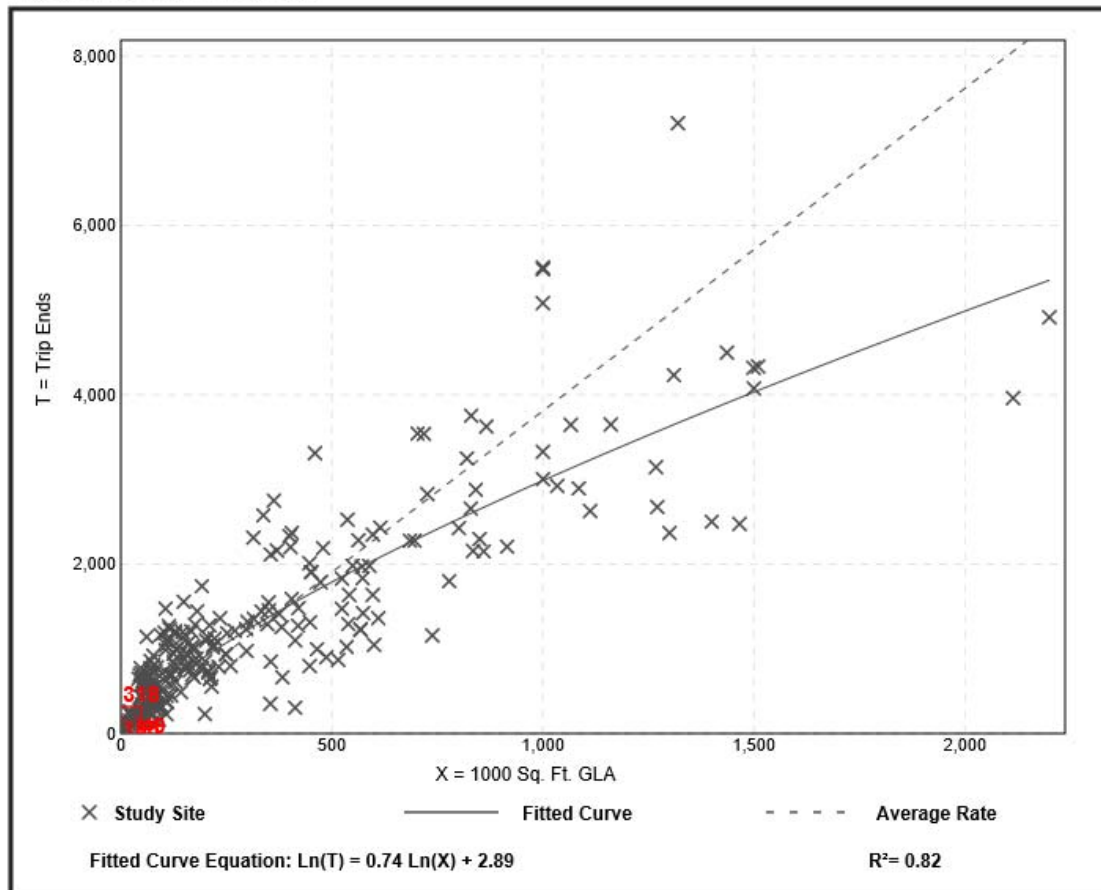
Shopping Center (820)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 261
 Avg. 1000 Sq. Ft. GLA: 327
 Directional Distribution: 48% entering, 52% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
3.81	0.74 - 18.69	2.04

Data Plot and Equation



Shopping Center (820)

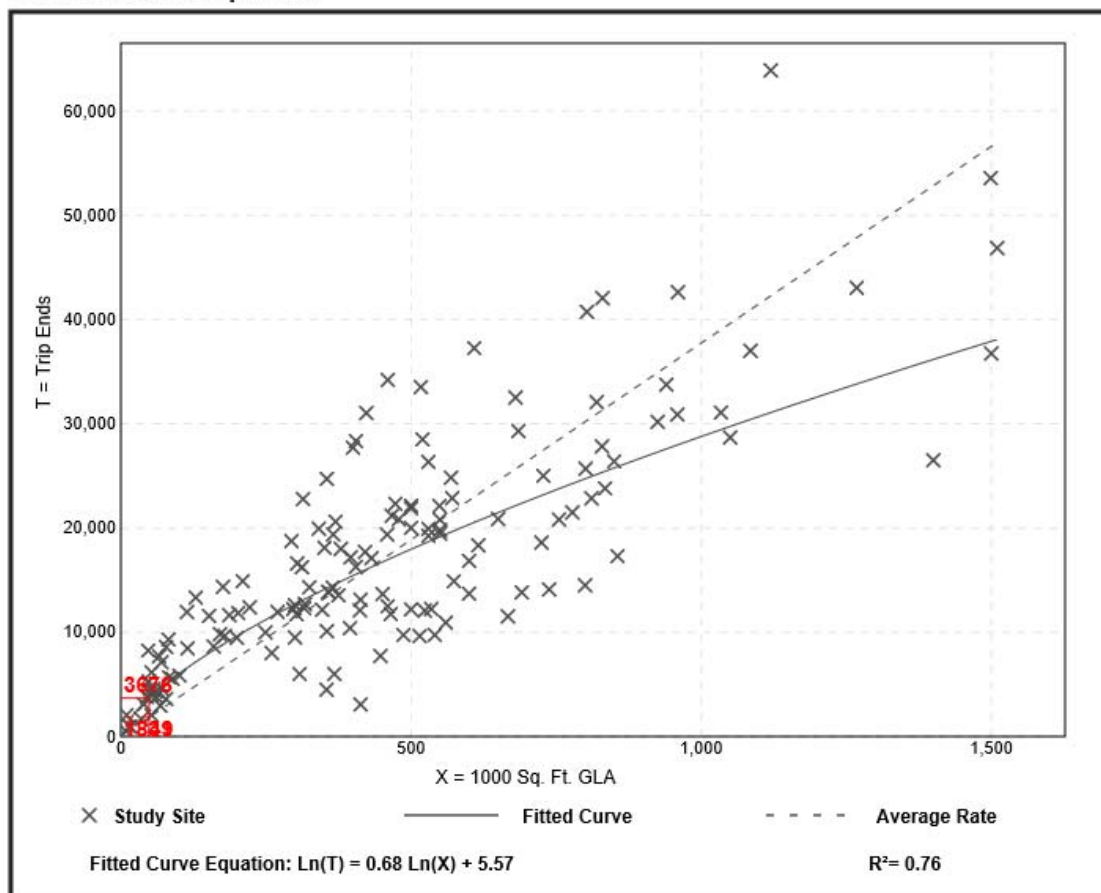
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 147
Avg. 1000 Sq. Ft. GLA: 453
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
37.75	7.42 - 207.98	16.41

Data Plot and Equation



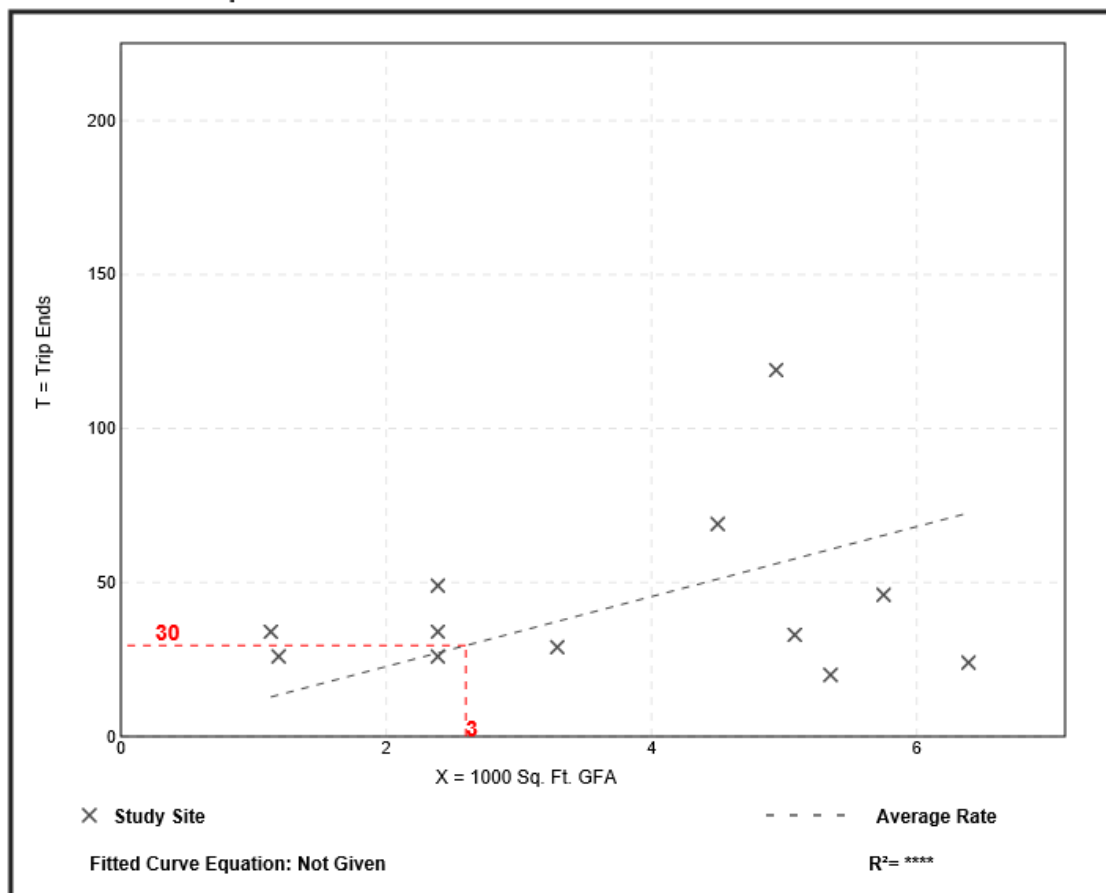
Drinking Place (925)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 12
 Avg. 1000 Sq. Ft. GFA: 4
 Directional Distribution: 66% entering, 34% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
11.36	3.74 - 30.09	7.81

Data Plot and Equation



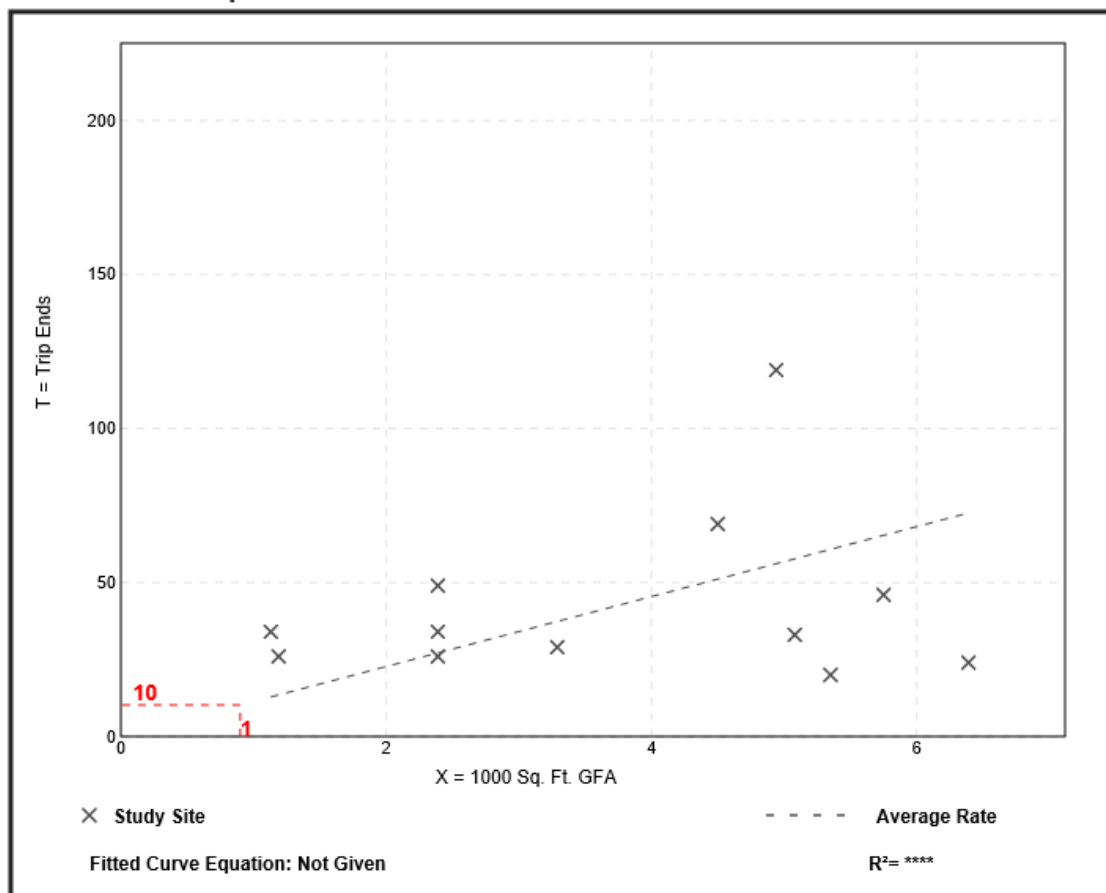
Drinking Place (925)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 12
 Avg. 1000 Sq. Ft. GFA: 4
 Directional Distribution: 66% entering, 34% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
11.36	3.74 - 30.09	7.81

Data Plot and Equation



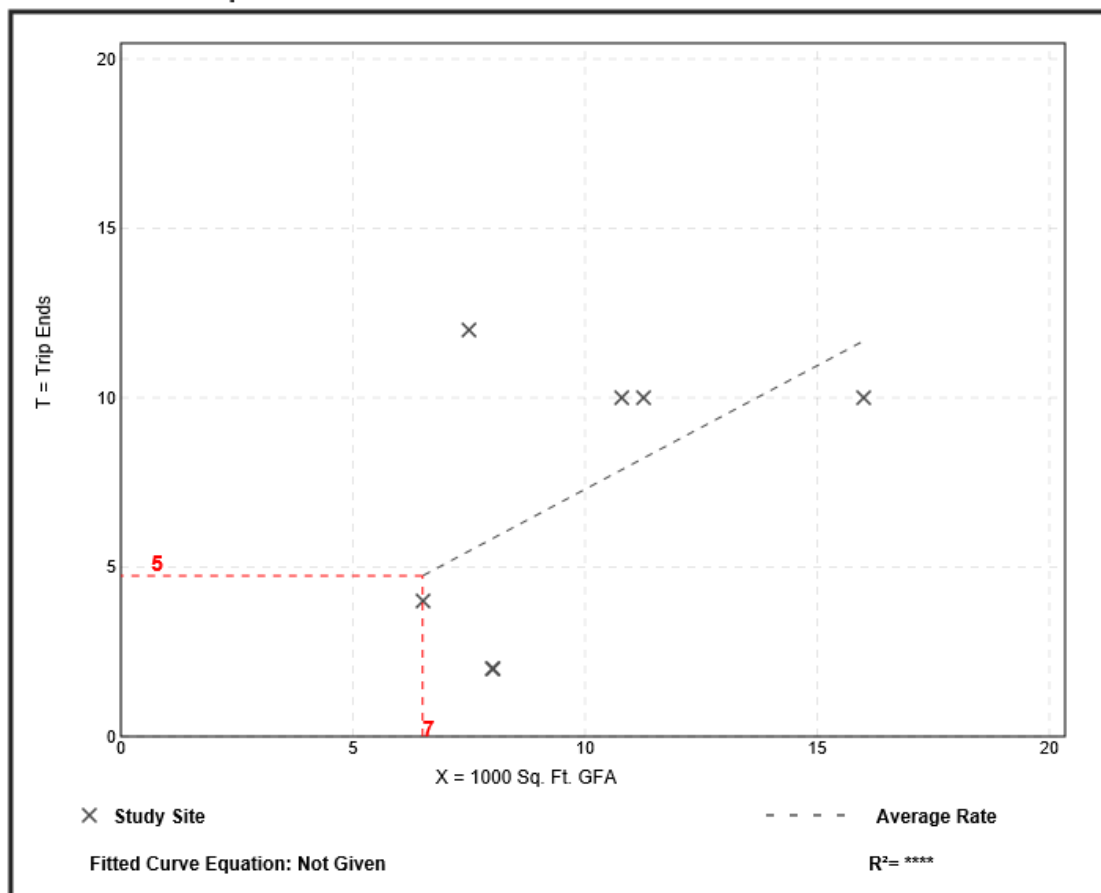
Quality Restaurant (931)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 7
 Avg. 1000 Sq. Ft. GFA: 10
 Directional Distribution: Not Available

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.73	0.25 - 1.60	0.42

Data Plot and Equation



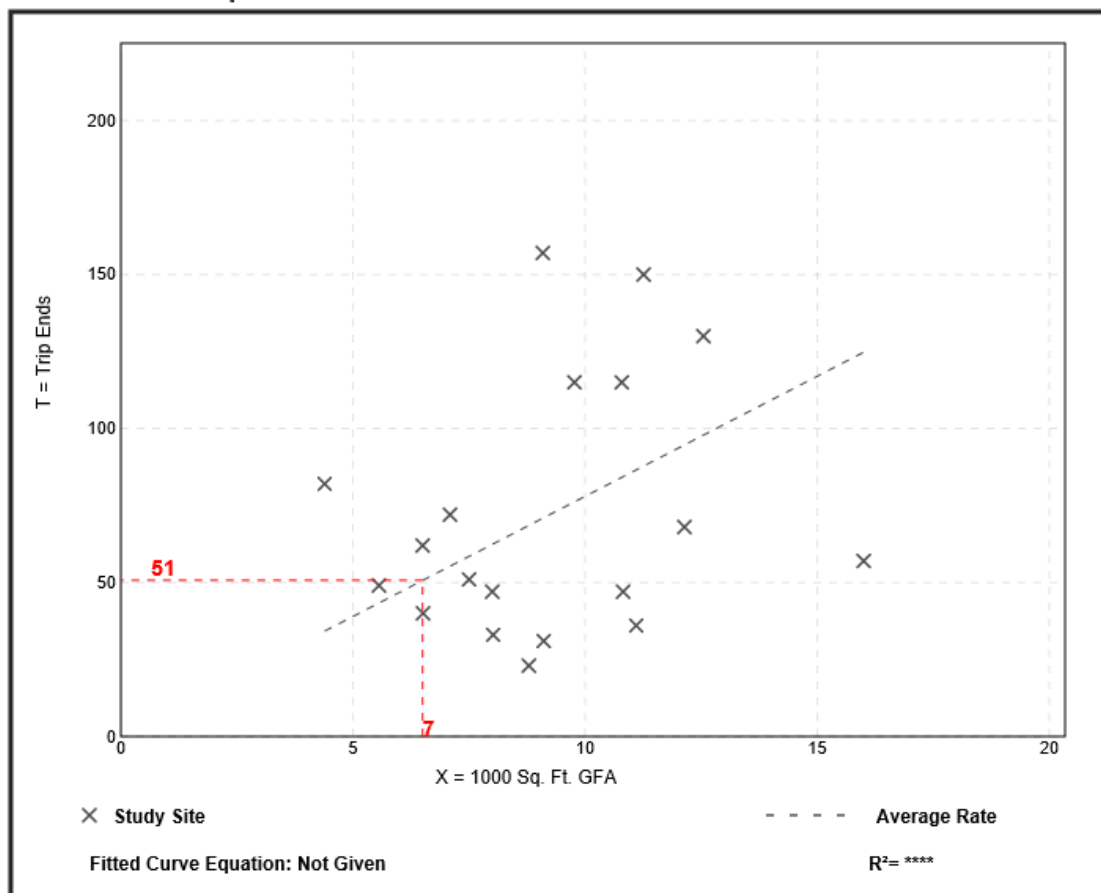
Quality Restaurant (931)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 19
 Avg. 1000 Sq. Ft. GFA: 9
 Directional Distribution: 67% entering, 33% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
7.80	2.62 - 18.68	4.49

Data Plot and Equation



Quality Restaurant (931)

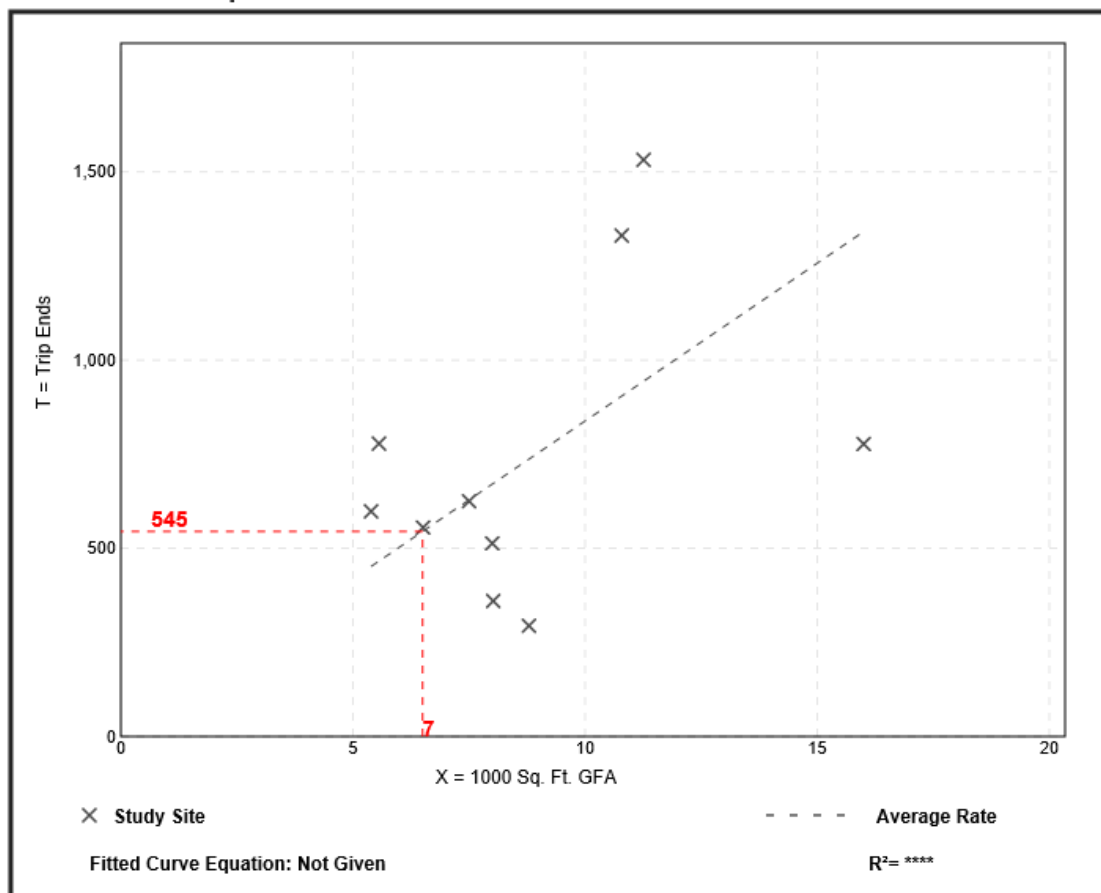
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 10
Avg. 1000 Sq. Ft. GFA: 9
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
83.84	33.45 - 139.93	40.01

Data Plot and Equation



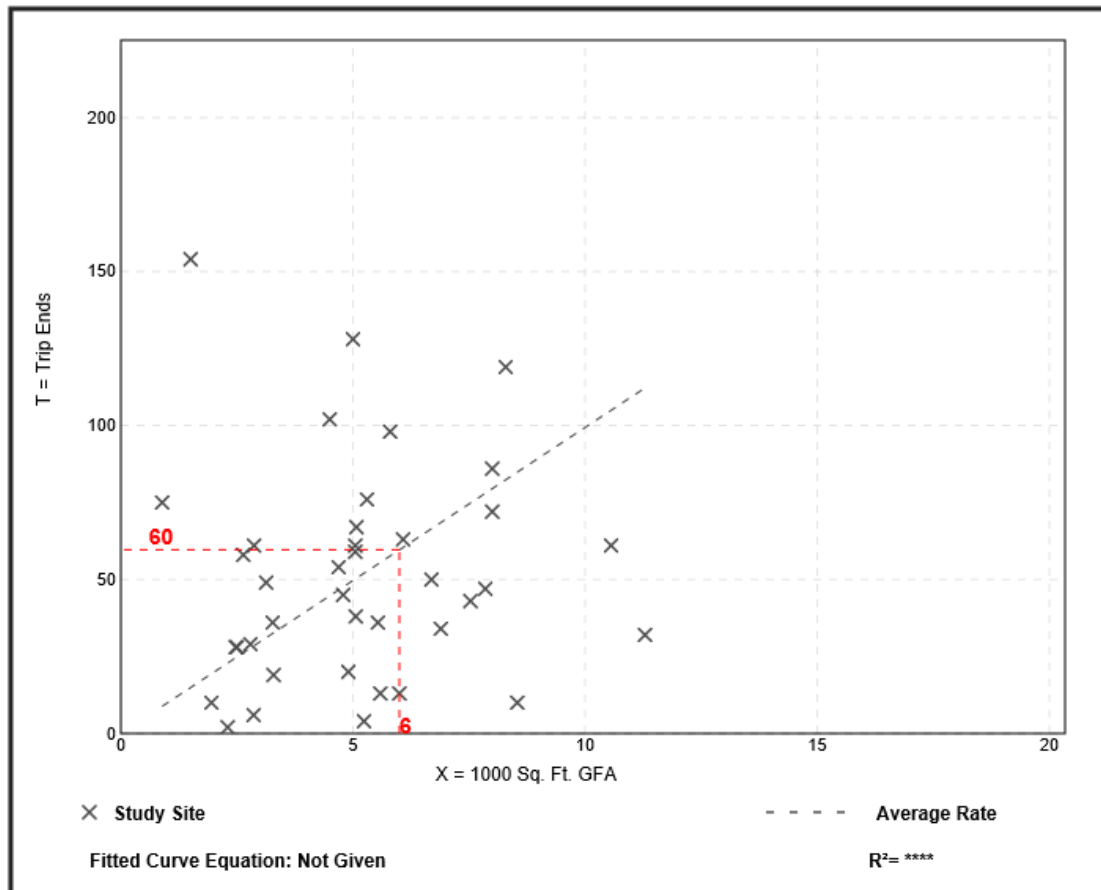
High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 39
 Avg. 1000 Sq. Ft. GFA: 5
 Directional Distribution: 55% entering, 45% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.94	0.76 - 102.39	11.33

Data Plot and Equation



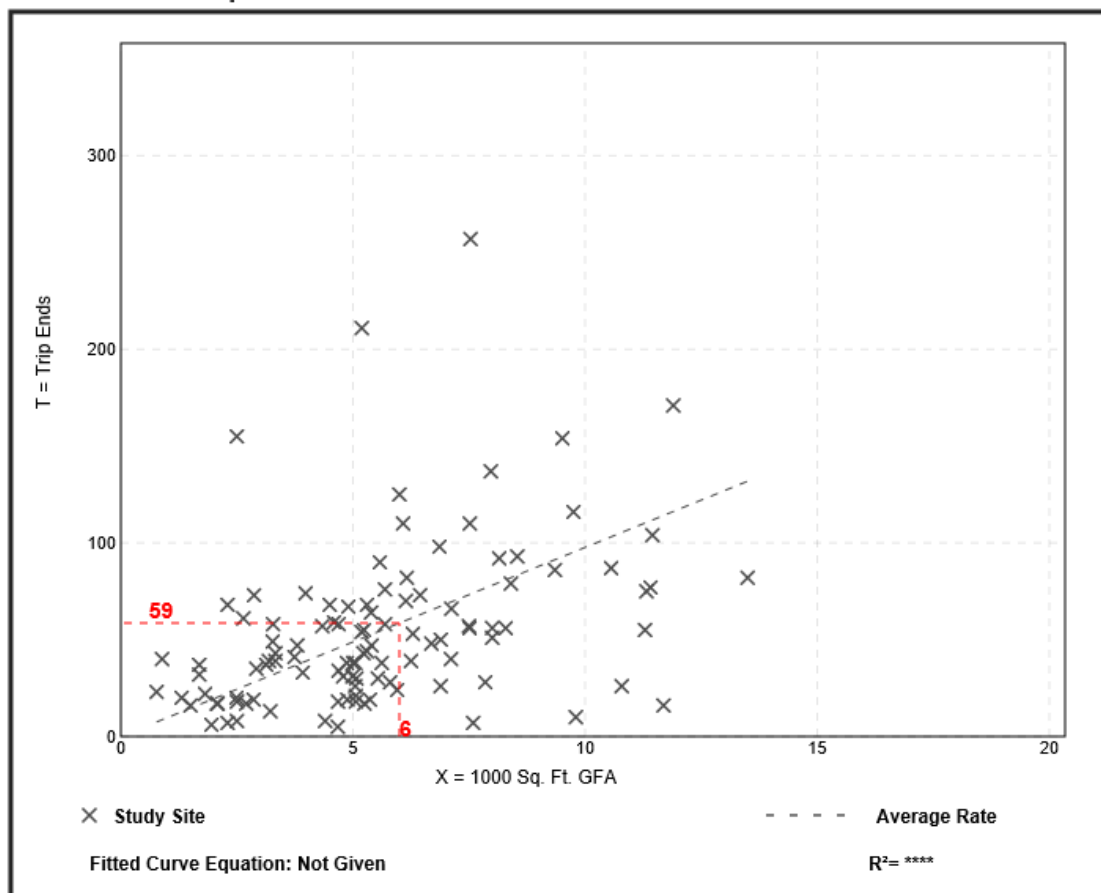
High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 107
 Avg. 1000 Sq. Ft. GFA: 6
 Directional Distribution: 62% entering, 38% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.77	0.92 - 62.00	7.37

Data Plot and Equation



High-Turnover (Sit-Down) Restaurant (932)

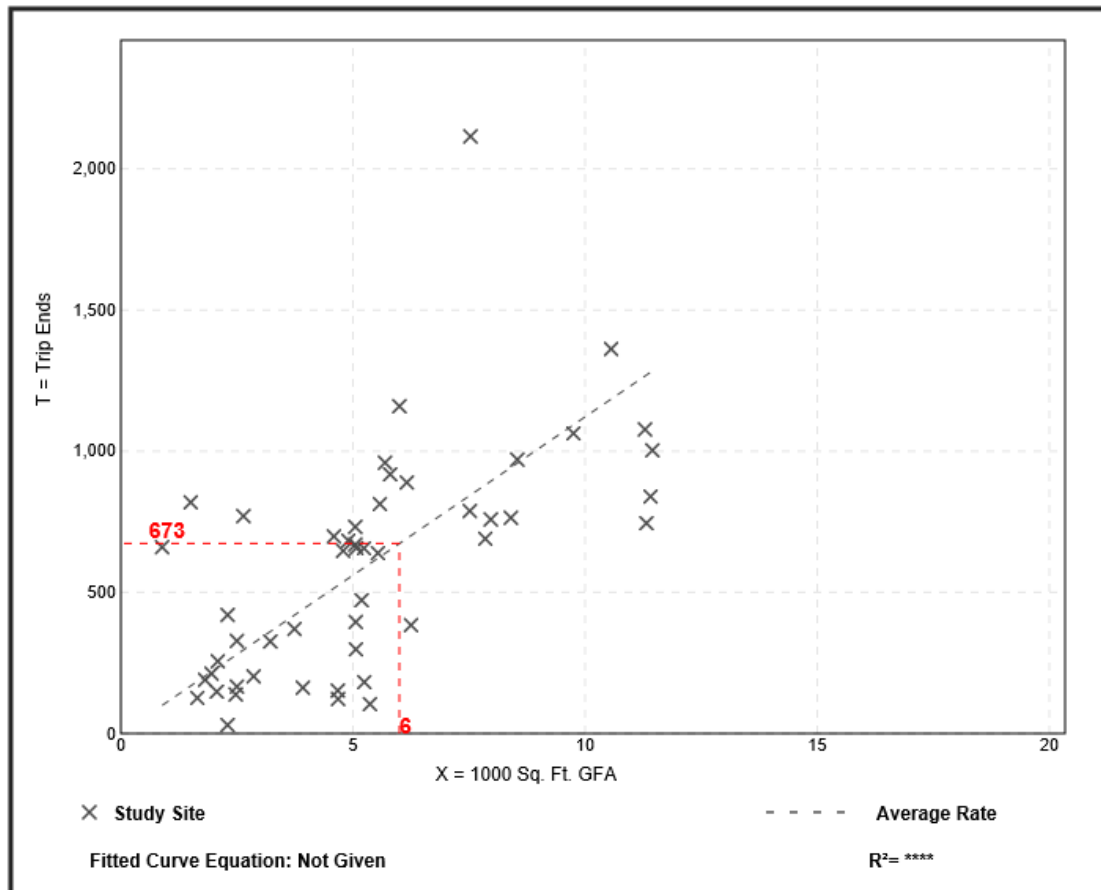
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 50
Avg. 1000 Sq. Ft. GFA: 5
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
112.18	13.04 - 742.41	72.51

Data Plot and Equation



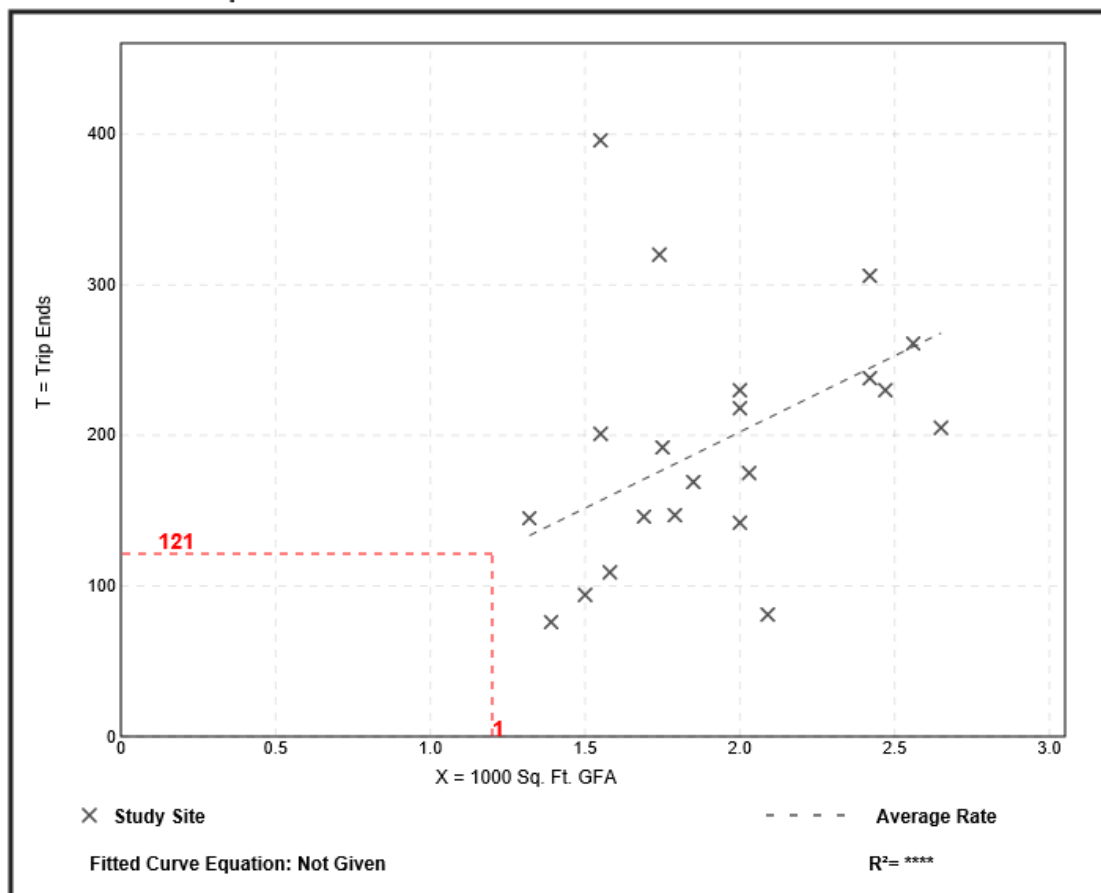
Coffee/Donut Shop without Drive-Through Window (936)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 21
 Avg. 1000 Sq. Ft. GFA: 2
 Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
101.14	38.76 - 255.48	43.44

Data Plot and Equation



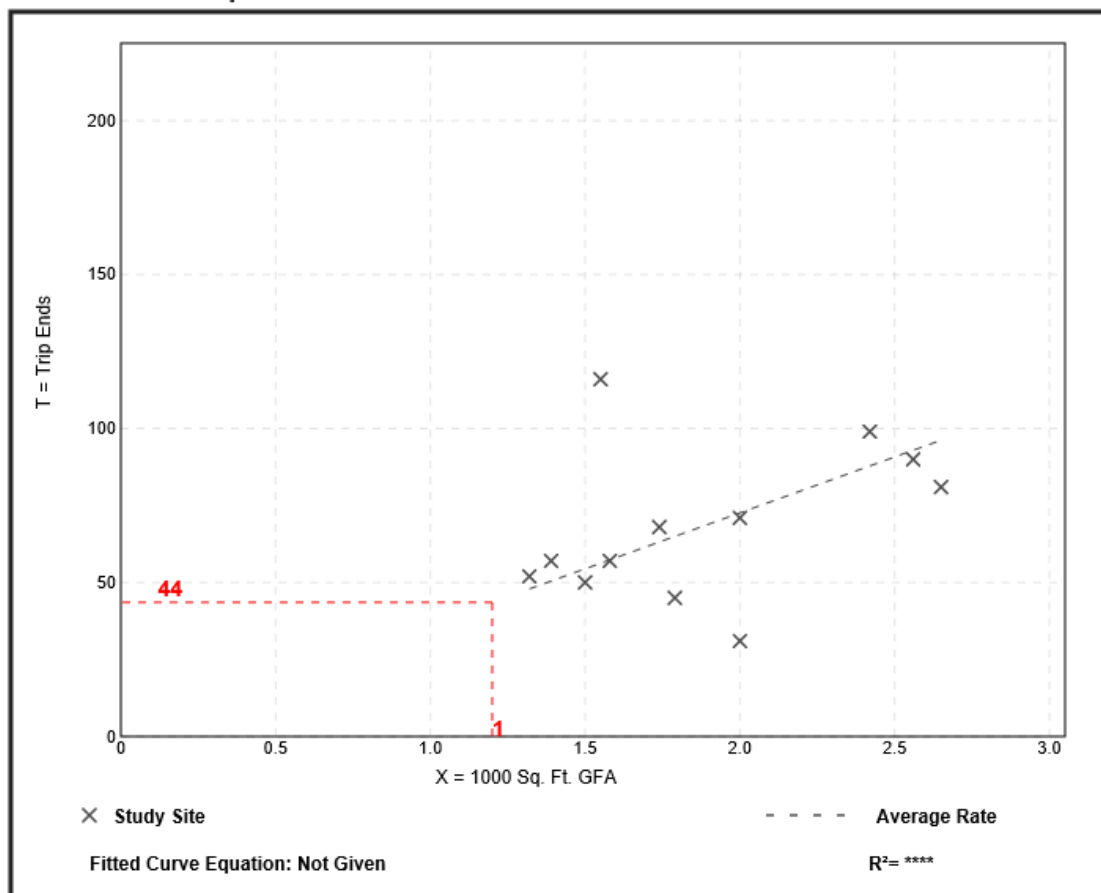
Coffee/Donut Shop without Drive-Through Window (936)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 12
 Avg. 1000 Sq. Ft. GFA: 2
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
36.31	15.50 - 74.84	13.22

Data Plot and Equation



Coffee/Donut Shop without Drive-Through Window (936)

Person Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

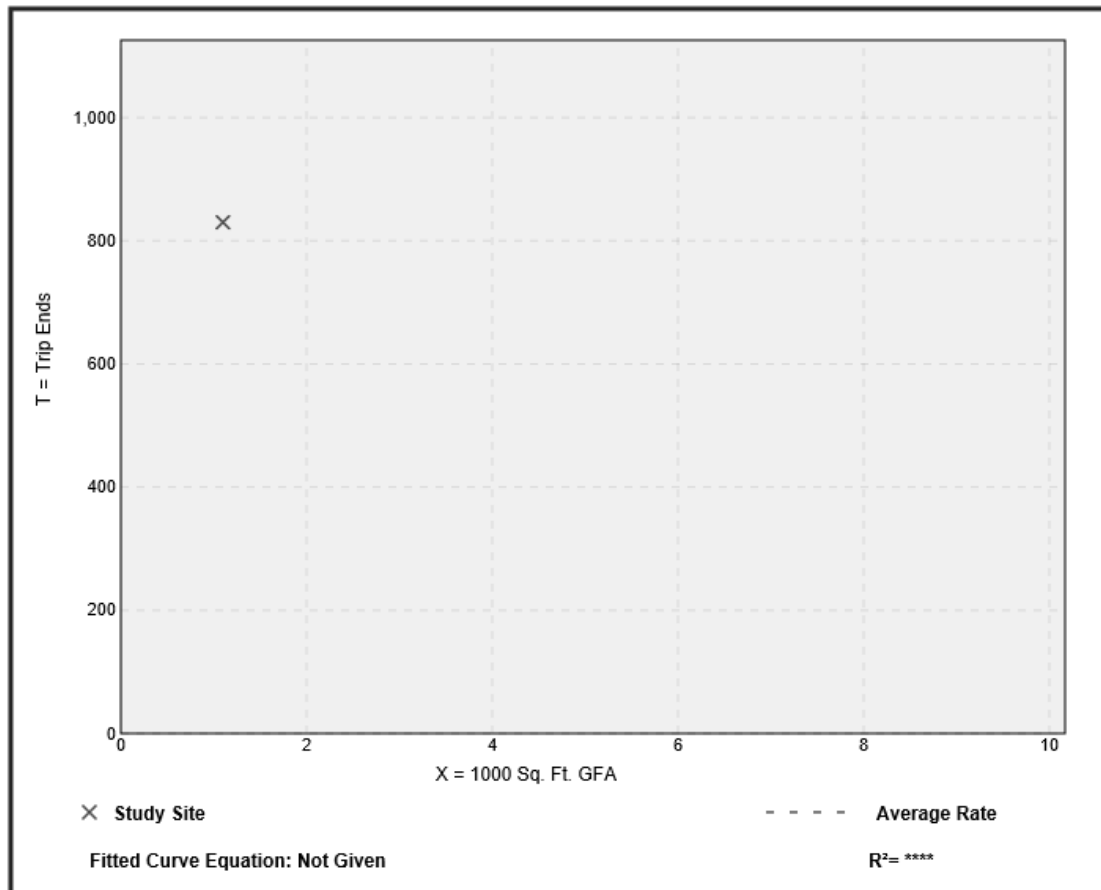
Setting/Location: Dense Multi-Use Urban
Number of Studies: 1
Avg. 1000 Sq. Ft. GFA: 1
Directional Distribution: 50% entering, 50% exiting

Person Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
754.55	754.55 - 754.55	*

Data Plot and Equation

Caution – Small Sample Size



APPENDIX IV

Results of Internal Trip Capture Calculations
Using
NCHRP Report 684 Estimator
(ITE Trip Generation Handbook, 3rd Edition Methodology)

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	North Lawrence Riverfront Development			Organization:	MGS
Project Location:	NWC of N. 2nd St (US 24-40) and Elm St.			Performed By:	MG
Scenario Description:	Buildings I thru VII and Hotel			Date:	10/9/2018
Analysis Year:	2018			Checked By:	
Analysis Period:	AM Street Peak Hour			Date:	

Table 1-A: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office	710	30,000	GFA sq. ft.	55	47	8
Retail	820	48,500	GFA sq. ft.	176	109	67
Restaurant	931 /932/936	19,700	GFA sq. ft.	246	133	113
Cinema/Entertainment	925	6,100	GFA sq. ft.	0	0	0
Residential	221	390	dwelling units	130	34	96
Hotel	310	150	rooms	70	41	29
All Other Land Uses ²				0		
Total				677	364	313

Table 2-A: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-A: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-A: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	5	0	0	0
Retail	2		9	0	1	0
Restaurant	7	9		0	2	2
Cinema/Entertainment	0	0	0		0	0
Residential	1	1	19	0		0
Hotel	1	4	3	0	0	

Table 5-A: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	677	364	313
Internal Capture Percentage	20%	19%	22%
External Vehicle-Trips ³	541	296	245
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-A: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	23%	88%
Retail	15%	18%
Restaurant	27%	18%
Cinema/Entertainment	N/A	N/A
Residential	9%	22%
Hotel	5%	28%

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Project Name:	North Lawrence Riverfront Development
Analysis Period:	AM Street Peak Hour

Table 7-A: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-A (D): Entering Trips			Table 7-A (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	47	47	1.00	8	8
Retail	1.00	109	109	1.00	67	67
Restaurant	1.00	133	133	1.00	113	113
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	34	34	1.00	96	96
Hotel	1.00	41	41	1.00	29	29

Table 8-A (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	5	0	0	0
Retail	19		9	0	9	0
Restaurant	35	16		0	5	3
Cinema/Entertainment	0	0	0		0	0
Residential	2	1	19	0		0
Hotel	22	4	3	0	0	

Table 8-A (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		35	31	0	0	0
Retail	2		67	0	1	0
Restaurant	7	9		0	2	2
Cinema/Entertainment	0	0	0		0	0
Residential	1	19	27	0		0
Hotel	1	4	8	0	0	

Table 9-A (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	11	36	47	36	0	0
Retail	16	93	109	93	0	0
Restaurant	36	97	133	97	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	3	31	34	31	0	0
Hotel	2	39	41	39	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-A (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	7	1	8	1	0	0
Retail	12	55	67	55	0	0
Restaurant	20	93	113	93	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	21	75	96	75	0	0
Hotel	8	21	29	21	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	North Lawrence Riverfront Development			Organization:	MGS
Project Location:	NWC of N. 2nd St (US 24-40) and Elm St.			Performed By:	MG
Scenario Description:	Buildings I thru VII and Hotel			Date:	10/9/2018
Analysis Year:	2018			Checked By:	
Analysis Period:	PM Street Peak Hour			Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office	710	30,000	GFA sq. ft.	36	6	30
Retail	820	48,500	GFA sq. ft.	318	153	165
Restaurant	931 /932/936	19,700	GFA sq. ft.	213	130	83
Cinema/Entertainment	925	6,100	GFA sq. ft.	70	47	23
Residential	221	390	dwelling units	164	100	64
Hotel	310	150	rooms	86	44	42
All Other Land Uses ²				0		
Total				887	480	407

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		1000	1000		1000	
Retail					1000	
Restaurant					1000	
Cinema/Entertainment					1000	
Residential		1000	1000			
Hotel					1650	

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		5	1	0	1	0
Retail	1		38	7	40	7
Restaurant	1	34		7	14	6
Cinema/Entertainment	0	5	4		2	0
Residential	2	12	10	0		2
Hotel	0	3	7	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	887	480	407
Internal Capture Percentage	47%	44%	51%
External Vehicle-Trips ³	469	271	198
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	67%	23%
Retail	39%	56%
Restaurant	46%	75%
Cinema/Entertainment	30%	48%
Residential	57%	41%
Hotel	34%	24%

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	North Lawrence Riverfront Development
Analysis Period:	PM Street Peak Hour

Table 7-P: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	6	6	1.00	30	30
Retail	1.00	153	153	1.00	165	165
Restaurant	1.00	130	130	1.00	83	83
Cinema/Entertainment	1.00	47	47	1.00	23	23
Residential	1.00	100	100	1.00	64	64
Hotel	1.00	44	44	1.00	42	42

Table 8-P (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		5	1	0	1	0
Retail	3		48	7	40	8
Restaurant	2	34		7	14	6
Cinema/Entertainment	0	5	7		2	0
Residential	3	20	10	0		2
Hotel	0	7	29	0	1	

Table 8-P (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		9	2	0	4	0
Retail	2		38	12	46	7
Restaurant	2	77		15	16	31
Cinema/Entertainment	0	6	4		4	0
Residential	3	12	14	0		5
Hotel	0	3	7	0	0	

Table 9-P (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	4	2	6	2	0	0
Retail	59	94	153	94	0	0
Restaurant	60	70	130	70	0	0
Cinema/Entertainment	14	33	47	33	0	0
Residential	57	43	100	43	0	0
Hotel	15	29	44	29	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-P (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	7	23	30	23	0	0
Retail	93	72	165	72	0	0
Restaurant	62	21	83	21	0	0
Cinema/Entertainment	11	12	23	12	0	0
Residential	26	38	64	38	0	0
Hotel	10	32	42	32	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

Table 7.1a Adjusted Internal Trip Capture Rates for Trip Origins within a Multi-Use Development

Land Use Pairs		Weekday	
		AM Peak Hour	PM Peak Hour
From OFFICE	To Office	0.0%	0.0%
	To Retail	28.0%	15.2%
	To Restaurant	63.0%	3.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	1.0%	1.9%
	To Hotel	0.0%	0.0%
From RETAIL	To Office	29.0%	2.0%
	To Retail	0.0%	0.0%
	To Restaurant	13.0%	29.0%
	To Cinema/Entertainment	0.0%	4.0%
	To Residential	14.0%	24.2%
	To Hotel	0.0%	5.0%
From RESTAURANT	To Office	31.0%	3.0%
	To Retail	14.0%	41.0%
	To Restaurant	0.0%	0.0%
	To Cinema/Entertainment	0.0%	8.0%
	To Residential	4.0%	16.7%
	To Hotel	3.0%	7.0%
From CINEMA/ENTERTAINMENT	To Office	0.0%	2.0%
	To Retail	0.0%	21.0%
	To Restaurant	0.0%	31.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	7.4%
	To Hotel	0.0%	2.0%
From RESIDENTIAL	To Office	2.0%	4.0%
	To Retail	1.0%	31.9%
	To Restaurant	20.0%	16.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	0.0%
	To Hotel	0.0%	3.0%
From HOTEL	To Office	75.0%	0.0%
	To Retail	14.0%	16.0%
	To Restaurant	9.0%	68.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	1.5%
	To Hotel	0.0%	0.0%

Table 7.2a Adjusted Internal Trip Capture Rates for Trip Destinations within a Multi-Use Development

Land Use Pairs		Weekday	
		AM Peak Hour	PM Peak Hour
To OFFICE	From Office	0.0%	0.0%
	From Retail	4.0%	31.0%
	From Restaurant	14.0%	30.0%
	From Cinema/Entertainment	0.0%	6.0%
	From Residential	3.0%	57.0%
	From Hotel	3.0%	0.0%
To RETAIL	From Office	32.0%	6.1%
	From Retail	0.0%	0.0%
	From Restaurant	8.0%	50.0%
	From Cinema/Entertainment	0.0%	4.0%
	From Residential	17.0%	7.6%
	From Hotel	4.0%	2.0%
To RESTAURANT	From Office	23.0%	1.5%
	From Retail	50.0%	29.0%
	From Restaurant	0.0%	0.0%
	From Cinema/Entertainment	0.0%	3.0%
	From Residential	20.0%	10.6%
	From Hotel	6.0%	5.0%
To CINEMA/ENTERTAINMENT	From Office	0.0%	1.0%
	From Retail	0.0%	26.0%
	From Restaurant	0.0%	32.0%
	From Cinema/Entertainment	0.0%	0.0%
	From Residential	0.0%	0.0%
	From Hotel	0.0%	0.0%
To RESIDENTIAL	From Office	0.0%	4.0%
	From Retail	2.0%	46.0%
	From Restaurant	5.0%	16.0%
	From Cinema/Entertainment	0.0%	4.0%
	From Residential	0.0%	0.0%
	From Hotel	0.0%	0.0%
To HOTEL	From Office	0.0%	0.0%
	From Retail	0.0%	17.0%
	From Restaurant	4.0%	71.0%
	From Cinema/Entertainment	0.0%	1.0%
	From Residential	0.0%	12.0%
	From Hotel	0.0%	0.0%

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	North Lawrence Riverfront Development			Organization:	MGS
Project Location:	NWC of N. 2nd St (US 24-40) and Elm St.			Performed By:	MG
Scenario Description:	Buildings I thru VII and Hotel			Date:	10/9/2018
Analysis Year:	2018			Checked By:	
Analysis Period:	AM Street Peak Hour			Date:	

Table 1-A: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office	710	30,000	GFA sq. ft.	55	47	8
Retail	820	74,300	GFA sq. ft.	189	117	72
Restaurant				0		
Cinema/Entertainment				0		
Residential	221	390	dwelling units	130	34	96
Hotel	310	150	rooms	70	41	29
All Other Land Uses ²				0		
Total				444	239	205

Table 2-A: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-A: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-A: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	0	0	0	0
Retail	2		0	0	1	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	1	1	0	0		0
Hotel	1	4	0	0	0	

Table 5-A: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	444	239	205
Internal Capture Percentage	5%	5%	6%
External Vehicle-Trips ³	420	227	193
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-A: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	9%	25%
Retail	6%	4%
Restaurant	N/A	N/A
Cinema/Entertainment	N/A	N/A
Residential	3%	2%
Hotel	0%	17%

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Project Name:	North Lawrence Riverfront Development
Analysis Period:	AM Street Peak Hour

Table 7-A: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-A (D): Entering Trips			Table 7-A (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	47	47	1.00	8	8
Retail	1.00	117	117	1.00	72	72
Restaurant	1.00	0	0	1.00	0	0
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	34	34	1.00	96	96
Hotel	1.00	41	41	1.00	29	29

Table 8-A (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		2	5	0	0	0
Retail	21		9	0	10	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	1	19	0		0
Hotel	22	4	3	0	0	

Table 8-A (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		37	0	0	0	0
Retail	2		0	0	1	0
Restaurant	7	9		0	2	2
Cinema/Entertainment	0	0	0		0	0
Residential	1	20	0	0		0
Hotel	1	5	0	0	0	

Table 9-A (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	4	43	47	43	0	0
Retail	7	110	117	110	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	1	33	34	33	0	0
Hotel	0	41	41	41	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-A (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	2	6	8	6	0	0
Retail	3	69	72	69	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	2	94	96	94	0	0
Hotel	5	24	29	24	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	North Lawrence Riverfront Development			Organization:	MGS
Project Location:	NWC of N. 2nd St (US 24-40) and Elm St.			Performed By:	MG
Scenario Description:	Buildings I thru VII and Hotel			Date:	10/9/2018
Analysis Year:	2018			Checked By:	
Analysis Period:	PM Street Peak Hour			Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office	710	30,000	GFA sq. ft.	36	6	30
Retail	820	74,300	GFA sq. ft.	436	209	227
Restaurant				0		
Cinema/Entertainment				0		
Residential	221	390	dwelling units	164	100	64
Hotel	310	150	rooms	86	44	42
All Other Land Uses ²				0		
Total				722	359	363

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.00	0%	0%	1.00	0%	0%
Retail	1.00	0%	0%	1.00	0%	0%
Restaurant	1.00	0%	0%	1.00	0%	0%
Cinema/Entertainment	1.00	0%	0%	1.00	0%	0%
Residential	1.00	0%	0%	1.00	0%	0%
Hotel	1.00	0%	0%	1.00	0%	0%
All Other Land Uses ²						

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		1000	1000		1000	
Retail					1000	
Restaurant					1000	
Cinema/Entertainment					1000	
Residential		1000	1000			
Hotel					1650	

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		5	0	0	1	0
Retail	2		0	0	46	7
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	3	16	0	0		2
Hotel	0	4	0	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	722	359	363
Internal Capture Percentage	24%	24%	24%
External Vehicle-Trips ³	550	273	277
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	83%	20%
Retail	12%	24%
Restaurant	N/A	N/A
Cinema/Entertainment	N/A	N/A
Residential	47%	33%
Hotel	20%	10%

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Project Name:	North Lawrence Riverfront Development
Analysis Period:	PM Street Peak Hour

Table 7-P: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.00	6	6	1.00	30	30
Retail	1.00	209	209	1.00	227	227
Restaurant	1.00	0	0	1.00	0	0
Cinema/Entertainment	1.00	0	0	1.00	0	0
Residential	1.00	100	100	1.00	64	64
Hotel	1.00	44	44	1.00	42	42

Table 8-P (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		5	1	0	1	0
Retail	5		66	9	55	11
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	3	20	10	0		2
Hotel	0	7	29	0	1	

Table 8-P (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		13	0	0	4	0
Retail	2		0	0	46	7
Restaurant	2	105		0	16	31
Cinema/Entertainment	0	8	0		4	0
Residential	3	16	0	0		5
Hotel	0	4	0	0	0	

Table 9-P (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	5	1	6	1	0	0
Retail	25	184	209	184	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	47	53	100	53	0	0
Hotel	9	35	44	35	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-P (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	6	24	30	24	0	0
Retail	55	172	227	172	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	21	43	64	43	0	0
Hotel	4	38	42	38	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

²Person-Trips

³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

*Indicates computation that has been rounded to the nearest whole number.

Table 7.1a Adjusted Internal Trip Capture Rates for Trip Origins within a Multi-Use Development

Land Use Pairs		Weekday	
		AM Peak Hour	PM Peak Hour
From OFFICE	To Office	0.0%	0.0%
	To Retail	28.0%	15.2%
	To Restaurant	63.0%	3.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	1.0%	1.9%
	To Hotel	0.0%	0.0%
From RETAIL	To Office	29.0%	2.0%
	To Retail	0.0%	0.0%
	To Restaurant	13.0%	29.0%
	To Cinema/Entertainment	0.0%	4.0%
	To Residential	14.0%	24.2%
	To Hotel	0.0%	5.0%
From RESTAURANT	To Office	31.0%	3.0%
	To Retail	14.0%	41.0%
	To Restaurant	0.0%	0.0%
	To Cinema/Entertainment	0.0%	8.0%
	To Residential	4.0%	16.7%
	To Hotel	3.0%	7.0%
From CINEMA/ENTERTAINMENT	To Office	0.0%	2.0%
	To Retail	0.0%	21.0%
	To Restaurant	0.0%	31.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	7.4%
	To Hotel	0.0%	2.0%
From RESIDENTIAL	To Office	2.0%	4.0%
	To Retail	1.0%	31.9%
	To Restaurant	20.0%	16.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	0.0%
	To Hotel	0.0%	3.0%
From HOTEL	To Office	75.0%	0.0%
	To Retail	14.0%	16.0%
	To Restaurant	9.0%	68.0%
	To Cinema/Entertainment	0.0%	0.0%
	To Residential	0.0%	1.5%
	To Hotel	0.0%	0.0%

Table 7.2a Adjusted Internal Trip Capture Rates for Trip Destinations within a Multi-Use Development

Land Use Pairs		Weekday	
		AM Peak Hour	PM Peak Hour
To OFFICE	From Office	0.0%	0.0%
	From Retail	4.0%	31.0%
	From Restaurant	14.0%	30.0%
	From Cinema/Entertainment	0.0%	6.0%
	From Residential	3.0%	57.0%
	From Hotel	3.0%	0.0%
To RETAIL	From Office	32.0%	6.1%
	From Retail	0.0%	0.0%
	From Restaurant	8.0%	50.0%
	From Cinema/Entertainment	0.0%	4.0%
	From Residential	17.0%	7.6%
	From Hotel	4.0%	2.0%
To RESTAURANT	From Office	23.0%	1.5%
	From Retail	50.0%	29.0%
	From Restaurant	0.0%	0.0%
	From Cinema/Entertainment	0.0%	3.0%
	From Residential	20.0%	10.6%
	From Hotel	6.0%	5.0%
To CINEMA/ENTERTAINMENT	From Office	0.0%	1.0%
	From Retail	0.0%	26.0%
	From Restaurant	0.0%	32.0%
	From Cinema/Entertainment	0.0%	0.0%
	From Residential	0.0%	0.0%
	From Hotel	0.0%	0.0%
To RESIDENTIAL	From Office	0.0%	4.0%
	From Retail	2.0%	46.0%
	From Restaurant	5.0%	16.0%
	From Cinema/Entertainment	0.0%	4.0%
	From Residential	0.0%	0.0%
	From Hotel	0.0%	0.0%
To HOTEL	From Office	0.0%	0.0%
	From Retail	0.0%	17.0%
	From Restaurant	4.0%	71.0%
	From Cinema/Entertainment	0.0%	1.0%
	From Residential	0.0%	12.0%
	From Hotel	0.0%	0.0%

APPENDIX V

Summary of Traffic Counts

PEAK AM Count

Signal Location	PEAK AM Count Last updated 2/23/17	Count AM Date	Begin Peak	Peak Volume	South Bound			West Bound			North Bound			East Bound			
STREET1	STREET2	COUNTDATE	PEAKTOT	SBRT	SBTHRU	SBLT	WBRT	WBTHRU	WBLT	NBRT	NBTHRU	NBLT	EBRT	EBTHRU	EBLT	Site_ID	
2 nd St.	Locust St.	29-Jan-16	7:30	2015	1	931	18	19	2	251	26	751	7	8	1	0	168
2 nd St.	Lyon St.	15-Apr-15	7:30	1627	0	786	7	17	0	116	38	662	1	0	0	0	2
2 nd St.	McDonald St	22-Apr-15	7:30	1525	33	525	14	18	18	14	21	804	15	12	15	36	3
3 rd St.	KTA	15-Sep-15	7:15	1742	23	610	120	67	35	166	256	289	151	17	5	3	4
6 th St.	Congressional	30-Mar-16	8:00	1295	24	13	87	25	432	29	24	47	39	15	486	74	72
6 th St.	Folks Road	08-Feb-17	7:00	2266	112	41	133	202	671	71	66	70	40	44	738	78	6
6 th St.	Iowa St.	06-May-15	7:30	2427	0	0	0	0	601	292	241	0	138	206	949	0	7
6 th St.	Kasold Dr.	14-Apr-16	7:30	2963	39	250	145	33	717	107	193	128	175	174	974	28	8
6 th St.	Kentucky St.	04-May-16	7:30	2434	0	0	0	0	975	0	177	0	486	0	796	0	9
6 th St.	Lawrence Ave.	19-Apr-16	7:30	2638	71	88	197	94	726	25	51	76	49	44	1147	70	10
6 th St.	Maine St.	03-May-16	7:30	2785	49	28	158	297	931	13	8	47	33	29	1088	104	11
6 th St.	Massachusetts St.	05-May-16	7:30	1919	0	0	0	312	221	0	7	141	69	153	498	518	12
6 th St.	Michigan St.	26-Apr-16	7:30	2415	57	32	196	54	852	2	18	21	40	13	1100	30	13
6 th St.	MontereyWay	14-Feb-17	7:30	2603	56	180	119	45	803	71	135	75	166	82	816	55	14
6 th St.	Rockledge Rd.	10-Apr-14	7:30	2285	119	17	15	19	690	16	21	29	37	45	1094	183	15
6 th St.	Schwarz Rd.	22-Feb-17	7:30	2288	3	1	17	0	871	9	20	0	24	16	1327	0	16
6 th St.	Stoneridge	07-Apr-16	7:30	1259	44	4	43	23	525	37	67	0	48	9	443	16	73
6 th St.	Vermont St.	25-Feb-16	7:30	2356	683	169	350	0	261	6	0	0	0	69	818	0	17
6 th St.	Wakarusa Dr.	05-May-15	7:30	2268	77	200	179	65	325	242	206	205	140	146	449	34	18
7 th St.	Kentucky St.	07-Oct-14	7:30	809	0	0	0	55	45	0	41	612	17	0	30	9	186
7 th St.	Massachusetts St.	30-Apr-14	8:00	504	17	94	19	17	42	14	23	164	23	19	37	35	19
7 th St.	New Hampshire St.	01-Oct-14	7:30	1039	5	151	306	394	41	4	5	43	4	17	47	22	20
7 th St.	Vermont St.	27-Aug-14	7:45	496	37	197	54	0	58	8	46	0	30	19	47	0	21
8 th St.	Kentucky St.	02-Oct-14	7:30	703	0	0	0	22	24	0	24	583	4	0	33	13	22
8 th St.	Massachusetts St.	29-Oct-14	7:30	447	10	139	5	5	18	8	17	179	9	6	38	13	23
8 th St.	Vermont St.	08-Oct-14	8:00	400	16	160	8	11	31	11	30	79	4	9	33	8	24
9 th St.	Emery Rd.	17-Nov-15	7:30	1118	0	0	0	0	341	44	47	0	22	30	634	0	25
9 th St.	Iowa St.	06-May-14	7:30	2672	39	860	227	126	135	163	14	677	32	54	248	97	26
9 th St.	Kentucky St.	29-Apr-14	7:30	1360	0	0	0	21	171	0	27	568	172	0	333	68	27
9 th St.	Maine St.	12-Nov-15	7:45	1130	12	45	39	16	295	8	6	13	8	28	644	16	28
9 th St.	Massachusetts St.	08-May-14	7:45	806	13	113	12	11	111	12	38	203	30	56	182	25	29
9 th St.	Mississippi St.	04-Nov-15	7:45	1379	16	64	41	22	367	62	26	14	23	76	660	8	30
9 th St.	Tennessee St.	29-Apr-14	7:45	1617	58	524	33	0	333	21	0	0	0	264	384	0	31
9 th St.	Vermont St.	14-May-14	7:45	790	29	116	22	14	129	5	12	65	28	79	239	52	32
10 th St.	Massachusetts St.	15-May-14	8:00	550	12	130	20	19	35	5	15	220	24	15	43	12	33
11 th St.	Kentucky St.	23-Oct-14	7:30	1022	0	0	0	147	117	0	48	487	54	0	158	11	34
11 th St.	Massachusetts St.	21-Aug-14	7:45	1167	13	170	14	27	147	33	68	218	182	141	131	23	1
11 th St.	Tennessee St.	28-Oct-14	7:30	1128	20	714	148	0	130	46	0	0	0	31	39	0	36
14 th St.	Kentucky St.	20-Jan-15	7:30	947	0	0	0	69	84	0	7	599	65	0	99	24	37
14 th St.	Massachusetts St.	22-Jan-15	7:30	1094	3	248	31	24	57	25	62	461	92	42	45	4	38
14 th St.	Tennessee St.	21-Jan-15	7:30	823	9	579	80	0	66	54	0	0	0	19	16	0	39
Bob Billings	Crestline Dr.	09-Apr-15	7:30	1874	94	83	73	16	298	15	10	21	73	252	855	84	40
15 th St.	Iowa St.	26-Jan-17	7:30	3094	154	748	130	38	63	36	134	801	122	229	347	292	41
Bob Billings	Kasold Dr.	28-Apr-15	7:30	2533	59	335	129	29	273	108	269	296	102	136	719	80	42
Bob Billings	Monterey Way	25-Mar-15	7:30	1553	167	0	220	78	361	0	0	0	0	0	620	107	80
17 th St.	Massachusetts St.	03-Feb-15	7:30	831	14	245	3	5	32	13	11	446	14	23	10	15	43
19 th St.	Haskell Rd.	12-Feb-15	7:30	1622	35	204	61	69	281	29	37	322	200	150	195	39	44
19 th St.	Iowa St.	29-Apr-15	7:45	2673	57	698	226	216	106	130	256	835	26	22	92	9	45
19 th St.	Kentucky St.	25-Feb-15	7:15	1498	0	0	0	187	441	0	11	66	4	7	516	266	46
19 th St.	Louisiana St.	07-Apr-15	7:30	1625	22	24	9	17	565	156	170	44	152	90	368	8	47

PEAK PM Count																	
Signal Location	Last Updated 2/23/17	Count PM Date	Begin Peak	Peak Volume	South Bound			West Bound			North Bound			East Bound			Site ID
STREET1	STREET2	COUNTDATE		PEAKTOT	SBRT	SBTHRU	SBLT	WBRT	WBTHRU	WBLT	NBRT	NBTHRU	NBLT	EBRT	EBTHRU	EBLT	
2 nd St.	Locust St.	29-Jan-16	16:45	2323	10	947	52	16	1	214	45	1002	18	11	3	4	168
2 nd St.	Lyon St.	15-Apr-15	16:30	1935	0	821	21	21	4	95	96	867	2	2	4	2	2
2 nd St.	McDonald St	22-Apr-15	16:45	2265	182	442	69	66	190	115	93	362	324	200	118	104	3
3 rd St.	KTA	15-Sep-15	17:00	1826	7	400	48	105	8	321	228	589	34	39	33	14	4
6 th St.	Congressional	30-Mar-16	17:00	1745	46	31	84	116	568	82	28	27	42	19	628	74	72
6 th St.	Folks Road	08-Feb-17	17:00	2639	124	38	108	94	1037	55	44	35	23	63	894	124	6
6 th St.	Iowa St.	06-May-15	16:45	3035	0	0	0	0	1156	339	219	0	337	213	771	0	7
6 th St.	Kasold Dr.	14-Apr-16	16:45	3499	48	164	83	63	1127	225	192	267	271	209	769	81	8
6 th St.	Kentucky St.	04-May-16	16:45	2969	0	0	0	0	1082	0	287	0	621	0	979	0	9
6 th St.	Lawrence Ave.	19-Apr-16	16:45	3162	82	102	170	167	1287	62	39	117	66	55	928	87	10
6 th St.	Maine St.	03-May-16	4:30PM	3112	126	57	265	197	1231	13	31	54	72	27	991	48	11
6 th St.	Massachusetts St.	05-May-16	16:45	2524	0	0	0	394	330	0	14	181	101	205	511	788	12
6 th St.	Michigan St.	26-Apr-16	16:15	2813	97	33	178	73	1357	9	17	23	59	28	927	12	13
6 th St.	MontereyWay	14-Feb-17	17:00	3413	58	159	135	141	1163	159	91	164	177	157	920	89	14
6 th St.	Rockledge Rd.	10-Apr-14	16:45	2884	228	23	29	19	1306	19	20	32	49	68	920	171	15
6 th St.	Schwarz Rd.	22-Feb-17	16:45	2884	1	0	2	0	1584	23	9	0	21	20	1224	0	16
6 th St.	Stoneridge	07-Apr-16	17:00	1570	18	2	34	70	547	69	37	0	11	58	674	50	73
6 th St.	Vermont St.	25-Feb-16	16:30	2859	675	212	354	0	393	22	0	0	0	121	1082	0	17
6 th St.	Wakarusa Dr.	05-May-15	17:00	2938	50	167	175	118	546	360	307	215	235	134	571	60	18
7 th St.	Massachusetts St.	30-Apr-14	17:00	916	23	178	17	31	94	27	73	264	44	64	41	60	19
7 th St.	New Hampshire St.	02-Oct-14	17:00	1348	30	176	277	403	95	18	17	124	32	102	55	19	20
7 th St.	Vermont St.	27-Aug-14	16:45	827	73	256	65	0	121	21	97	0	88	33	73	0	21
7th St.	Kentucky St.	07-Oct-14	16:45	1246	0	0	0	159	136	0	79	792	33	0	34	13	186
8 th St.	Kentucky St.	02-Oct-14	16:30	1068	0	0	0	92	98	0	36	770	13	0	46	13	22
8 th St.	Massachusetts St.	29-Oct-14	16:15	857	36	193	11	20	75	36	43	286	34	19	65	39	23
8 th St.	Vermont St.	08-Oct-14	16:45	797	44	259	16	38	85	30	65	175	26	12	35	12	24
9 th St.	Emery Rd.	17-Nov-15	17:00	1579	0	0	0	0	771	68	72	0	52	36	579	0	25
9 th St.	Iowa St.	06-May-14	16:45	3260	65	813	173	203	378	279	17	796	69	62	270	135	26
9 th St.	Kentucky St.	29-Apr-14	16:45	1999	0	0	0	43	410	0	56	627	330	0	454	79	27
9 th St.	Maine St.	12-Nov-15	17:00	1764	21	35	50	40	785	14	22	55	28	10	688	16	28
9 th St.	Massachusetts St.	08-May-14	16:45	1363	58	200	14	32	229	19	45	304	74	109	206	73	29
9 th St.	Mississippi St.	04-Nov-15	17:00	1978	23	44	40	50	713	74	98	52	142	55	665	22	30
9 th St.	Tennessee St.	29-Apr-13	17:00	2229	147	579	37	0	659	37	0	0	0	265	505	0	31
9 th St.	Vermont St.	14-May-14	16:45	1330	101	185	27	29	287	11	20	121	89	80	273	107	32
10 th St.	Massachusetts St.	15-May-14	17:00	1157	80	244	24	74	128	23	41	378	46	48	54	17	33
11 th St.	Kentucky St.	23-Oct-14	17:00	1462	0	0	0	199	205	0	107	680	48	0	204	19	34
11 th St.	Massachusetts St.	21-Aug-14	16:45	1712	23	251	21	51	230	114	81	334	163	200	172	72	1
11 th St.	Tennessee St.	28-Oct-14	16:30	1453	20	872	113	0	106	129	0	0	0	91	122	0	36
14 th St.	Kentucky St.	20-Jan-15	16:45	1140	0	0	0	100	89	0	27	654	76	0	160	34	37
14 th St.	Massachusetts St.	22-Jan-15	16:30	1439	17	534	16	8	37	17	28	540	109	81	38	14	38
14 th St.	Tennessee St.	21-Jan-15	16:30	1417	32	947	110	0	112	67	0	0	0	67	82	0	39
Bob Billings	Crestline Dr.	09-Apr-15	16:45	2149	98	48	63	67	752	24	20	98	284	96	521	78	40
15 th St.	Iowa St.	26-Jan-17	17:00	3963	225	1000	81	136	359	155	143	993	250	209	242	170	41
Bob Billings	Kasold Dr.	28-Apr-15	16:45	3077	78	464	76	113	634	259	149	426	161	169	426	122	42
Bob Billings	Monterey Way	25-Mar-15	16:30	1776	126	0	189	268	613	0	0	0	0	0	435	145	80
17 th St.	Massachusetts St.	03-Feb-15	16:45	1555	11	617	9	9	15	10	65	651	37	64	28	39	43
19 th St.	Haskell Rd.	12-Feb-15	16:45	1799	55	312	88	89	208	23	58	268	139	246	270	43	44

Summary of Vehicular Turning Movement Counts

N. 2nd Street & Lincoln Street
Morning Peak-Hours
Sunny, Warm

File Name : N2nd&lincoln-eam
Site Code : 1
Start Date : 7/16/2013
Page No : 1

Groups Printed- Unshifted

	N. 2nd Street From North					Lincoln Street From East					N. 2nd Street From South					From West					
Start Time	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Int. Total
Factor	1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		
07:00 AM	0	0	0	0	0	0	0	2	0	2	1	0	0	0	1	0	0	0	0	0	3
07:15 AM	0	0	1	0	1	1	0	5	0	6	3	0	0	0	3	0	0	0	0	0	10
07:30 AM	0	0	0	0	0	1	0	4	0	5	3	0	0	0	3	0	0	0	0	0	8
07:45 AM	0	0	2	0	2	2	0	5	0	7	6	0	0	0	6	0	0	0	0	0	15
Total	0	0	3	0	3	4	0	16	0	20	13	0	0	0	13	0	0	0	0	0	36
08:00 AM	0	0	0	0	0	1	0	7	0	8	10	0	0	0	10	0	0	0	0	0	18
08:15 AM	0	0	0	0	0	1	0	5	0	6	2	0	0	0	2	0	0	0	0	0	8
08:30 AM	0	0	5	0	5	1	0	0	0	1	3	0	0	0	3	0	0	0	0	0	9
08:45 AM	0	0	2	0	2	0	0	3	0	3	2	0	0	0	2	0	0	0	0	0	7
Total	0	0	7	0	7	3	0	15	0	18	17	0	0	0	17	0	0	0	0	0	42
Grand Total	0	0	10	0	10	7	0	31	0	38	30	0	0	0	30	0	0	0	0	0	78
Apprch %	0	0	100	0		18.4	0	81.6	0		100	0	0	0		0	0	0	0		
Total %	0	0	12.8	0	12.8	9	0	39.7	0	48.7	38.5	0	0	0	38.5	0	0	0	0	0	

Summary of Vehicular Turning Movement Counts

N. 2nd Street & Lincoln Street
Morning Peak-Hours
Sunny, Warm

File Name : N2nd&lincoln-eam
Site Code : 1
Start Date : 7/16/2013
Page No : 2

	N. 2nd Street From North					Lincoln Street From East					N. 2nd Street From South					From West					
Start Time	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Int. Total
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:15 AM																					
07:15 AM	0	0	1	0	1	1	0	5	0	6	3	0	0	0	3	0	0	0	0	0	10
07:30 AM	0	0	0	0	0	1	0	4	0	5	3	0	0	0	3	0	0	0	0	0	8
07:45 AM	0	0	2	0	2	2	0	5	0	7	6	0	0	0	6	0	0	0	0	0	15
08:00 AM	0	0	0	0	0	1	0	7	0	8	10	0	0	0	10	0	0	0	0	0	18
Total Volume	0	0	3	0	3	5	0	21	0	26	22	0	0	0	22	0	0	0	0	0	51
% App. Total	0	0	100	0		19.2	0	80.8	0		100	0	0	0		0	0	0	0		
PHF	.000	.000	.375	.000	.375	.625	.000	.750	.000	.813	.550	.000	.000	.000	.550	.000	.000	.000	.000	.000	.708

Summary of Vehicular Turning Movement Counts

N. 2nd Street & Lincoln Street
Afternoon Peak-Hours
Sunny, warm

File Name : N2nd&lincoln-epm
Site Code : 1
Start Date : 7/16/2013
Page No : 1

Groups Printed- Unshifted

	N. 2nd Street From North					Lincoln Street From East					N. 2nd Street From South					From West					
Start Time	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Int. Total
Factor	1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		1.0	1.0	1.0	1.0		
04:00 PM	0	0	1	0	1	0	0	2	0	2	9	0	0	0	9	0	0	0	0	0	12
04:15 PM	0	0	1	0	1	1	0	2	0	3	39	0	0	0	39	0	0	0	0	0	43
04:30 PM	0	0	12	0	12	3	0	3	0	6	12	0	0	0	12	0	0	0	0	0	30
04:45 PM	0	0	1	0	1	0	0	5	0	5	9	0	0	0	9	0	0	0	0	0	15
Total	0	0	15	0	15	4	0	12	0	16	69	0	0	0	69	0	0	0	0	0	100
05:00 PM	0	0	1	0	1	3	0	4	0	7	16	0	0	0	16	0	0	0	0	0	24
05:15 PM	0	0	1	0	1	0	0	5	0	5	16	0	0	0	16	0	0	0	0	0	22
05:30 PM	0	0	2	0	2	3	0	8	0	11	12	0	0	0	12	0	0	0	0	0	25
05:45 PM	0	0	0	0	0	2	0	2	0	4	7	0	0	0	7	0	0	0	0	0	11
Total	0	0	4	0	4	8	0	19	0	27	51	0	0	0	51	0	0	0	0	0	82
Grand Total	0	0	19	0	19	12	0	31	0	43	120	0	0	0	120	0	0	0	0	0	182
Apprch %	0	0	100	0		27.9	0	72.1	0		100	0	0	0		0	0	0	0		
Total %	0	0	10.4	0	10.4	6.6	0	17	0	23.6	65.9	0	0	0	65.9	0	0	0	0	0	

Summary of Vehicular Turning Movement Counts

N. 2nd Street & Lincoln Street
Afternoon Peak-Hours
Sunny, warm

File Name : N2nd&lincoln-epm
Site Code : 1
Start Date : 7/16/2013
Page No : 2

	N. 2nd Street From North					Lincoln Street From East					N. 2nd Street From South					From West					
Start Time	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Right	Thru	Left		App. Total	Int. Total
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:15 PM																					
04:15 PM	0	0	1	0	1	1	0	2	0	3	39	0	0	0	39	0	0	0	0	0	43
04:30 PM	0	0	12	0	12	3	0	3	0	6	12	0	0	0	12	0	0	0	0	0	30
04:45 PM	0	0	1	0	1	0	0	5	0	5	9	0	0	0	9	0	0	0	0	0	15
05:00 PM	0	0	1	0	1	3	0	4	0	7	16	0	0	0	16	0	0	0	0	0	24
Total Volume	0	0	15	0	15	7	0	14	0	21	76	0	0	0	76	0	0	0	0	0	112
% App. Total	0	0	100	0		33.3	0	66.7	0		100	0	0	0		0	0	0	0		
PHF	.000	.000	.313	.000	.313	.583	.000	.700	.000	.750	.487	.000	.000	.000	.487	.000	.000	.000	.000	.000	.651

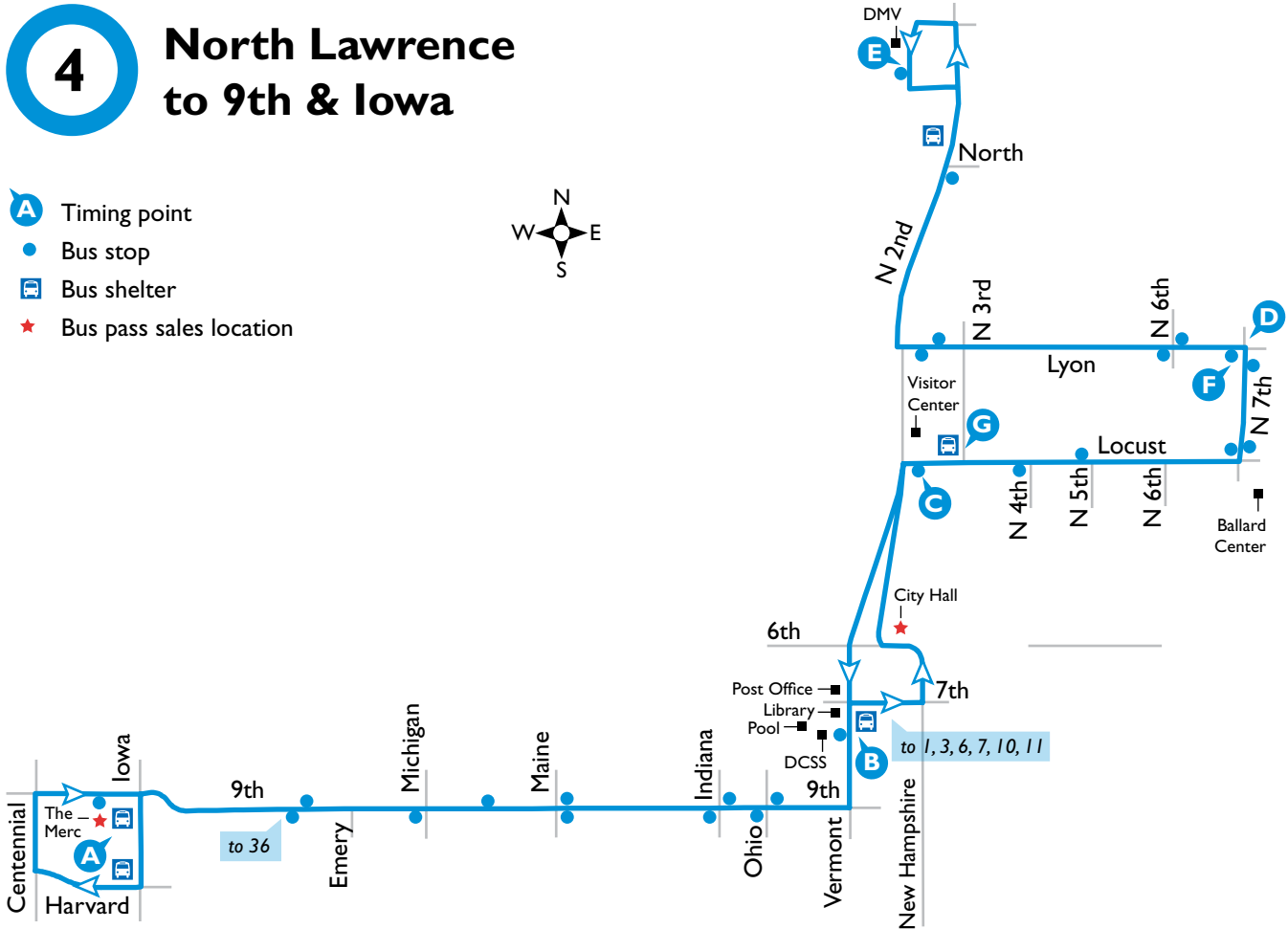
APPENDIX VI

Current Transit Route and Schedule
(Line 4, North Lawrence)



4 North Lawrence to 9th & Iowa

- A** Timing point
- Bus stop
- Bus shelter
- ★ Bus pass sales location



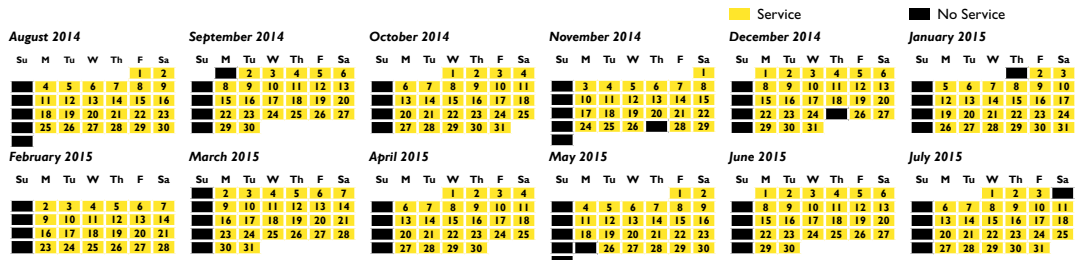
NORTHBOUND

	A	B	C	D	E
	9th & Iowa	7th & Vermont	2nd & Locust	7th & Lyon	DMV
AM	6:10	6:25	6:31	6:35	6:42
	7:10	7:25	7:31	7:35	7:42
	8:10	8:25	8:31	8:35	8:42
	9:10	9:25	9:31	9:35	9:42
	10:10	10:25	10:31	10:35	10:42
PM	11:10	11:25	11:31	11:35	11:42
	12:10	12:25	12:31	12:35	12:42
	1:10	1:25	1:31	1:35	1:42
	2:10	2:25	2:31	2:35	2:42
	3:10	3:25	3:31	3:35	3:42
	4:10	4:25	4:31	4:35	4:42
	5:10	5:25	5:31	5:35	5:42
	6:10	6:25	6:31	6:35	6:42
	7:10	7:25	7:31	7:35	7:42

SOUTHBOUND

	E	F	G	B	A
	DMV	7th & Lyon	2nd & Locust	7th & Vermont	9th & Iowa
AM	6:47	6:52	6:54	6:02	6:10
	7:47	7:52	7:54	7:02	7:10
	8:47	8:52	8:54	8:02	8:10
	9:47	9:52	9:54	9:02	9:10
	10:47	10:52	10:54	10:02	10:10
	11:47	11:52	11:54	11:02	11:10
PM	12:47	12:52	12:54	12:02	12:10
	1:47	1:52	1:54	1:02	1:10
	2:47	2:52	2:54	2:02	2:10
	3:47	3:52	3:54	3:02	3:10
	4:47	4:52	4:54	4:02	4:10
	5:47	5:52	5:54	5:02	5:10
	6:47	6:52	6:54	6:02	6:10
	7:47	7:52	7:54	8:00	7:10

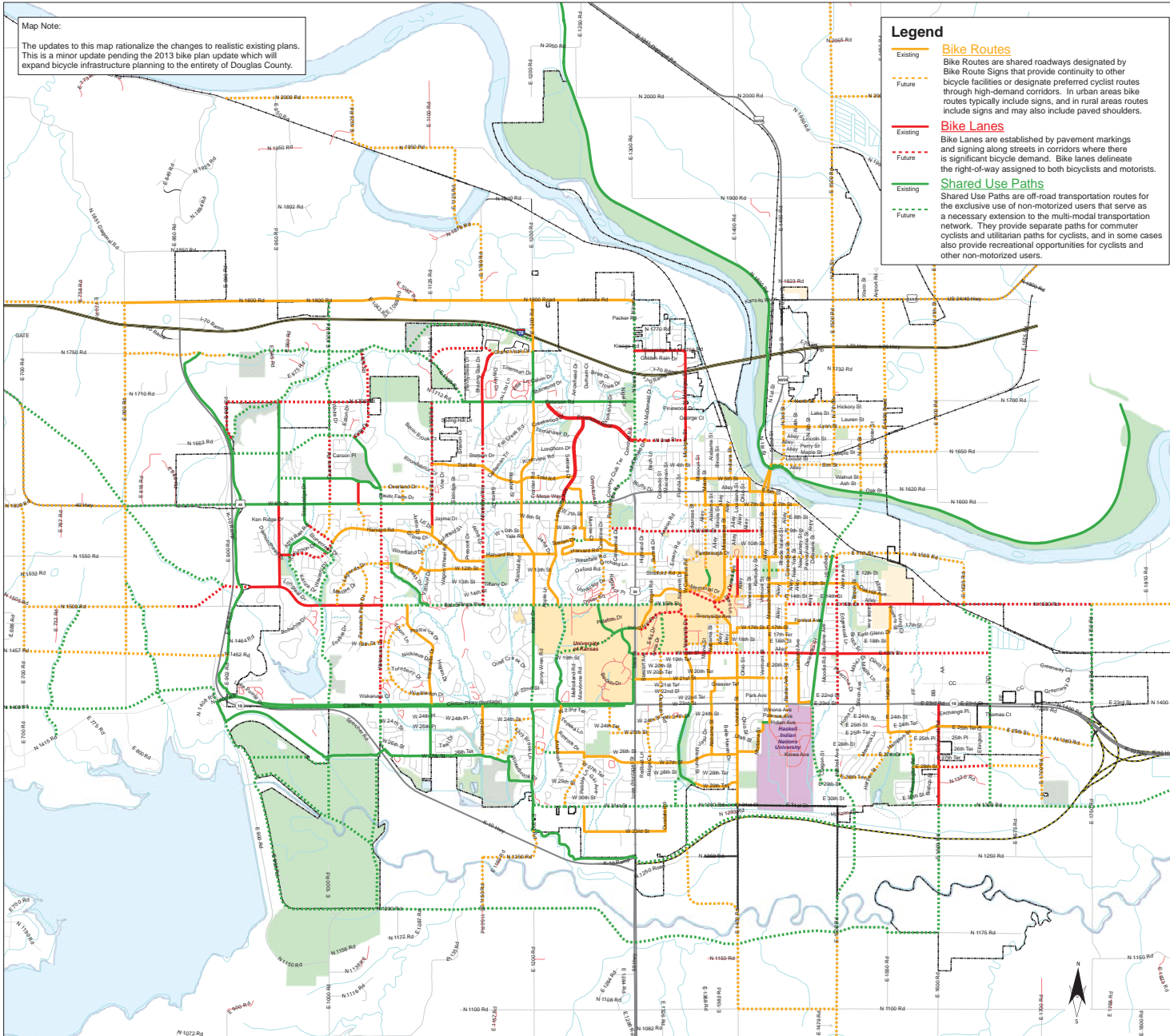
This route operates Monday–Saturday, except holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day.



APPENDIX VII

Current and Future Bikeways
(Source: City's T2040 Bicycle System Map)

T2040 Bikeway System Map



Lawrence-Douglas County Metropolitan Planning Organization

Drafted by David R. Guntert

Map Notes:

It is understood that future roadways are projections only. Development plans that extend roads designated to have bicycle facilities should also continue those bicycle facilities. All developments that seek changes in the designated bicycle facilities shown on this map are subject to review by the Bicycle Advisory Committee, which shall make its recommendations to the Lawrence City Commission, Douglas County Commission, and the Lawrence-Douglas County MPO. The MPO by approval of this map is establishing a regional plan for bikeway development that is made a part of the Metropolitan Transportation Plan.

Approved by the Lawrence-Douglas County Metropolitan Planning Organization on April 16, 2009. Minor network updates last made on March 21, 2013. T2040 Metropolitan Transportation Plan Approved on March 21, 2013.

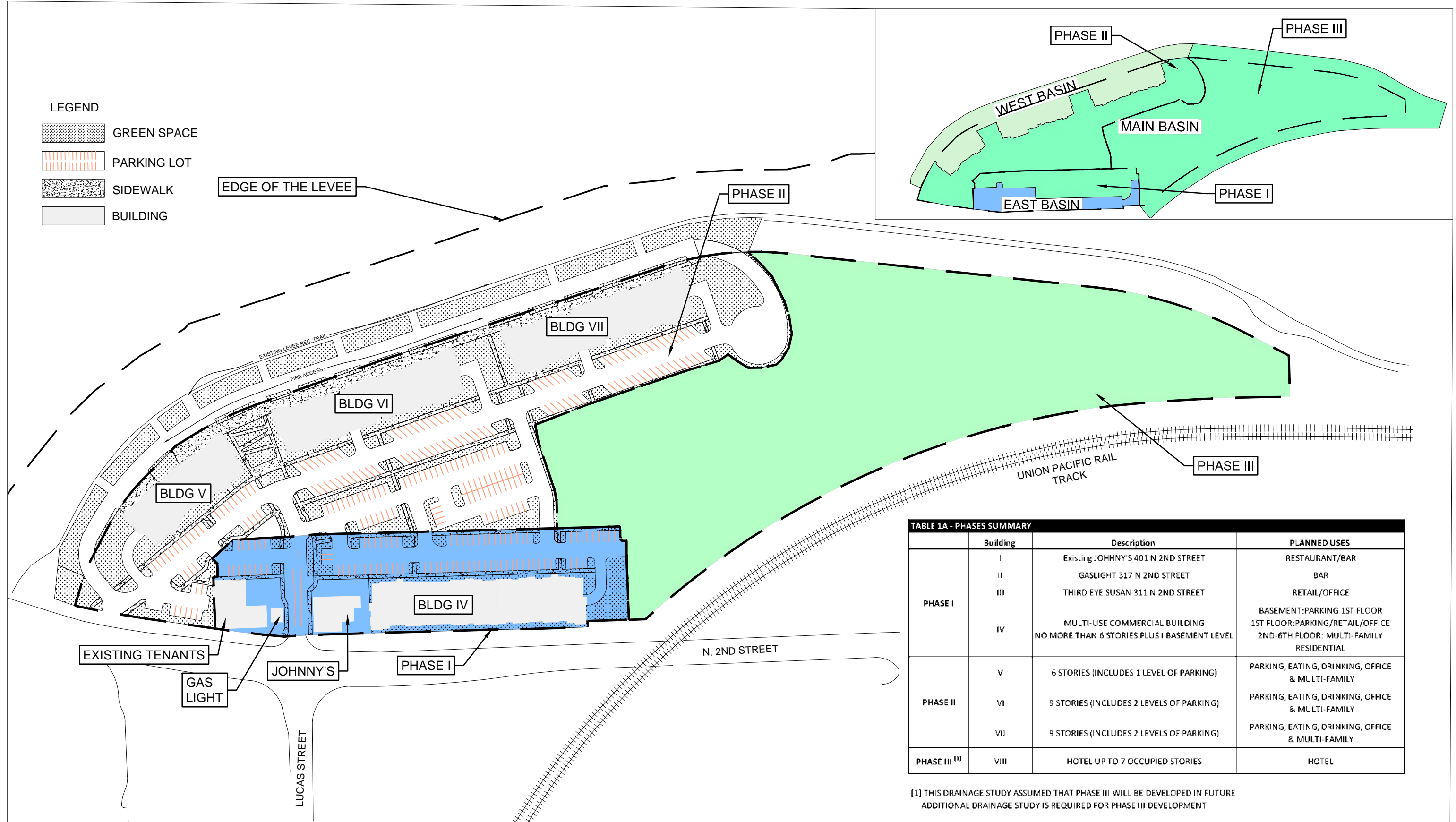


TABLE 1A - PHASES SUMMARY			
	Building	Description	PLANNED USES
PHASE I	I	Existing JOHNNY'S 401 N 2ND STREET	RESTAURANT/BAR
	II	GASLIGHT 317 N 2ND STREET	BAR
	III	THIRD EYE SUSAN 311 N 2ND STREET	RETAIL/OFFICE
	IV	MULTI-USE COMMERCIAL BUILDING NO MORE THAN 6 STORIES PLUS 1 BASEMENT LEVEL	BASEMENT: PARKING 1ST FLOOR 1ST FLOOR: PARKING/RETAIL/OFFICE 2ND-6TH FLOOR: MULTI-FAMILY RESIDENTIAL
PHASE II	V	6 STORIES (INCLUDES 1 LEVEL OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
	VI	9 STORIES (INCLUDES 2 LEVELS OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
	VII	9 STORIES (INCLUDES 2 LEVELS OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
PHASE III ^[1]	VIII	HOTEL UP TO 7 OCCUPIED STORIES	HOTEL

[1] THIS DRAINAGE STUDY ASSUMED THAT PHASE III WILL BE DEVELOPED IN FUTURE
ADDITIONAL DRAINAGE STUDY IS REQUIRED FOR PHASE III DEVELOPMENT

SCALE: 1" = 150'



**NORTH LAWRENCE RIVERFRONT
LAND USES AND PHASING
EXHIBIT 1
OCTOBER, 2018**





MEMORANDUM

TO : City of Lawrence / Lynne Zollner
FROM : Paul Werner
RE : North Lawrence Riverfront Development
DATE : October 15, 2018

The overall project goal is to provide a good mixed use design which allows for places to live, work and play. It will provide areas for residents and the general public to enjoy. The development includes new buildings as well as maintains several of the existing buildings around the entrance and along North Second Street. A portion of the project site is within the environs of the Union Pacific Depot.

Building I - Johnny's Tavern (401 N 2nd Street)

Description of Buildings and Work:

The current Johnny's Tavern consists of a two-story brick building from the 1860s and an addition to the north that was built in 1988.

The original two-story building will remain as Johnny's Tavern. The exterior brick walls are covered by stucco on all elevations except the North. The stucco will be removed on the South elevation where it is already separated from the brick and failing. The stucco appears to be in good condition on the other elevations and will remain. The roof is a simple gable with composition shingles in good condition. The storefronts on the East elevation will remain in their current configuration. The transoms and the north storefront door are covered with wood panel siding. The lower half of the northernmost storefront window was replaced with wood panel siding because the stairs going to the second floor begin here. The second story windows will remain in their current configuration, except the window that is covered with wood siding on the East elevation will be replaced with a wood window to match the adjacent windows. The first floor has a dining area, bar, and kitchen, and the second floor has a private dining room and offices. Both floors are about 2,670 square feet for a total of 5,340 square feet.

The addition to the original Johnny's Tavern was built in 1988 and is attached to the North. It will be demolished to allow for the new smaller additions. It has additional dining area, pizza kitchen, restrooms, and an outdoor covered patio. It is approximately 2,060 square feet with a 900 square feet outdoor covered patio.

A small one-story 360 square foot addition to the West of the existing building is planned for a kitchen addition. This addition will have stucco exterior walls and a flat roof with a parapet to screen rooftop mechanical equipment. It will be set back slightly from the face of the existing building.



A second one-story 1,300 square foot addition is planned to the North of the existing building. This building will have a brick exterior with a flat roof. The building will provide new restrooms and a new entry for the restaurant from the West. It will also enclose the existing stairs to the second floor, which are currently enclosed by the 1988 addition. The East elevation of the addition will have a three-part storefront with a brick bulkhead, aluminum storefront and transom to address the North Second Street frontage. To separate the new addition from the original building and to help accommodate the existing stair location (where it is difficult to locate windows) there is a small recess of brick between the two. Beyond this recess is a small two-story portion that finishes the enclosure for the existing stairs. It will be stucco so to not read as a portion of the East elevation.

Cost Estimate of New Construction for the portion to be demolished:

Based on similar buildings, we estimate the cost to build the 1988 addition building new would be about \$283,200 (2,060 SF x \$120/SF + 900 SF x \$40/SF)

Cost Estimate for Repair:

Any repairs needed for this building are minor, but \$30,000-\$40,000 could be spent to make it more appealing to customers.

Building II - Gaslight (317 N 2nd Street)

Description of Building and Work:

This building, which now operates as a pub, was originally a D-X gas station built sometime in the 1930s. It was built in a cottage house style which was popular for gas stations during that time. The building is in good condition and is planned to remain in its current condition and use.

Building III (311 N 2nd Street)

Description of Building and Work:

The existing building is a one-story limestone L-shaped building with a flat roof. It was most likely built during the 1930s around the time of the neighboring gas station. On the East elevation there are two openings that used to be overhead doors, but are now walled in. The door and two windows on this elevation have been replaced with newer ones and have a wood shake canopy over them. On the North and West elevations there are several more overhead door size openings that have been walled in. There are many steel windows on the various elevations some of which are boarded over or have window units in them.

Overall, the building is in decent condition, and the stone walls look to be sound. Many of the windows are broken or boarded over and would need to be repaired. It also needs a new roof.

The proposed design is to keep the existing building and build three stories of residential units over the top of it. Adding additional uses and density to this building will help bring prominence to this leading corner of the development. The new building will be independently supported by columns outside the footprint of the existing building to not damage the integrity of the existing building. Because the existing building is L-shaped, the new building design fills in the northwest corner to make the first floor rectangular. The first floor would have small commercial spaces and the entry for the units above.

401 N 2nd Street - Grain Elevator

This building is proposed to be demolished.

Description of Existing Property:

The grain elevator was built sometime between 1912 and the 1920s. There is a 480 square foot storage building to the North that looks to have been built at the same time. The elevator and storage buildings are sided in brick patterned sheet metal siding and have standing seam metal roofs. It has not been used as a grain elevator since about the 1940s. It is currently being used as storage.

The sheet metal siding is in varying degrees of rust, some of which need to be replaced. The existing doors need new hardware.

Cost Estimate of New Construction:

The estimated price to build a new 8,000-bushel metal clad grain elevator is about \$112,000. This is based on a State of Michigan appraisal guide. The estimated price to build the attached storage building is about \$24,000 (480 SF x \$50/SF). This is a total of \$136,000.

If viewed as storage only without grain storage, the estimated price to build a new 1,040 square foot storage building is \$62,400 (1,040 SF x \$60/SF).

Cost Estimate for Repair:

The estimated cost to repair the grain elevator and storage building is based on a continued use as storage and does not include the grain storage equipment. Repairs are estimated to be \$31,200 (1,040 SF x \$30/SF).

409 N 2nd Street - Storage

This building is proposed to be demolished.

Description of Existing Property:



The existing building is a one-story brick building on a slab with a flat roof. The front East elevation has two windows and two doors, all boarded over. The West elevation has a garage door. The building has been most recently used for storage and is about 624 square feet.

A building on this site appeared on the Sandborn Maps originally in 1897, but it is a larger building than what is there now. It is unclear when this building was built.

The building needs a new roof and possibly new roof framing as well as replaced doors and windows.

Cost Estimate for New Construction:

The estimated price to build a new storage building is about \$37,440 (624 SF x \$60/SF).

Cost Estimate for Repair:

We estimate the cost to repair this building to be about \$18,720 (624 SF x \$30/SF)

415 N 2nd Street - Shop Building

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a two-story structure on a slab. It has not been occupied for many years and is in disrepair. It is currently covered with horizontal siding on all sides and the windows and doors are boarded up and/or broken. There is only one window to the second floor. The portions of the foundation that are exposed appear to be poured concrete and concrete block. The first floor was most recently a repair shop and the second floor was most recently used for storage.

A building (2 adjoining buildings) in this location does appear on the Sanborn Maps as early as 1905, but it is unclear if those original buildings are still in existence in any form within the current building. The only item visible are some wood brackets at the roof overhang.

The building would need extensive repair. Much of the existing siding needs to be replaced, portions of the interior structure are rotting, and it needs a new roof. The last tenants moved out due to the building's condition.

Cost Estimate for New Construction:

Based on similar buildings, we estimate the cost to build a new building of this size would be \$416,000 (2,600 SF x 2 floors x \$80/SF). (This estimate assumes a wood framed structure, not masonry since there is no current evidence of masonry.)

Cost Estimate for Repair:

We estimate the cost to repair this building for use as a shop and storage facility to be about \$208,000 (2,600 SF x 2 floors x \$40/SF).

501 N 2nd Street - House

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a one-story house on a slab with varying roof lines and wood panel siding. There is an addition to the north with a flat roof. The house is about 1,050 square feet with two bedrooms and one bathroom.

The house is in livable condition, but needs considerable repair to the siding and trim. The roof needs to be replaced entirely. It does not have central AC or heat, but currently uses through-wall units for air conditioning and a gas unit heater.

Cost Estimate for New Construction:

Based on other residential projects, we estimate the cost to build a new house of this size would be about \$115,500 (1,050 SF x \$110/SF).

Cost Estimate for Repair:

We estimate the cost to repair this house to be about \$31,500 (1,050 SF x \$30/SF) to make it slightly more livable.

505 N 2nd Street - House

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a two-story house over a partial crawl space with an attached garage to the north. The house has three bedrooms and one bathroom with about 1,250 square feet on the first floor and 500 square feet on the second floor. It has wood panel siding and a composition tile roof both of which are in reasonable condition.

Cost Estimate for New Construction:



Based on other residential projects, we estimate the cost to build a new house of this size would be about \$192,500 (1,750 SF x \$110/SF).

Cost Estimate for Repair:

We estimate the cost to repair this house to improve its livability to be \$52,500 (1,750 SF x \$30/SF).

Building IV

This building fronts North Second Street and extends from the Johnny's building to the train bridge. It is only 5 stories due to its relationship to the Union Pacific Depot. This is primarily a residential building with only a few office and small retail uses. The first two levels are parking, one level of commercial, and three levels of residential. The sidewalk at the old street level will be maintained along the building's East side.

Building V, VI, and VII

Building five is a multi-story building consisting of one level of parking at grade, one level of commercial, one level of office, and 3 levels of residential for rent. Buildings six and seven are multi-story buildings consisting of two levels of parking at grade, one level of commercial, and six levels of for rent and for purchase residential units.

The height of these buildings is taller than building four since they sit farther away from the Union Pacific Depot, are not in its direct line of sight, and front the river levee to take advantage of views. They may be up to ninety feet tall as allowed by "CD" zoning.



MEMORANDUM

TO : City of Lawrence
FROM : Paul Werner
RE : North Mass – Shared Parking Analysis
DATE : October 15, 2018

Please review the attached tables providing a parking analysis for the North Mass Development.

We have used percentages provided by Planning Staff for these calculations.

We have provided a parking analysis for each of the seven buildings, as well as combined analysis for Buildings I-V, Buildings I-VI, and Buildings I-VII.

The combined analysis of Buildings I-V take in to consideration Phases I and II of the development, and the analysis shows there is going to be enough parking provided to serve these phases.

When adding Building VI, based on the City's sharing factors, the property starts to show a shortfall of parking. This is clearly driven by the Eating & Drinking Establishment percentages in the calculations. The Preliminary Development Plan shows ample opportunity to build additional parking if deemed necessary.

As with the addition of Building VI, Building VII shows the same shortfall of parking. Depending on how the tenants fill in and their exact uses, the development could be short of parking provided. We feel we will have a much greater understanding of the parking needs by the time construction of Building VII is set to begin.

In addition to parking analysis for all the buildings, we have added an alternative calculation using percentages that we feel are more realistic. Due to the number of variables, we look forward to an ongoing review of the parking provided, and what the parking needs are deemed to be.

Please know that the developers want to ensure there is adequate parking for all residents, tenants, customers, and owners to ensure a successful development.

BUILDING I

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
0	Residential	0	0	0	0	0
0	Office	0	0	0	0	0
0	Retail	0	0	0	0	0
0	Hotel	0	0	0	0	0
41	Eating & Drinking	4	0	29	0	33
41	Total:	4	0	29	0	33

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building:

BUILDING II

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
0	Residential	0	0	0	0	0
0	Office	0	0	0	0	0
0	Retail	0	0	0	0	0
0	Hotel	0	0	0	0	0
9	Eating & Drinking	1	0	6	0	7
9	Total:	1	0	6	0	7

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building:

BUILDING III

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
26	Residential	26	16	23	21	23
6	Office	0	6	1	1	0
10	Retail	1	7	9	10	7
0	Hotel	0	0	0	0	0
0	Eating & Drinking	0	0	0	0	0
42	Total:	27	29	33	31	31

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building:

BUILDING IV

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
152	Residential	152	91	137	122	137
10	Office	1	10	1	1	1
7	Retail	0	5	6	7	5
0	Hotel	0	0	0	0	0
12	Eating & Drinking	1	0	8	0	10
181	Total:	154	106	153	130	152

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building: 98

BUILDING V

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
63	Residential	63	38	57	50	57
47	Office	2	47	5	5	2
23	Retail	1	16	21	23	16
0	Hotel	0	0	0	0	0
50	Eating & Drinking	5	0	35	0	40
183	Total:	72	101	117	78	115

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building: 32

BUILDING VI

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
202	Residential	202	121	182	162	182
10	Office	1	10	1	1	1
30	Retail	2	21	27	30	21
0	Hotel	0	0	0	0	0
50	Eating & Drinking	5	0	35	0	40
292	Total:	209	152	245	193	243

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building: 96

BUILDING VII

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
202	Residential	202	121	182	162	182
10	Office	1	10	1	1	1
30	Retail	2	21	27	30	21
0	Hotel	0	0	0	0	0
50	Eating & Drinking	5	0	35	0	40
292	Total:	209	152	245	193	243

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establi	10%	0%	70%	0%	80%

Spaces Provided on Interior of Building: 96

Combined - Buildings I-V

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Night</u>	<u>Weekday</u>		<u>Weekend</u>	
			<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
241	Residential	241	145	217	193	217
63	Office	3	63	6	6	3
40	Retail	2	28	36	40	28
0	Hotel	0	0	0	0	0
112	Eating & Drinking	11	0	78	0	90
456	Total:	257	236	338	239	338

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establish	10%	0%	70%	0%	80%

Spaces Provided on Interior of Buildings:	130
Spaces Provided on Exterior Parking Lots:	213
Total Parking Provided:	343

Combined - Buildings I-VI

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
443	Residential	443	266	399	354	399
73	Office	4	73	7	7	4
70	Retail	4	49	63	70	49
0	Hotel	0	0	0	0	0
162	Eating & Drinking	16	0	113	0	130
748	Total:	466	388	582	432	581

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establish	10%	0%	70%	0%	80%

Spaces Provided on Interior of Buildings:	226
Spaces Provided on Exterior Parking Lots:	262
Total Parking Provided:	488

Combined - Buildings I-VII

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
645	Residential	645	387	581	516	581
83	Office	4	83	8	8	4
100	Retail	5	70	90	100	70
0	Hotel	0	0	0	0	0
212	Eating & Drinking	21	0	148	0	170
1,040	Total:	675	540	827	624	824

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	90%	100%	70%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establish	10%	0%	70%	0%	80%

Spaces Provided on Interior of Buildings:	322
Spaces Provided on Exterior Parking Lots:	308
Total Parking Provided:	630
Additional (Dashed) Parking Shown:	95
Total Including Additional Parking:	725

Combined - All Buildings (Alternate Percentages)

Shared Parking Analysis

Parking Calculation - Shared Factor Applied

<u>Required Parking</u>	<u>Use Category</u>	<u>Weekday</u>			<u>Weekend</u>	
		<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
645	Residential	645	387	581	516	581
83	Office	4	83	8	8	4
100	Retail	5	70	70	100	60
0	Hotel	0	0	0	0	0
212	Eating & Drinking	11	11	106	21	148
1,040	Total:	665	551	765	646	793

City Shared Parking Table:

	<u>Weekday</u>			<u>Weekend</u>	
	<u>Night</u>	<u>Day</u>	<u>Evening</u>	<u>Day</u>	<u>Evening</u>
	2am - 8am	8am - 5pm	5pm - 12am	8am - 5pm	5pm - 2am
Residential	100%	60%	90%	80%	90%
Office	5%	100%	10%	10%	5%
Retail	5%	70%	70%	100%	60%
Hotel	80%	55%	100%	50%	100%
Eating & Drinking Establish	5%	5%	50%	10%	70%

Spaces Provided on Interior of Buildings:	322
Spaces Provided on Exterior Parking Lots:	308
Total Parking Provided:	630
Additional (Dashed) Parking Shown:	95
Total Including Additional Parking:	725



MEMORANDUM

TO : City of Lawrence / Lynne Zollner
FROM : Paul Werner
RE : North Lawrence Riverfront Development
DATE : October 15, 2018

The overall project goal is to provide a good mixed use design which allows for places to live, work and play. It will provide areas for residents and the general public to enjoy. The development includes new buildings as well as maintains several of the existing buildings around the entrance and along North Second Street. A portion of the project site is within the environs of the Union Pacific Depot.

Building I - Johnny's Tavern (401 N 2nd Street)

Description of Buildings and Work:

The current Johnny's Tavern consists of a two-story brick building from the 1860s and an addition to the north that was built in 1988.

The original two-story building will remain as Johnny's Tavern. The exterior brick walls are covered by stucco on all elevations except the North. The stucco will be removed on the South elevation where it is already separated from the brick and failing. The stucco appears to be in good condition on the other elevations and will remain. The roof is a simple gable with composition shingles in good condition. The storefronts on the East elevation will remain in their current configuration. The transoms and the north storefront door are covered with wood panel siding. The lower half of the northernmost storefront window was replaced with wood panel siding because the stairs going to the second floor begin here. The second story windows will remain in their current configuration, except the window that is covered with wood siding on the East elevation will be replaced with a wood window to match the adjacent windows. The first floor has a dining area, bar, and kitchen, and the second floor has a private dining room and offices. Both floors are about 2,670 square feet for a total of 5,340 square feet.

The addition to the original Johnny's Tavern was built in 1988 and is attached to the North. It will be demolished to allow for the new smaller additions. It has additional dining area, pizza kitchen, restrooms, and an outdoor covered patio. It is approximately 2,060 square feet with a 900 square feet outdoor covered patio.

A small one-story 360 square foot addition to the West of the existing building is planned for a kitchen addition. This addition will have stucco exterior walls and a flat roof with a parapet to screen rooftop mechanical equipment. It will be set back slightly from the face of the existing building.



A second one-story 1,300 square foot addition is planned to the North of the existing building. This building will have a brick exterior with a flat roof. The building will provide new restrooms and a new entry for the restaurant from the West. It will also enclose the existing stairs to the second floor, which are currently enclosed by the 1988 addition. The East elevation of the addition will have a three-part storefront with a brick bulkhead, aluminum storefront and transom to address the North Second Street frontage. To separate the new addition from the original building and to help accommodate the existing stair location (where it is difficult to locate windows) there is a small recess of brick between the two. Beyond this recess is a small two-story portion that finishes the enclosure for the existing stairs. It will be stucco so to not read as a portion of the East elevation.

Cost Estimate of New Construction for the portion to be demolished:

Based on similar buildings, we estimate the cost to build the 1988 addition building new would be about \$283,200 (2,060 SF x \$120/SF + 900 SF x \$40/SF)

Cost Estimate for Repair:

Any repairs needed for this building are minor, but \$30,000-\$40,000 could be spent to make it more appealing to customers.

Building II - Gaslight (317 N 2nd Street)

Description of Building and Work:

This building, which now operates as a pub, was originally a D-X gas station built sometime in the 1930s. It was built in a cottage house style which was popular for gas stations during that time. The building is in good condition and is planned to remain in its current condition and use.

Building III (311 N 2nd Street)

Description of Building and Work:

The existing building is a one-story limestone L-shaped building with a flat roof. It was most likely built during the 1930s around the time of the neighboring gas station. On the East elevation there are two openings that used to be overhead doors, but are now walled in. The door and two windows on this elevation have been replaced with newer ones and have a wood shake canopy over them. On the North and West elevations there are several more overhead door size openings that have been walled in. There are many steel windows on the various elevations some of which are boarded over or have window units in them.

Overall, the building is in decent condition, and the stone walls look to be sound. Many of the windows are broken or boarded over and would need to be repaired. It also needs a new roof.

The proposed design is to keep the existing building and build three stories of residential units over the top of it. Adding additional uses and density to this building will help bring prominence to this leading corner of the development. The new building will be independently supported by columns outside the footprint of the existing building to not damage the integrity of the existing building. Because the existing building is L-shaped, the new building design fills in the northwest corner to make the first floor rectangular. The first floor would have small commercial spaces and the entry for the units above.

401 N 2nd Street - Grain Elevator

This building is proposed to be demolished.

Description of Existing Property:

The grain elevator was built sometime between 1912 and the 1920s. There is a 480 square foot storage building to the North that looks to have been built at the same time. The elevator and storage buildings are sided in brick patterned sheet metal siding and have standing seam metal roofs. It has not been used as a grain elevator since about the 1940s. It is currently being used as storage.

The sheet metal siding is in varying degrees of rust, some of which need to be replaced. The existing doors need new hardware.

Cost Estimate of New Construction:

The estimated price to build a new 8,000-bushel metal clad grain elevator is about \$112,000. This is based on a State of Michigan appraisal guide. The estimated price to build the attached storage building is about \$24,000 (480 SF x \$50/SF). This is a total of \$136,000.

If viewed as storage only without grain storage, the estimated price to build a new 1,040 square foot storage building is \$62,400 (1,040 SF x \$60/SF).

Cost Estimate for Repair:

The estimated cost to repair the grain elevator and storage building is based on a continued use as storage and does not include the grain storage equipment. Repairs are estimated to be \$31,200 (1,040 SF x \$30/SF).

409 N 2nd Street - Storage

This building is proposed to be demolished.

Description of Existing Property:



The existing building is a one-story brick building on a slab with a flat roof. The front East elevation has two windows and two doors, all boarded over. The West elevation has a garage door. The building has been most recently used for storage and is about 624 square feet.

A building on this site appeared on the Sandborn Maps originally in 1897, but it is a larger building than what is there now. It is unclear when this building was built.

The building needs a new roof and possibly new roof framing as well as replaced doors and windows.

Cost Estimate for New Construction:

The estimated price to build a new storage building is about \$37,440 (624 SF x \$60/SF).

Cost Estimate for Repair:

We estimate the cost to repair this building to be about \$18,720 (624 SF x \$30/SF)

415 N 2nd Street - Shop Building

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a two-story structure on a slab. It has not been occupied for many years and is in disrepair. It is currently covered with horizontal siding on all sides and the windows and doors are boarded up and/or broken. There is only one window to the second floor. The portions of the foundation that are exposed appear to be poured concrete and concrete block. The first floor was most recently a repair shop and the second floor was most recently used for storage.

A building (2 adjoining buildings) in this location does appear on the Sanborn Maps as early as 1905, but it is unclear if those original buildings are still in existence in any form within the current building. The only item visible are some wood brackets at the roof overhang.

The building would need extensive repair. Much of the existing siding needs to be replaced, portions of the interior structure are rotting, and it needs a new roof. The last tenants moved out due to the building's condition.

Cost Estimate for New Construction:

Based on similar buildings, we estimate the cost to build a new building of this size would be \$416,000 (2,600 SF x 2 floors x \$80/SF). (This estimate assumes a wood framed structure, not masonry since there is no current evidence of masonry.)

Cost Estimate for Repair:

We estimate the cost to repair this building for use as a shop and storage facility to be about \$208,000 (2,600 SF x 2 floors x \$40/SF).

501 N 2nd Street - House

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a one-story house on a slab with varying roof lines and wood panel siding. There is an addition to the north with a flat roof. The house is about 1,050 square feet with two bedrooms and one bathroom.

The house is in livable condition, but needs considerable repair to the siding and trim. The roof needs to be replaced entirely. It does not have central AC or heat, but currently uses through-wall units for air conditioning and a gas unit heater.

Cost Estimate for New Construction:

Based on other residential projects, we estimate the cost to build a new house of this size would be about \$115,500 (1,050 SF x \$110/SF).

Cost Estimate for Repair:

We estimate the cost to repair this house to be about \$31,500 (1,050 SF x \$30/SF) to make it slightly more livable.

505 N 2nd Street - House

This building is proposed to be demolished.

Description of Existing Property:

The existing building is a two-story house over a partial crawl space with an attached garage to the north. The house has three bedrooms and one bathroom with about 1,250 square feet on the first floor and 500 square feet on the second floor. It has wood panel siding and a composition tile roof both of which are in reasonable condition.

Cost Estimate for New Construction:



Based on other residential projects, we estimate the cost to build a new house of this size would be about \$192,500 (1,750 SF x \$110/SF).

Cost Estimate for Repair:

We estimate the cost to repair this house to improve its livability to be \$52,500 (1,750 SF x \$30/SF).

Building IV

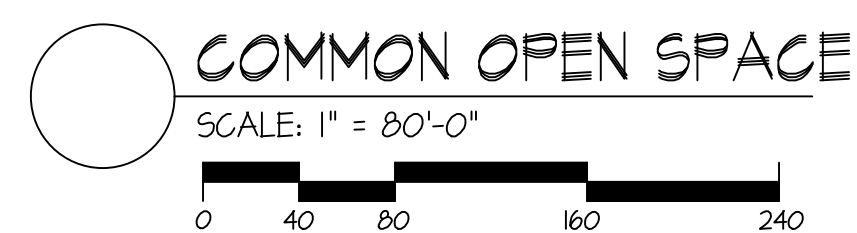
This building fronts North Second Street and extends from the Johnny's building to the train bridge. It is only 5 stories due to its relationship to the Union Pacific Depot. This is primarily a residential building with only a few office and small retail uses. The first two levels are parking, one level of commercial, and three levels of residential. The sidewalk at the old street level will be maintained along the building's East side.

Building V, VI, and VII

Building five is a multi-story building consisting of one level of parking at grade, one level of commercial, one level of office, and 3 levels of residential for rent. Buildings six and seven are multi-story buildings consisting of two levels of parking at grade, one level of commercial, and six levels of for rent and for purchase residential units.

The height of these buildings is taller than building four since they sit farther away from the Union Pacific Depot, are not in its direct line of sight, and front the river levee to take advantage of views. They may be up to ninety feet tall as allowed by "CD" zoning.

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**NORTH LAWRENCE RIVERFRONT
PRELIMINARY
DEVELOPMENT PLAN**
LAWRENCE, KANSAS

PROJECT # 217-350
AUGUST 2018

RELEASE: 1.0 DATE: 10.15.18

EXHIBIT

DRAINAGE STUDY

for

North Lawrence Riverfront

Preliminary Plat

Lawrence, Kansas

October 2018

LPE Project No. 20182022

Prepared for:

PAUL WERNER ARCHITECTS

Prepared by:

Landplan Engineering, P.A.

Lawrence, KS



Shawn R. Bryan, PE
Kansas License 17899

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GENERAL

North Lawrence Riverfront property is located entirely in Lawrence, Kansas. The property is bounded by Pacific Union railroad track to the North and East, N 2nd Street to the East, and Kansas River Levee to the South and West. The current zoning for the 16.11 acres property is intended to be Mixed Use (MU) including multi-dwelling, and commercial uses while current land uses include Industrial (IG), Commercial (CS) uses along with Open spaces (OS). According to the North Lawrence Watershed Drainage Study (2005), the property lies within the SYSTEM 3 tributary area. This system has a gravity flow outlet through the Kansas River levee for approximately a 20-acre area. The outlet pipe is a 48" RCP pipe located on the west side of the property which discharges to the Kansas river via a sluice gate constructed in the Kansas River Levee in the upstream of the Bowersock Dam protecting the site from high water level in the Kansas River during its flood time (mean annual WSE=814.0).

FLOODPLAIN INFORMATION

Most of the site lies within areas with reduced flood risk due to river levee (Zone X) as depicted on the FEMA Flood Insurance Rate Map (FIRM) Community Panel No. 200090 0176 E (City of Lawrence), Revised: September 2, 2015. According to this Panel, the east of the levee, Kansas River, is located within regulatory floodway with Base Flood Elevation (BFE) equal to 827.7 from sea level. Also, small portion of the site located in the west of N. 2nd street between Locust Street and Pacific Union railroad tracks and east of N Massachusetts Street, is identified as special flood hazard area (ZONE AH) with BFE equal to 820.0 subjected to inundation by the 1% annual chance, according to the FEMA FIRM Panel. A copy of the FEMA FIRM is included in Appendix D.

EXISTING CONDITIONS

The existing site consists of mostly open spaces, bounded by the public and private roadways and properties. Miscellaneous brush and trees are present along the East and the North East property. The site runoff is split between two discharge points. A majority of the onsite and offsite runoff for approximately 19 acres is conveyed via swales and pipes in a flat and interrupted drainage network located in the Perry, N 1st, and Maple street in the north of the site and N Massachusetts street in the south of the site, discharging into the 48" RCP pipe storm sewer crossing Kansas River Levee. No formal storm water management system currently exists on this part of the site, and the runoff is discharged directly to the Kansas river via various conveyance systems described above.

An approximate 1 acre of the site, as a small portion, located in the West of the N 2nd street and East of N Massachusetts street discharges to the West of N 2nd street right-of-way. This portion includes properties located in 311 N 2nd Street (known as The Last Carnival), 317 N 2nd street (known as Gaslight Gardens), 401 N 2nd street (known as Johnny's Tavern), along with 415 N 2nd street. As described above, this portion is confined in the 100-year floodplain (FEMA Zone AH), discharges via sheet flow and shallow concentrated flow to the N 2nd street and Locust street right-of-way. The only existing drainage storm inlet in this part of the site is located in the East of the N Massachusetts street right-of-way collects low flows from this part and then connected to a storm junction box in the Locust street which ultimately discharges to a 36" CMP pipe located in the west side of N 2nd street. The storm runoff of this part of the site ultimately discharges into Kansas River at the downstream of Bowersock Dam.

The site soil is highly permeable and includes Eudora-Urban land complex, rarely flooded (7119). The Eudora-Urban land complex is classified as the hydrologic group B soil which was used for this drainage study. Refer to Figure 1 for soil maps from the USDA Websoil Survey website.

PROPOSED IMPROVEMENTS

The proposed improvements for the property was planned onto three phases. The initial development includes proposed improvements for Phase I and Phase II while future improvements on the site, named Phase III are expected on the north side of the site. Phase I and Phase II include constructing several multi-story residential-commercial buildings, impervious parking lots, site access pavements and green

spaces while Phase III includes building a multi-story hotel. Refer to table 1A and Exhibit 1. The proposed improvement divides the site into three drainage basins including East, Main, and West Basin. The East Basin is located within the FEMA floodplain Zone AH while the Main basin drains to the Kansas River via the existing 48" RCP mentioned above.. A detention facility has been only sized for Main basin to accommodate and retain a typical SCS 24 hours-100 year stormwater runoff when Kansas River is under flooding, and gravity discharges to the river is not possible. The detention pond is connected to the 48' RCP existing pipe via two 36" RCP pipes which function as equalizer and conveyance pipes. West Basin directly drains to the Kansas river via rooftops drains and existing Levee trail above the top of the levee. Also, refer to Tables 1 through 3 on Page 3-5 for summaries of CN calculations, time of concentration calculations, and hydrologic modeling results for post condition for associated subbasins. Refer to Exhibit 2 for information on drainage areas and detention pond layout.

ANALYSIS

Times of concentration was calculated based on the Lawrence Stormwater Management Criteria (Feb 1996) using overland flow, shallow concentrated flow, and channel/system flow. The CN for each drainage area is based on a weighted average of pervious and impervious areas for hydrologic group B soils. All storm routing calculations were performed using Autodesk Storm and Sanitary Analysis ® software. Peak flows for 100-year storm events were modeled using SCS Type II 24-hr hyetograph storm based on 2014 rainfall intensities for Douglas County supplied by the Kansas Department of Transportation.

CONCLUSION

This study indicates that the proposed detention option in the Main Basin provide adequate detention for protecting the site from flooding due to increased runoff and blockage of gravity flow of its outlet resulting from Kansas River flooding period.

TABLE 1A - PHASES SUMMARY

	Building	Description	PLANNED USES
PHASE I	I	Existing JOHNNY'S 401 N 2ND STREET	RESTAURANT/BAR
	II	GASLIGHT 317 N 2ND STREET	BAR
	III	THIRD EYE SUSAN 311 N 2ND STREET	RETAIL/OFFICE
	IV	MULTI-USE COMMERCIAL BUILDING NO MORE THAN 6 STORIES PLUS I BASEMENT LEVEL	BASEMENT: PARKING 1ST FLOOR 1ST FLOOR: PARKING/RETAIL/OFFICE 2ND-6TH FLOOR: MULTI-FAMILY RESIDENTIAL
PHASE II	V	6 STORIES (INCLUDES 1 LEVEL OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
	VI	9 STORIES (INCLUDES 2 LEVELS OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
	VII	9 STORIES (INCLUDES 2 LEVELS OF PARKING)	PARKING, EATING, DRINKING, OFFICE & MULTI-FAMILY
PHASE III ^[1]	VIII	HOTEL UP TO 7 OCCUPIED STORIES	HOTEL

[1] THIS DRAINAGE STUDY ASSUMED THAT PHASE III WILL BE DEVELOPED IN FUTURE
THIS DRAINAGE STUDY SHALL BE REVISED FOR PHASE III DEVELOPMENT

TABLE 1B - BASINS CURVE NUMBER CALCULATIONS SUMMARY

		SCS CURVE NUMBER		COMP.	
BASIN	SUB BASIN	73	98	AREA	CN
EAST BASIN					
ONSITE					
	A1-1	0.23 ac	0.35 ac	0.58 ac	88.1
	A1-2	0.19 ac	0.32 ac	0.51 ac	88.7
TOTAL				1.09 ac	88.4
MAIN BASIN					
ONSITE					
	A1-3	0.19 ac	1.24 ac	1.43 ac	94.7
	A2-1	1.37 ac	3.12 ac	4.49 ac	90.4
	A3 ^[2]	6.19 ac	1.02 ac	7.21 ac	76.5
OFFSITE					
	B3 ^[2]	3.00 ac	0.00 ac	3.00 ac	73.0
TOTAL				16.13 ac	81.3
WEST BASIN					
ONSITE					
	A2-2 ^[2]	0.10 ac	1.79 ac	1.89 ac	96.7
OFFSITE					
	B2 ^[2]	0.09 ac	1.63 ac	1.72 ac	96.7
TOTAL				3.61 ac	96.7
Grand Total				20.83 ac	84.37
ONSITE				16.11 ac	
OFFSITE				4.72 ac	

[2] For Hydrological Calculation a basin area equal to the total area of Onsite and Offsite Basins were used

TABLE 2 - TIMES OF CONCENTRATION SUMMARY

	OVERLAND FLOW				SHALLOW CONCENTRATED FLOW			CHANNEL/SYSTEM FLOW				TIME OF CONC.
SUB BASIN	C	D	S	T(OLF)	D	S	T(SCF)	D	S	V	T(C/S)	
EAST BASIN	A1-1	0.7	134 ft	4.3%	5.1 min							5.1 min
	A1-2	0.7	74 ft	1.0%	6.2 min							6.2 min
MAIN BASIN	A1-3	0.8	72 ft	1.0%	4.6 min			700 ft	0.5%	3 fps	3.9 min	8.5 min
	A2-1	0.9	50 ft	1.0%	2.5 min	Paved	233 ft	417 ft	0.5%	3 fps	2.3 min	5.8 min
	A3 & B3 ^[2]	0.3	75 ft	8.0%	6.2 min	Unpaved	400 ft	150 ft	0.5%	2 fps	1.3 min	11.6 min
WEST BASIN	A2-2 & B2 ^[2]	0.9	80 ft	1.0%	3.2 min	Paved	25 ft	190 ft	0.5%	2 fps	1.6 min	5.0 min

[2] For Hydrological Calculation a basin area equal to the total area of Onsite and Offsite Basins were used

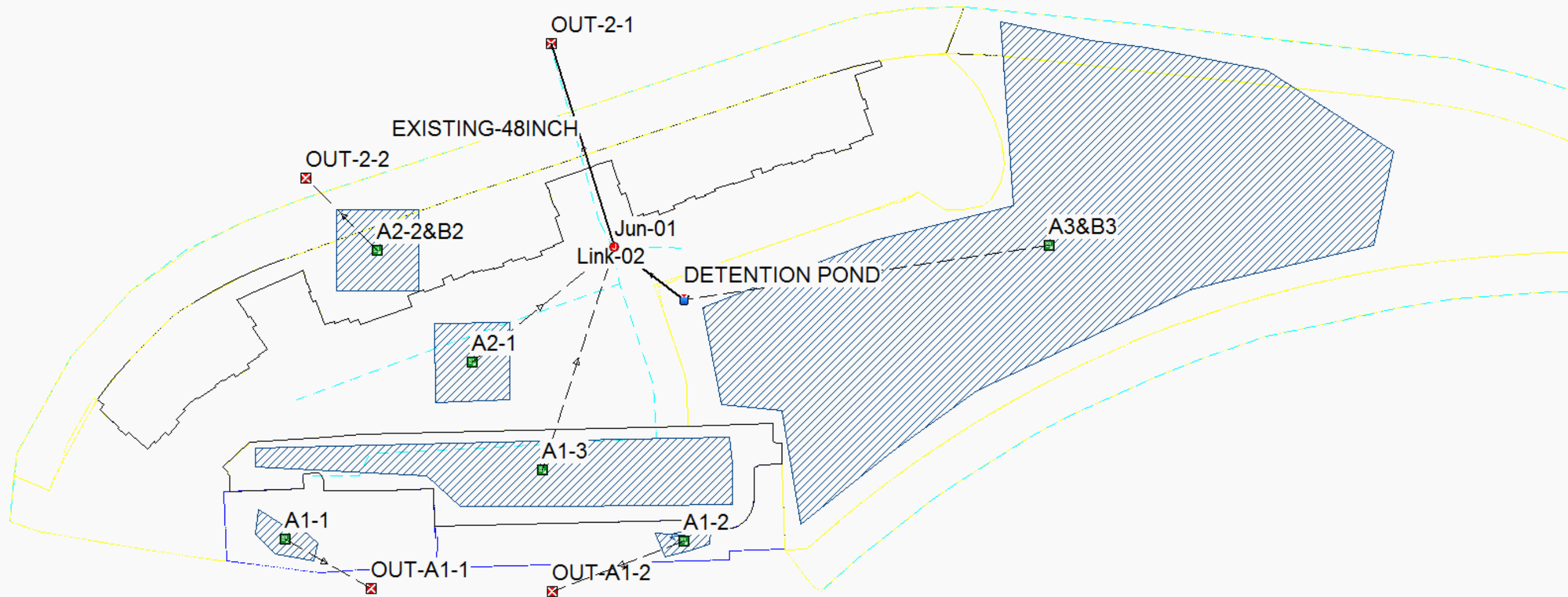
TABLE 3 - SUMMARY OF HYDROLOGIC MODELING AND DISCHARGE		
	Hydrograph	100-YR
EAST BASIN		
A1-1	A1-1	5.8 cfs
A1-2	A1-2	5.0 cfs
MAIN BASIN		
A1-3	A1-3	13.8 cfs
A2-1	A2-1	45.1 cfs
A3 & B3	A3 & B3	71.0 cfs
TOTAL DISCHARGE	OUT2-1	36.3 cfs
WEST BASIN		
A2-2 & B2	A2-2 & B2	38.6 cfs

Hydrologic Soil Group—Douglas County, Kansas
(North Lawrence)

FIGURE 1



MODEL LAYOUT



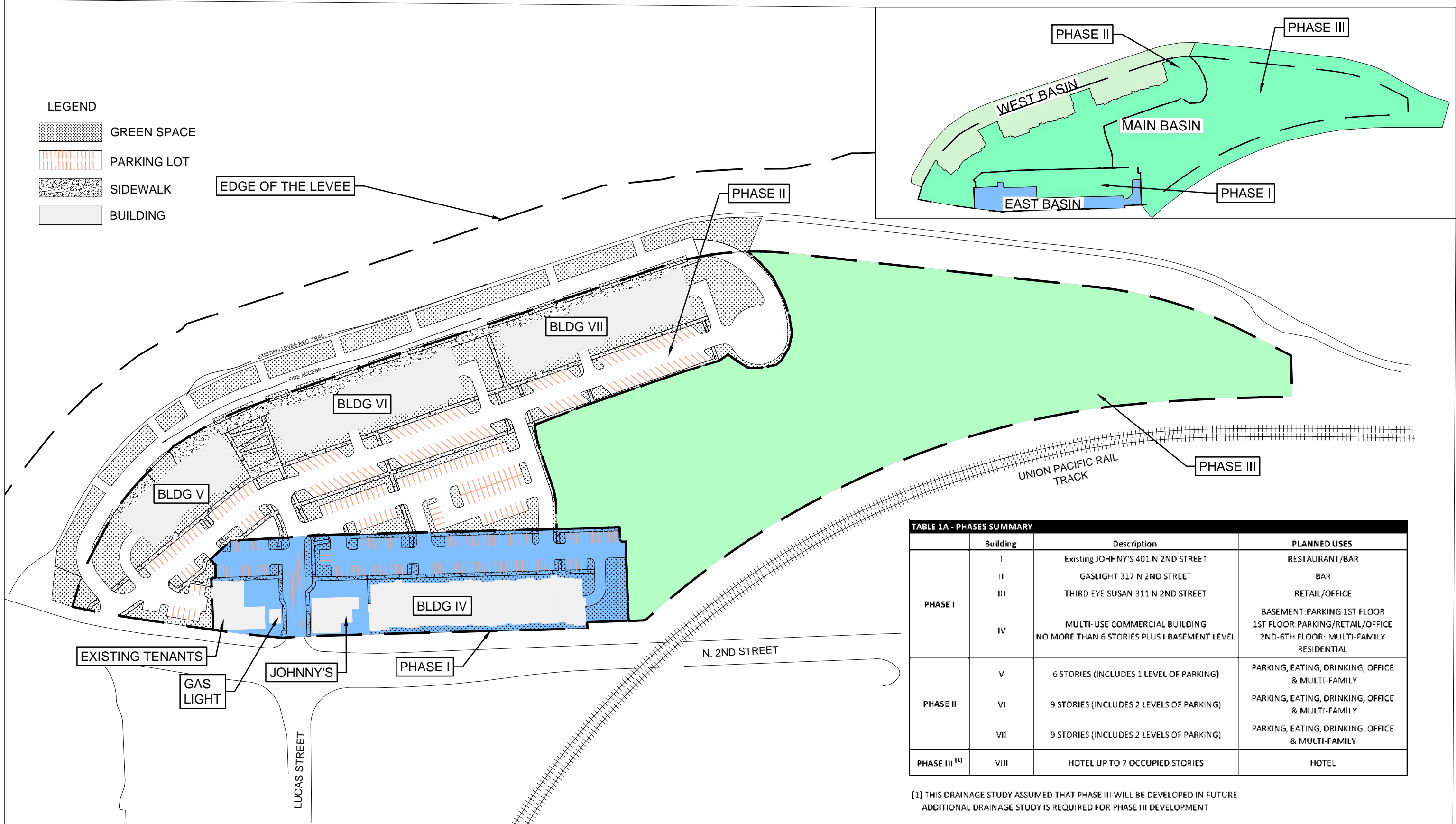
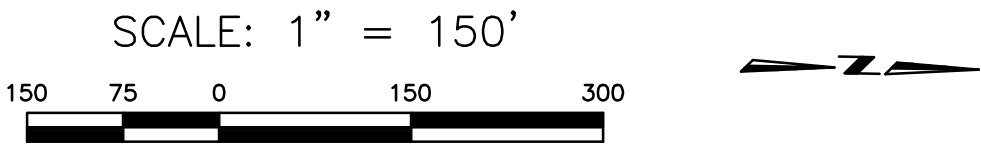


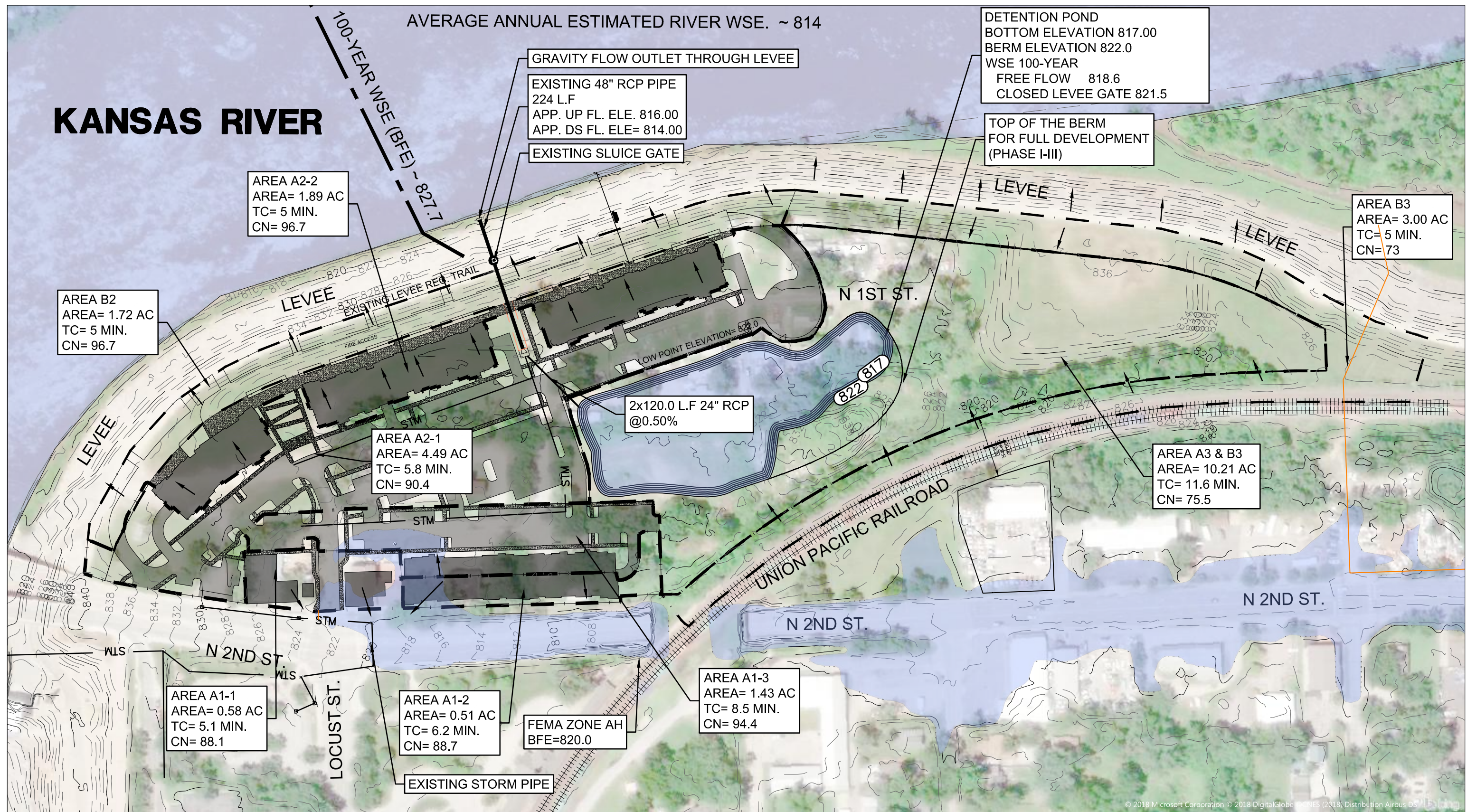
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PHASE III ^[1]	VIII	HOTEL UP TO 7 OCCUPIED STORIES	HOTEL

[1] THIS DRAINAGE STUDY ASSUMED THAT PHASE III WILL BE DEVELOPED IN FUTURE
ADDITIONAL DRAINAGE STUDY IS REQUIRED FOR PHASE III DEVELOPMENT



NORTH LAWRENCE RIVERFRONT
LAND USES AND PHASING
EXHIBIT 1
OCTOBER, 2018





APPENDIX A

DETENTION POND SIZING

Summary of Design Data

Proposed Condition:

Assumed case:

Sluice gate on West of the site is closed.

Detention pond should have enough volume for 100-year, 24-hour precipitation

1. Complete Development of Phase I, II

Detainable Drainage Area = 16.13 Acres (CN=82)

Including Following Basins

Onsite A1-3 (1.43 ac. CN=94.7)

Onsite A2-1 (4.49 ac. CN=90.4)

Onsite A3 (7.21 ac. CN=76.5)

Offsite B3 (3.00 ac. CN=73)

Runoff Volume from 100-Year, 24-Hour Storm Event (SCS-TR-55)

Soil types (NRCS): Eudora-Urban land. Soils is in hydrologic group 'B'

Weight SCS curve number (CN) = 82

100-year, 24-hour precipitation (P) = 8.16 inches

$S = (1000/CN) - 10 = 2.19$ Inch

$Q = (P - 0.2S)^2 / (P + 0.8S) = 6.01$ Inch

Converted to volume (V) of rainfall: 6.01 Inches x 1/12 x 16.13 = 8.07 Acre-feet

Required Detention Volume: 8.10 Acre-feet

2. Complete Development of Phase I, II, and III

Detainable Drainage Area = 16.13 Acres (CN=88)

Including Following Basins

Onsite A1-3 (1.43 ac. CN=94.7)

Onsite A2-1 (4.49 ac. CN=90.4)

Onsite A3 (7.21 ac. CN=95)

Offsite B3 (3.00 ac. CN=73)

Weight SCS curve number (CN) = 90

100-year, 24-hour precipitation (P) = 8.16 inches

$S = (1000/CN) - 10 = 1.11$ Inch

$Q = (P - 0.2S)^2 / (P + 0.8S) = 6.96$ Inch

Converted to volume (V) of rainfall: 6.96 Inches x 1/12 x 16.18 = 9.35 Acre-feet

DETENTION POND STORAGE CURVE (DEPTH VS AREA)

1. Complete Development of Phase I, II

ELEVATION	DEPTH (ft)	AREA (ft ²)	VOLUME (ft ³)	VOLUME (ac-ft)
817	0	66526	0	0
818	1	70333	68429.50	1.57
819	2	74197	140694.50	3.23
820	3	78117	216851.50	4.98
821	4	82093	296956.50	6.82
822	5	86126	381066.00	8.75

2. Complete Development of Phase I, II, III

ELEVATION	DEPTH (ft)	AREA (ft ²)	VOLUME (ft ³)	VOLUME (ac-ft)
817	0	69000	0	0
818	1	73334	71167.00	1.63
819	2	77940	146804.00	3.37
820	3	82835	227191.50	5.22
821	4	88039	312628.50	7.18
822	5	93568	403432.00	9.26

APPENDIX B
100-YEAR STORM EVENT
POST CONDITION
INITIAL DEVELOPMENT (PHASE I &II)

Project Description

File Name 20182022-Drainage_ PH I & PH II.SPF

Project Options

Flow Units CFS
Elevation Type Elevation
Hydrology Method SCS TR-55
Time of Concentration (TOC) Method Kirpich
Link Routing Method Hydrodynamic
Enable Overflow Ponding at Nodes YES
Skip Steady State Analysis Time Periods .. NO

Analysis Options

Start Analysis On Oct 05, 2018 00:00:00
End Analysis On Oct 07, 2018 00:00:00
Start Reporting On Oct 05, 2018 00:00:00
Antecedent Dry Days 0 days
Runoff (Dry Weather) Time Step 0 01:00:00 days hh:mm:ss
Runoff (Wet Weather) Time Step 0 00:05:00 days hh:mm:ss
Reporting Time Step 0 00:10:00 days hh:mm:ss
Routing Time Step 30 seconds

Number of Elements

Qty
Rain Gages 1
Subbasins..... 6
Nodes..... 6
 Junctions 1
 Outfalls 4
 Flow Diversions 0
 Inlets 0
 Storage Nodes 1
Links..... 2
 Channels 0
 Pipes 2
 Pumps 0
 Orifices 0
 Weirs 0
 Outlets 0
Pollutants 0
Land Uses 0

Rainfall Details

SN	Rain Gage ID	Data Source	Data Source ID	Rainfall Type	Rain Units	State	County	Return Period (years)	Rainfall Depth (inches)	Rainfall Distribution
1		Time Series	TS-100	Year Intensity	inches	Kansas	Douglas	100	8.16	SCS Type II 24-hr

Subbasin Summary

SN	Subbasin ID	Area	Weighted Curve Number	Average Slope	Flow Length	Total Rainfall	Total Runoff	Total Runoff Volume	Peak Runoff	Time of Concentration
		(ac)		(%)	(ft)	(in)	(in)	(ac-in)	(cfs)	(days hh:mm:ss)
1	A1-1	0.58	88.10	0.5000	500.00	8.16	6.74	3.91	5.82	0 00:05:06
2	A1-2	0.51	88.70	0.5000	500.00	8.16	6.81	3.47	5.00	0 00:06:12
3	A1-3	1.43	94.70	0.5000	500.00	8.16	7.53	10.76	13.76	0 00:08:30
4	A2-1	4.49	90.40	0.5000	500.00	8.16	7.01	31.48	45.13	0 00:05:48
5	A2-2&B2	3.61	96.70	0.5000	500.00	8.16	7.76	28.03	38.60	0 00:05:00
6	A3&B3	10.21	75.50	0.5000	500.00	8.16	5.25	53.55	71.01	0 00:11:36

Node Summary

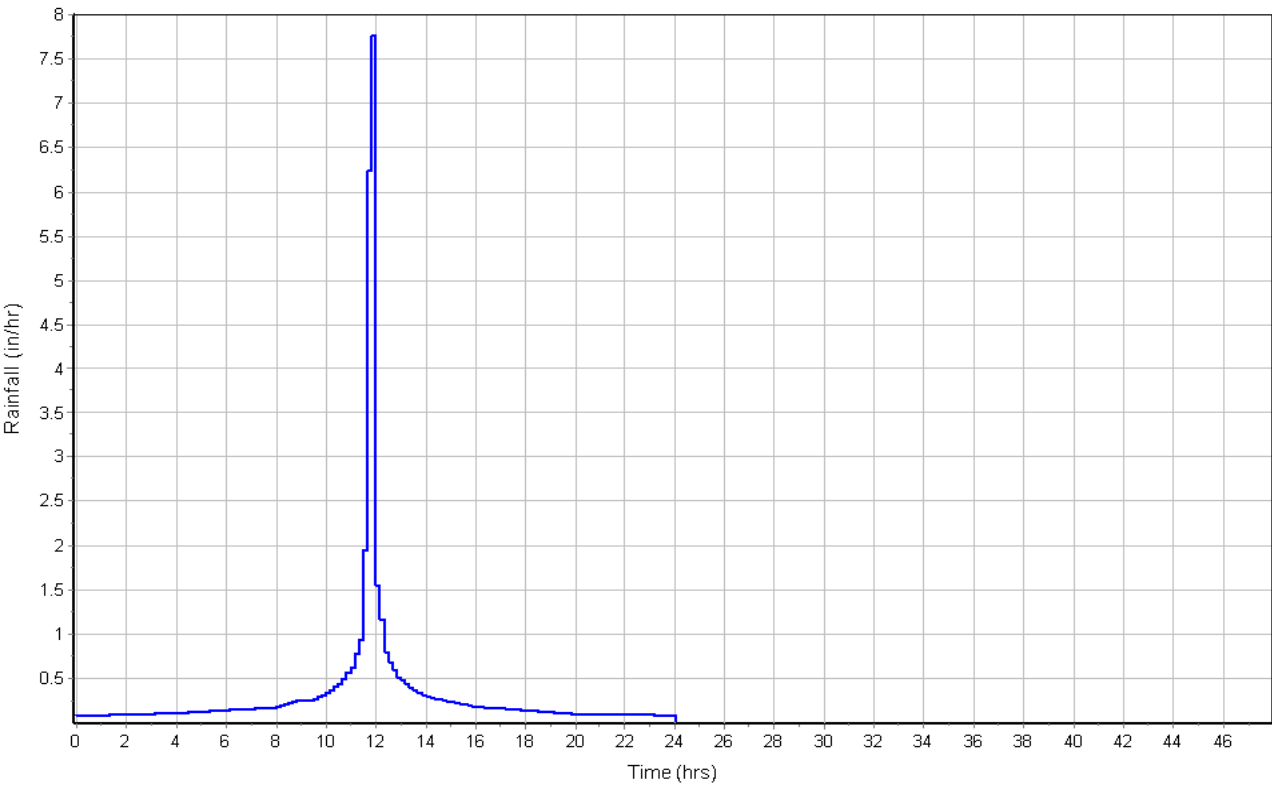
SN	Element ID	Element Type	Invert Elevation	Ground/Rim (Max) Elevation	Initial Water Elevation	Surcharge Elevation	Ponded Area	Peak Inflow	Max HGL Elevation Attained	Max Surcharge Depth Attained	Min Freeboard Attained	Time of Peak Flooding Occurrence	Total Flooded Volume	Total Time Flooded
			(ft)	(ft)	(ft)	(ft)	(ft²)	(cfs)	(ft)	(ft)	(ft)	(days hh:mm)	(ac-in)	(min)
1	Jun-01	Junction	816.00	822.00	0.00	822.00	10.00	49.45	821.52	0.00	0.48	0 00:00	0.00	0.00
2	OUT-2-1	Outfall	813.00					0.00	827.70					
3	OUT-2-2	Outfall	0.00					29.68	0.00					
4	OUT-A1-1	Outfall	0.00					4.56	0.00					
5	OUT-A1-2	Outfall	0.00					4.19	0.00					
6	DETENTION POND	Storage Node	817.00	822.00	817.00		81364.00	120.00	821.52				0.00	0.00

Link Summary

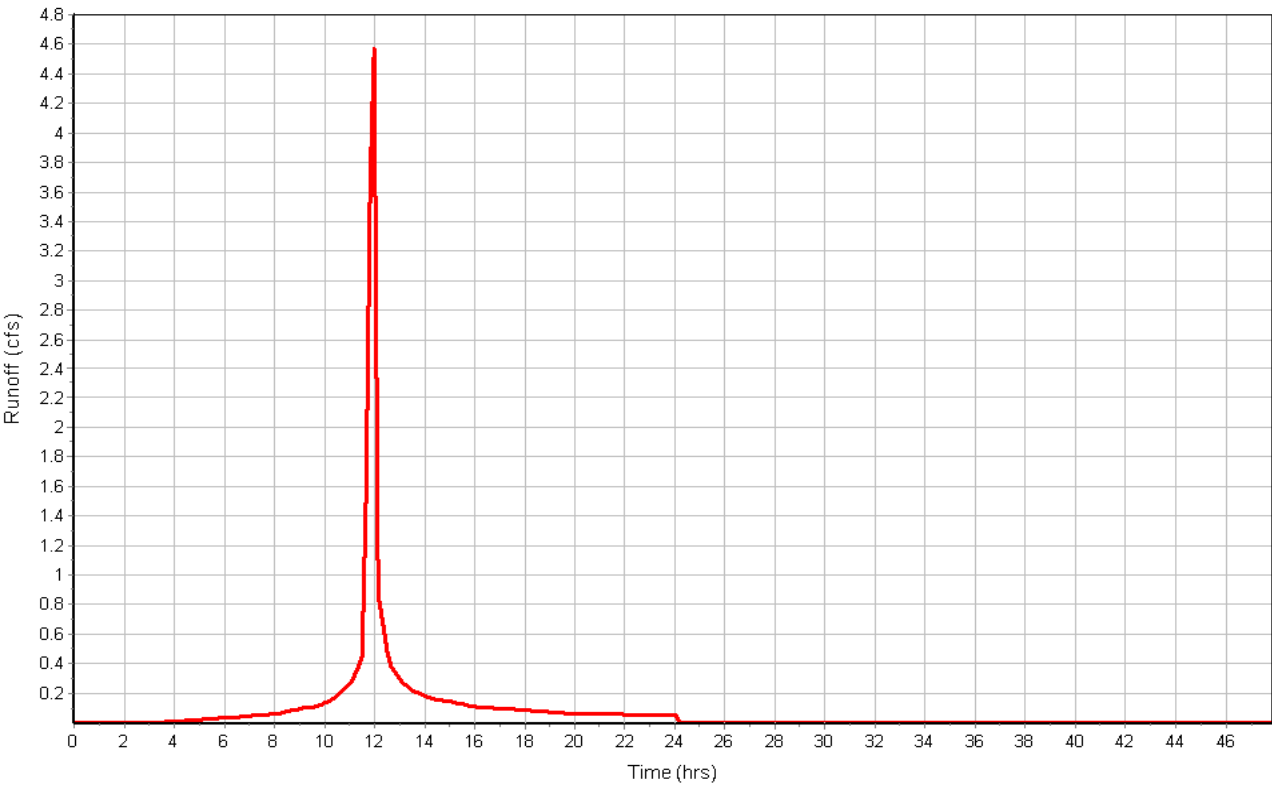
SN	Element ID	Element Type	From (Inlet) Node	To (Outlet) Node	Length	Inlet Invert Elevation	Outlet Invert Elevation	Average Slope	Diameter or Height	Manning's Roughness	Peak Flow	Design Flow Capacity	Peak Flow/ Design Flow Ratio	Peak Flow Velocity	Peak Flow Depth	Peak Flow Depth/ Total Depth Ratio	Total Time Reported Surcharged	Condition
					(ft)	(ft)	(ft)	(%)	(in)		(cfs)	(cfs)		(ft/sec)	(ft)		(min)	
1	EXISTING-48INCH	Pipe	Jun-01	OUT-2-1	225.00	816.00	813.70	1.0200	48.000	0.0130	0.00	145.23	0.00	0.00	4.00	1.00	2142.00	SURCHARGED
2	Link-02	Pipe	DETENTION POND	Jun-01	120.00	816.60	816.00	0.5000	24.000	0.0130	49.44	41.30	1.20	7.87	2.00	1.00	2151.00	SURCHARGED

Subbasin : A1-1

Rainfall Intensity Graph

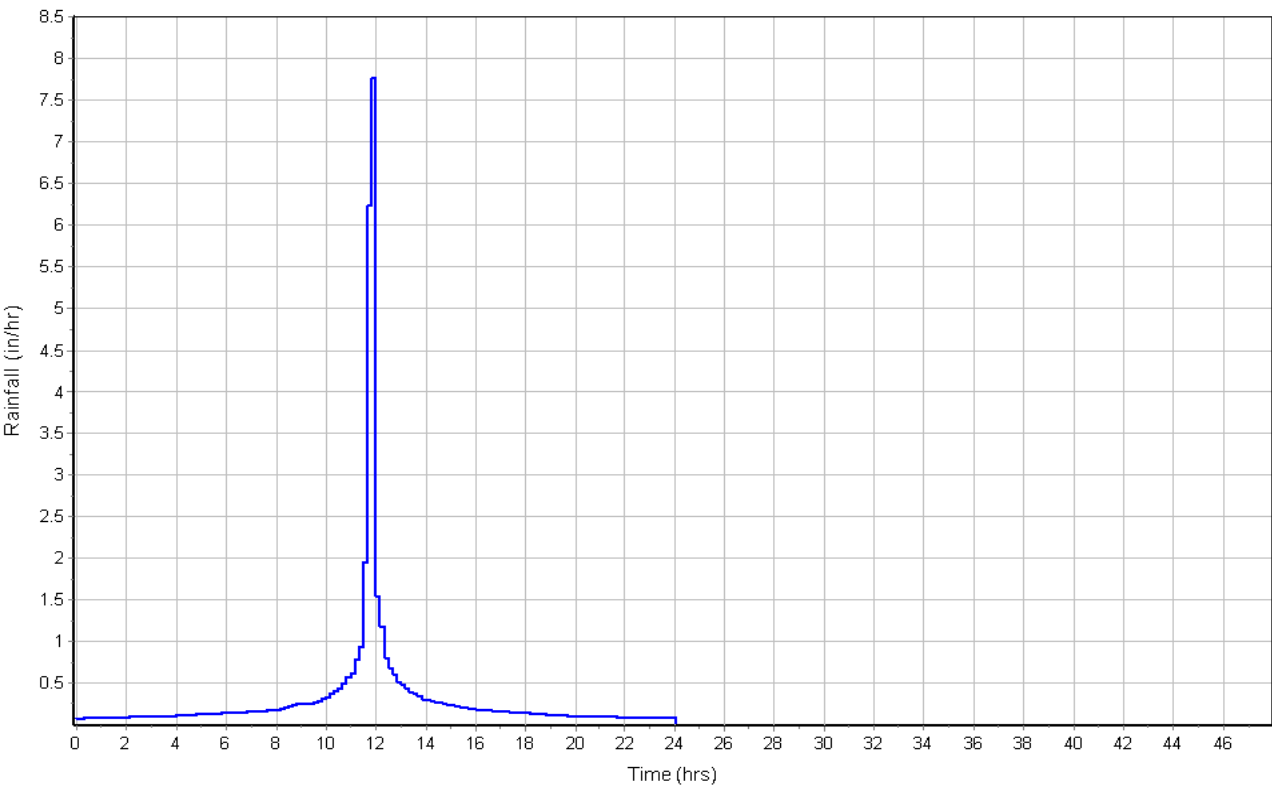


Runoff Hydrograph

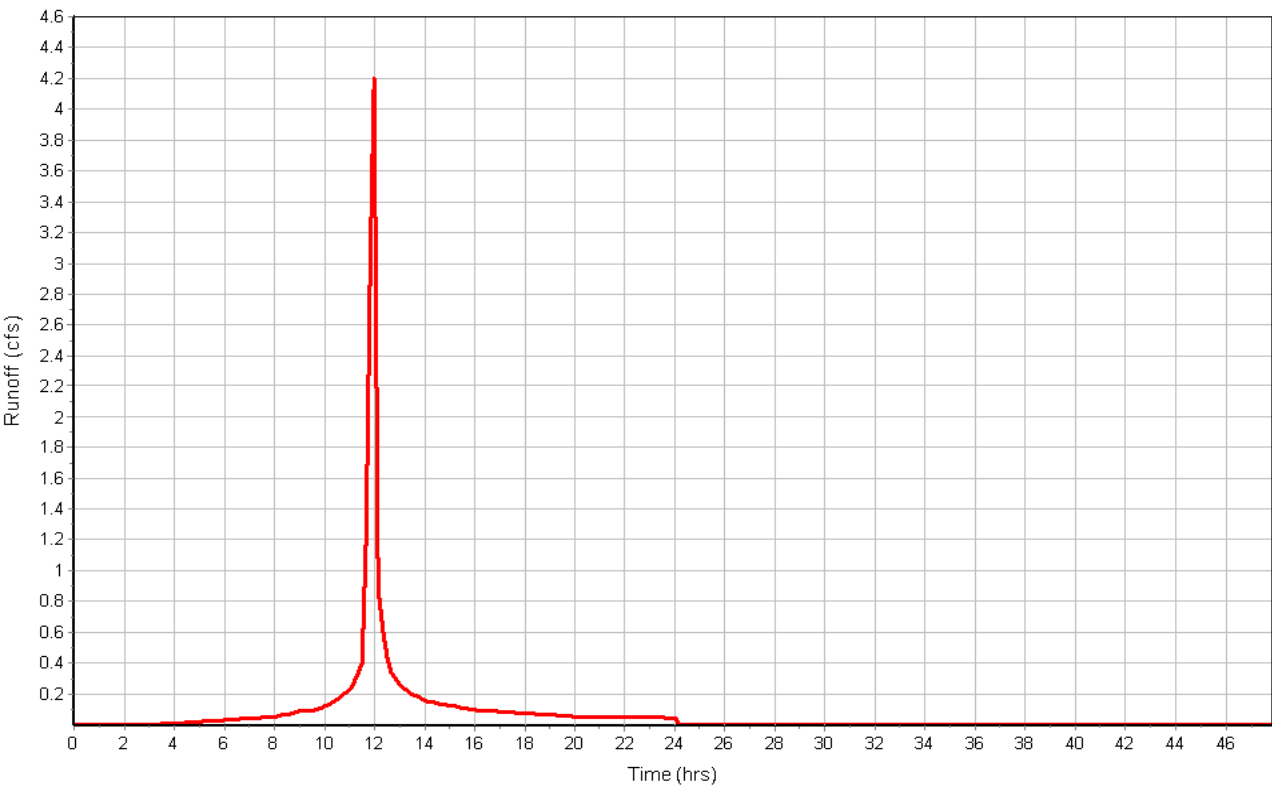


Subbasin : A1-2

Rainfall Intensity Graph

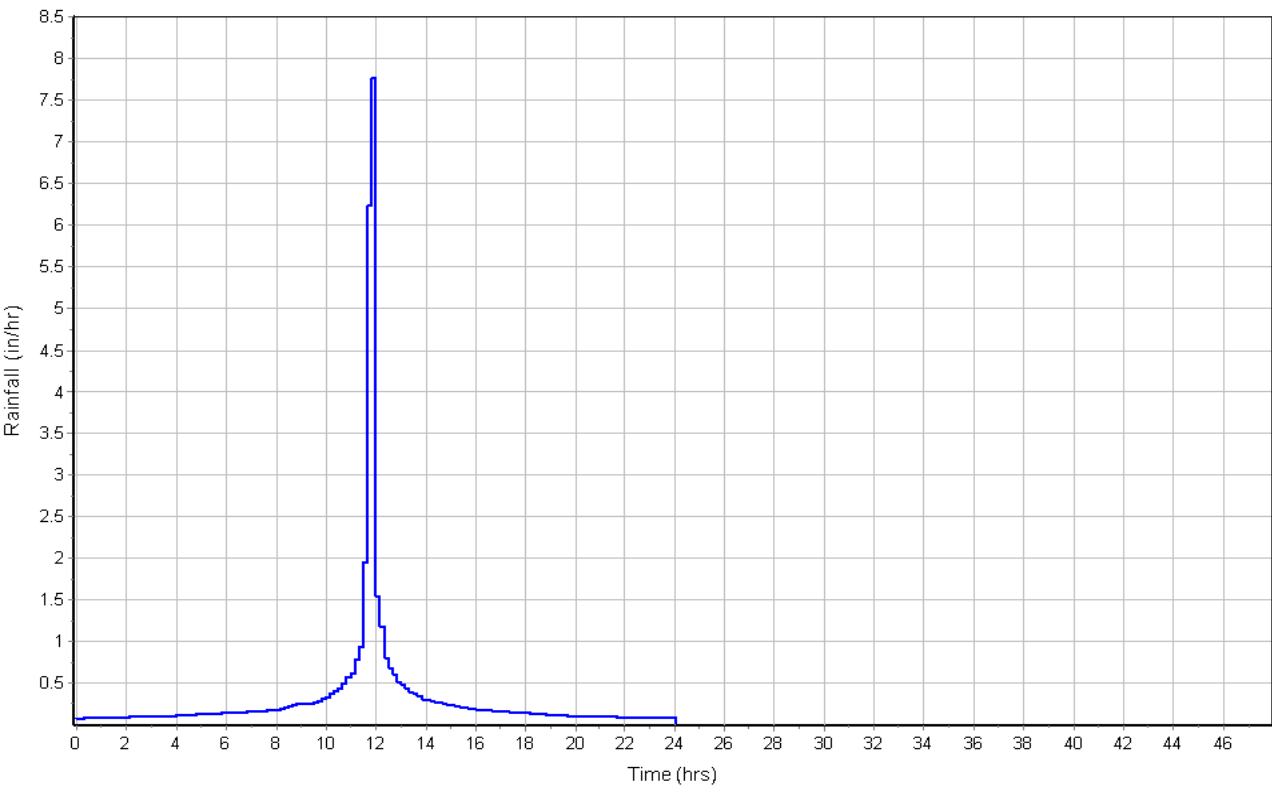


Runoff Hydrograph

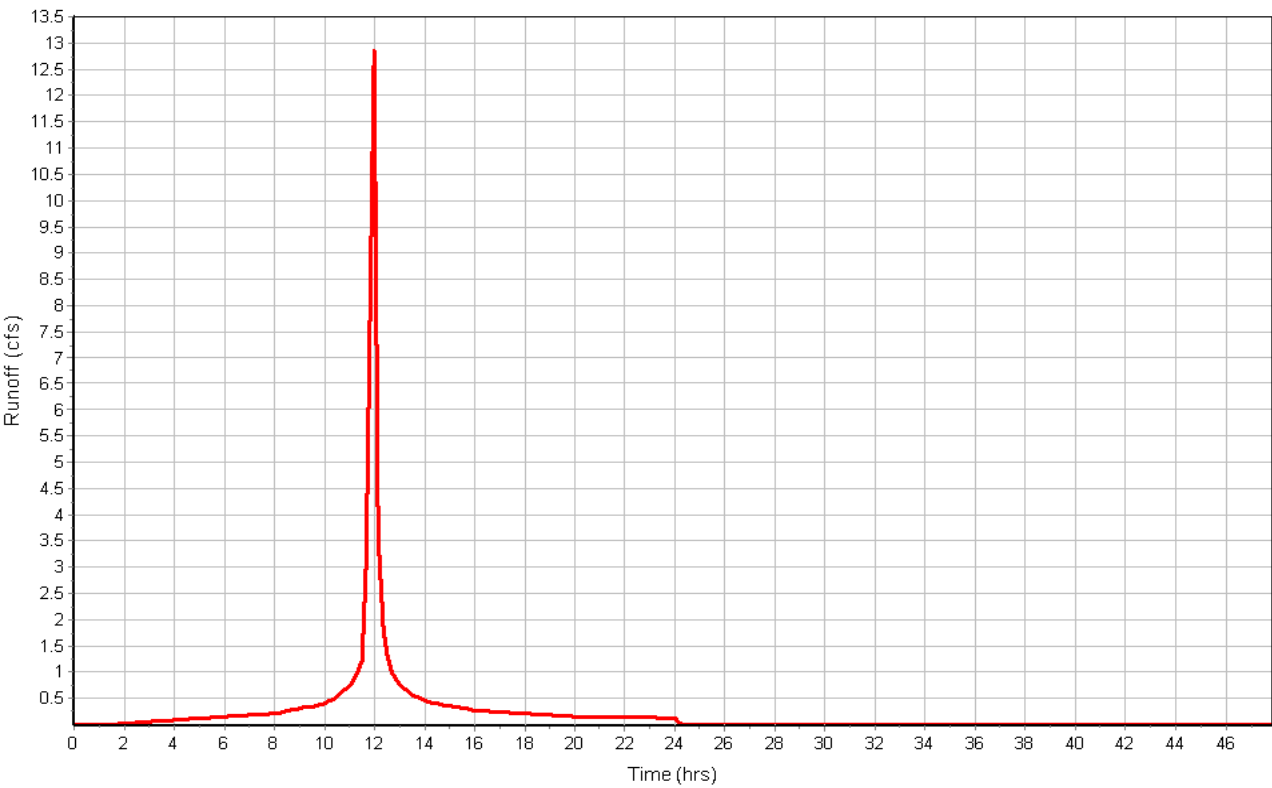


Subbasin : A1-3

Rainfall Intensity Graph

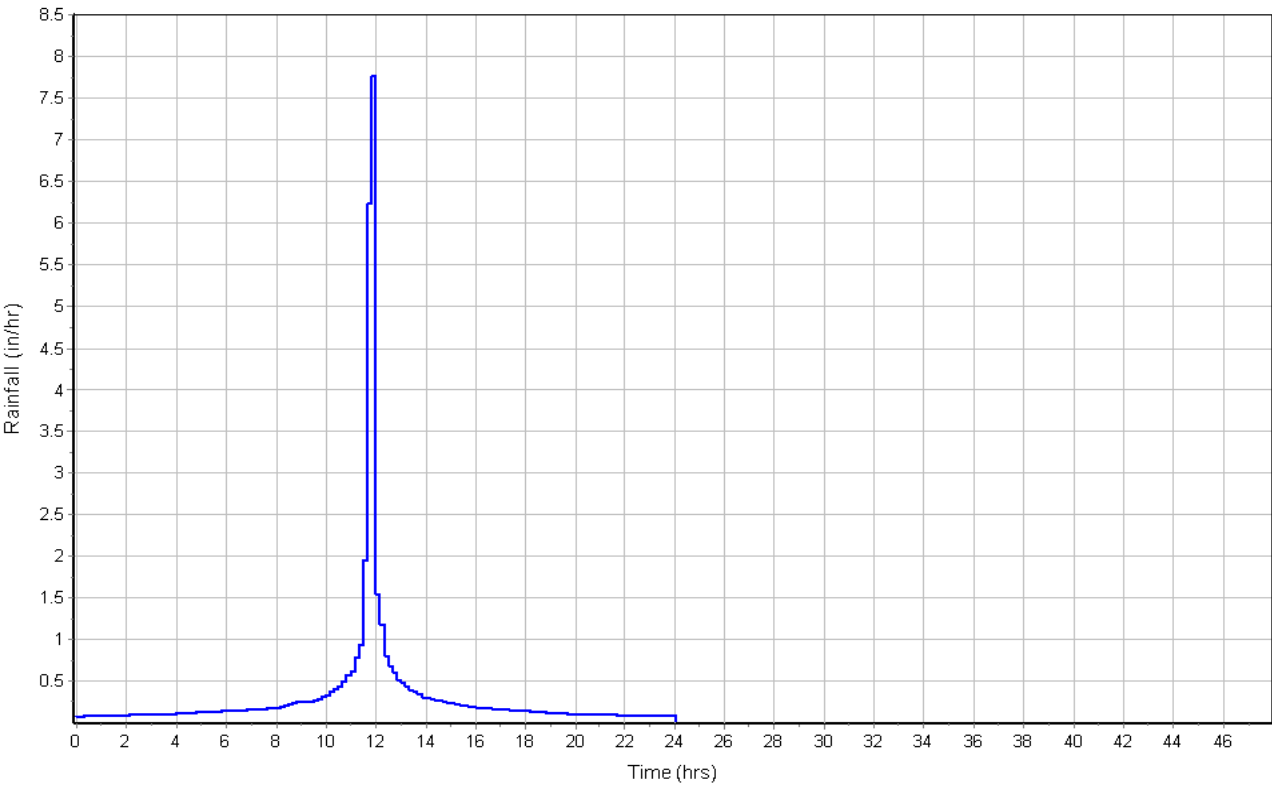


Runoff Hydrograph

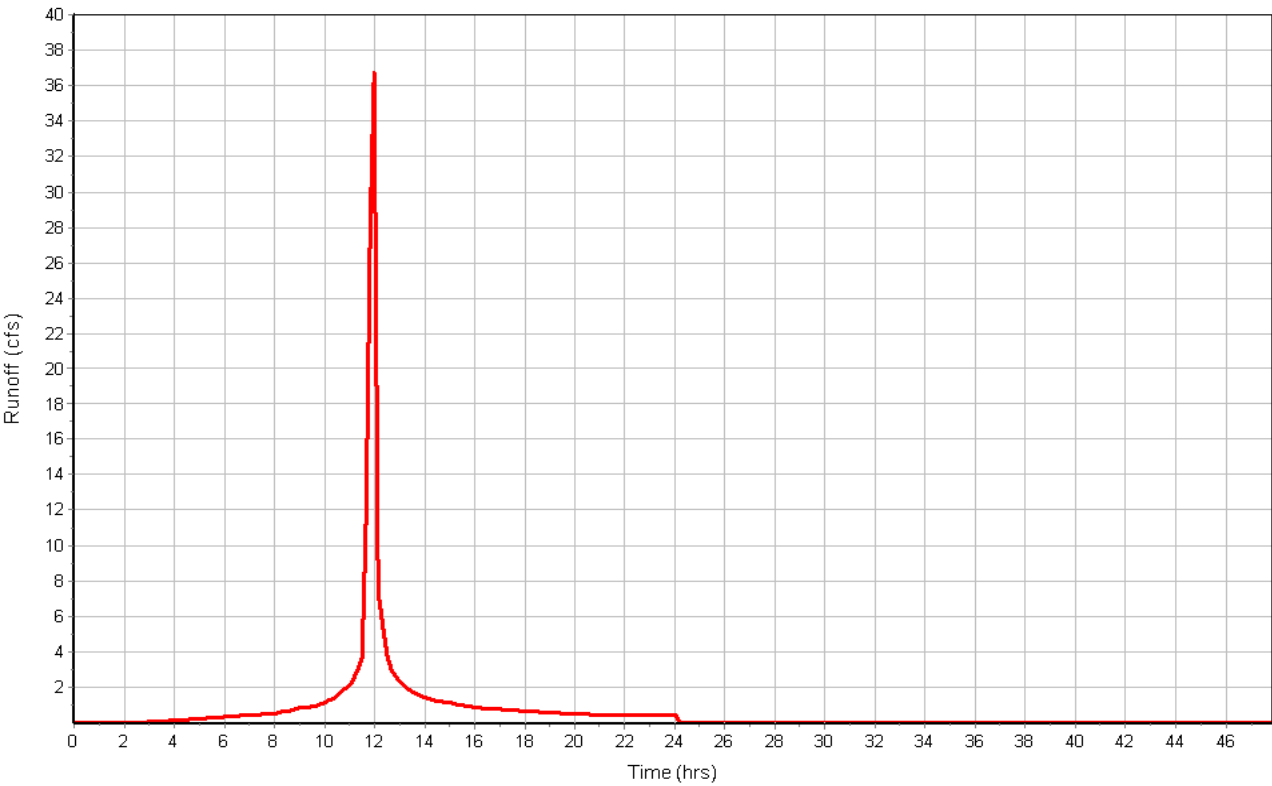


Subbasin : A2-1

Rainfall Intensity Graph

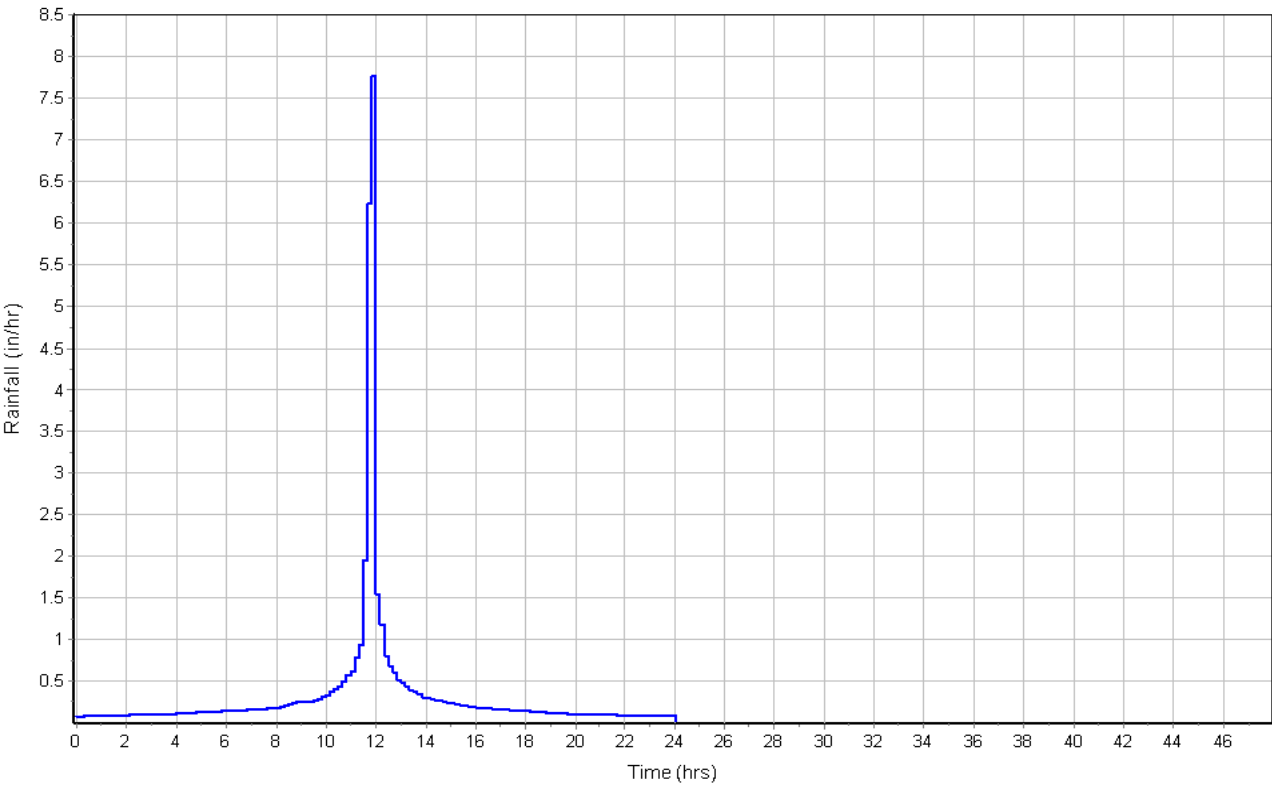


Runoff Hydrograph

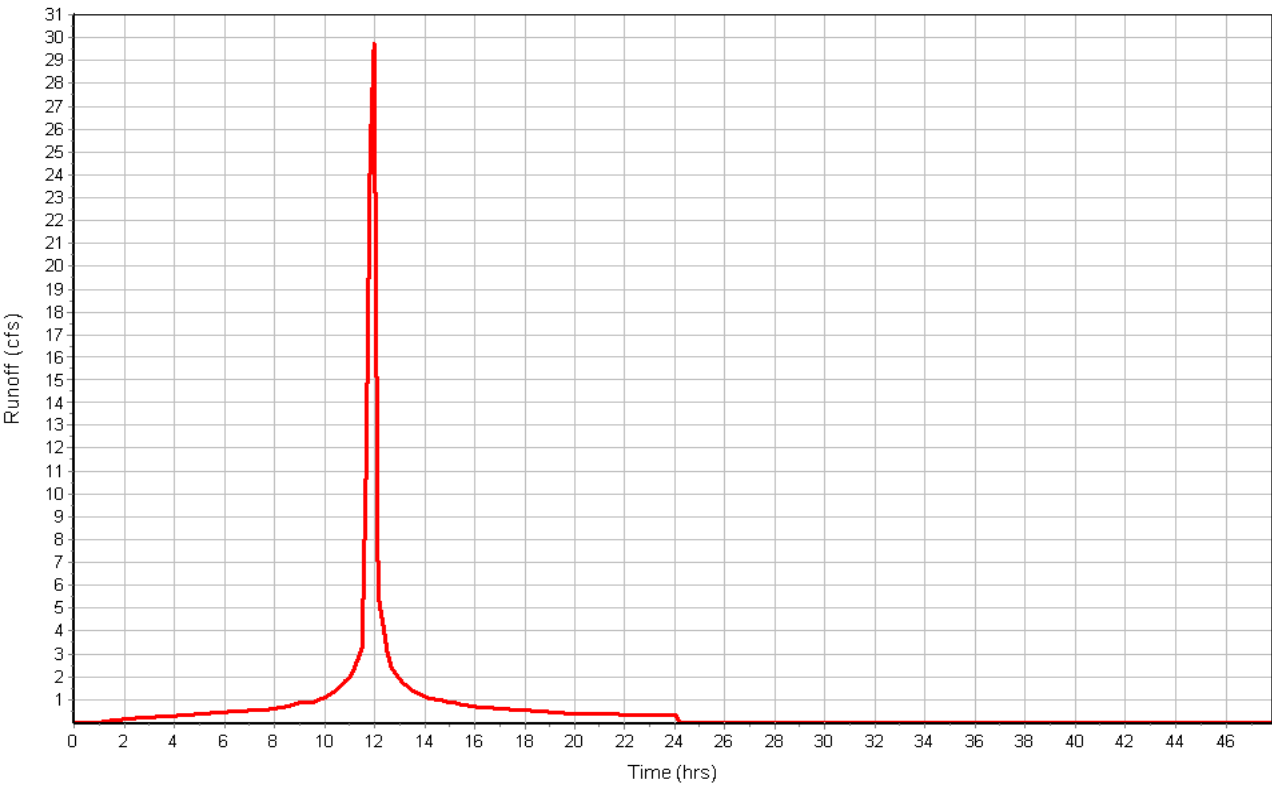


Subbasin : A2-2&B2

Rainfall Intensity Graph

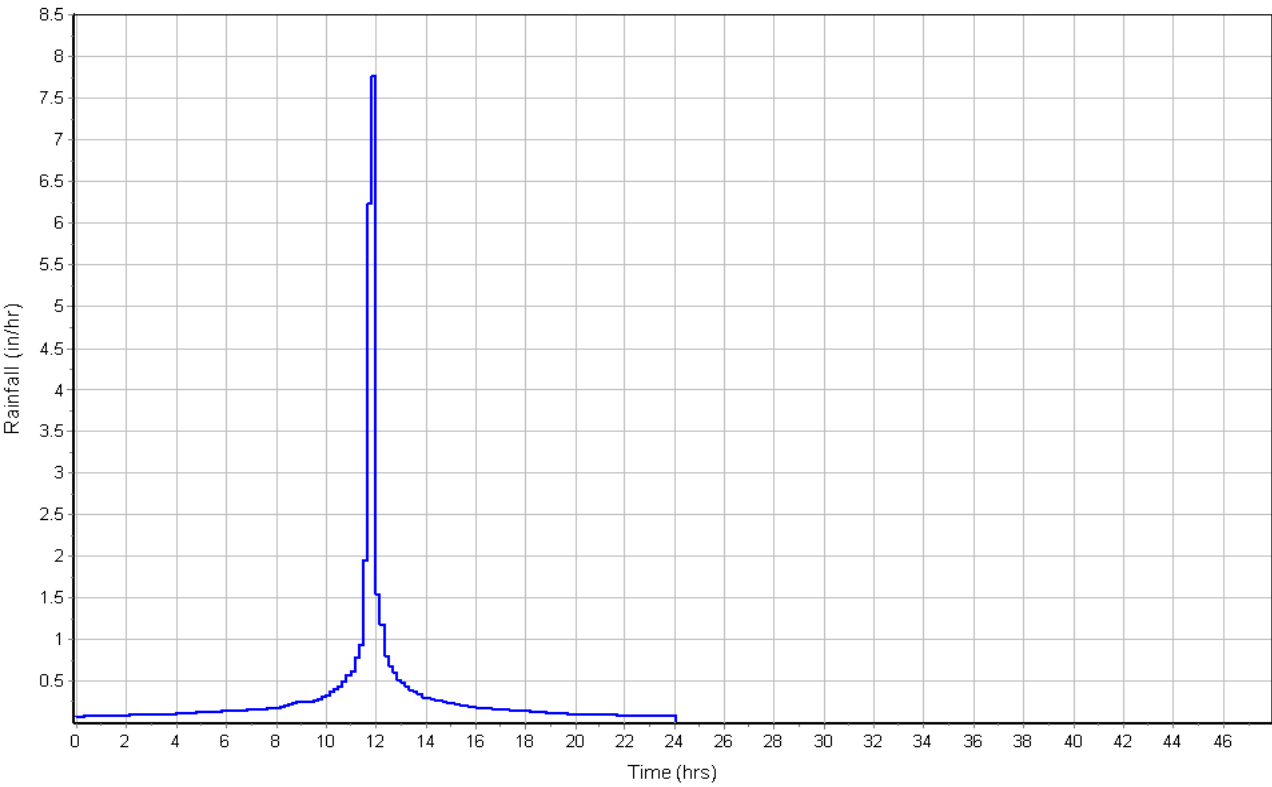


Runoff Hydrograph

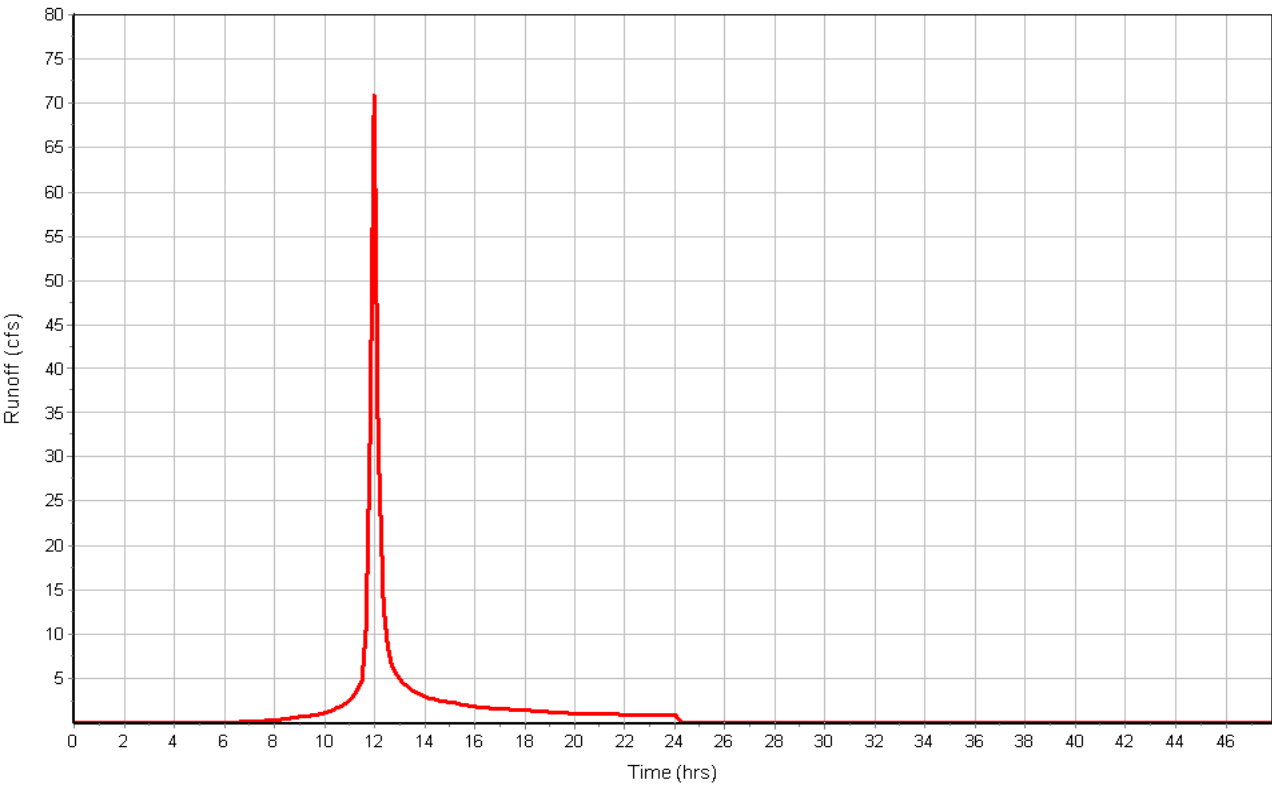


Subbasin : A3&B3

Rainfall Intensity Graph



Runoff Hydrograph



Junction Results

SN	Element ID	Peak Inflow	Peak Lateral Inflow	Max HGL Elevation Attained	Max HGL Depth Attained	Max Surge Depth Attained	Min Freeboard Attained	Average HGL Elevation Attained	Average HGL Depth Attained	Time of Max HGL Occurrence	Time of Peak Flooding Occurrence	Total Flooded Volume	Total Time Flooded
		(cfs)	(cfs)	(ft)	(ft)	(ft)	(ft)	(ft)	(ft)	(days hh:mm)	(days hh:mm)	(ac-in)	(min)
1	Jun-01	49.45	49.45	821.52	5.52	0.00	0.48	820.17	4.17	1 08:25	0 00:00	0.00	0.00

Pipe Results

SN Element ID	Peak Flow	Time of Peak Flow Occurrence	Design Flow Capacity	Peak Flow/ Design Flow Ratio	Peak Flow Velocity	Travel Time	Peak Flow Depth	Peak Flow Depth/ Total Depth Ratio	Total Time Surcharged	Froude Number	Reported Condition
	(cfs)	(days hh:mm)	(cfs)		(ft/sec)	(min)	(ft)		(min)		
1 EXISTING-48INCH	0.00	0 00:00	145.23	0.00	0.00		4.00	1.00	2142.00		SURCHARGED
2 Link-02	49.44	0 12:09	41.30	1.20	7.87	0.25	2.00	1.00	2151.00		SURCHARGED

Storage Nodes

Storage Node : DETENTION POND

Input Data

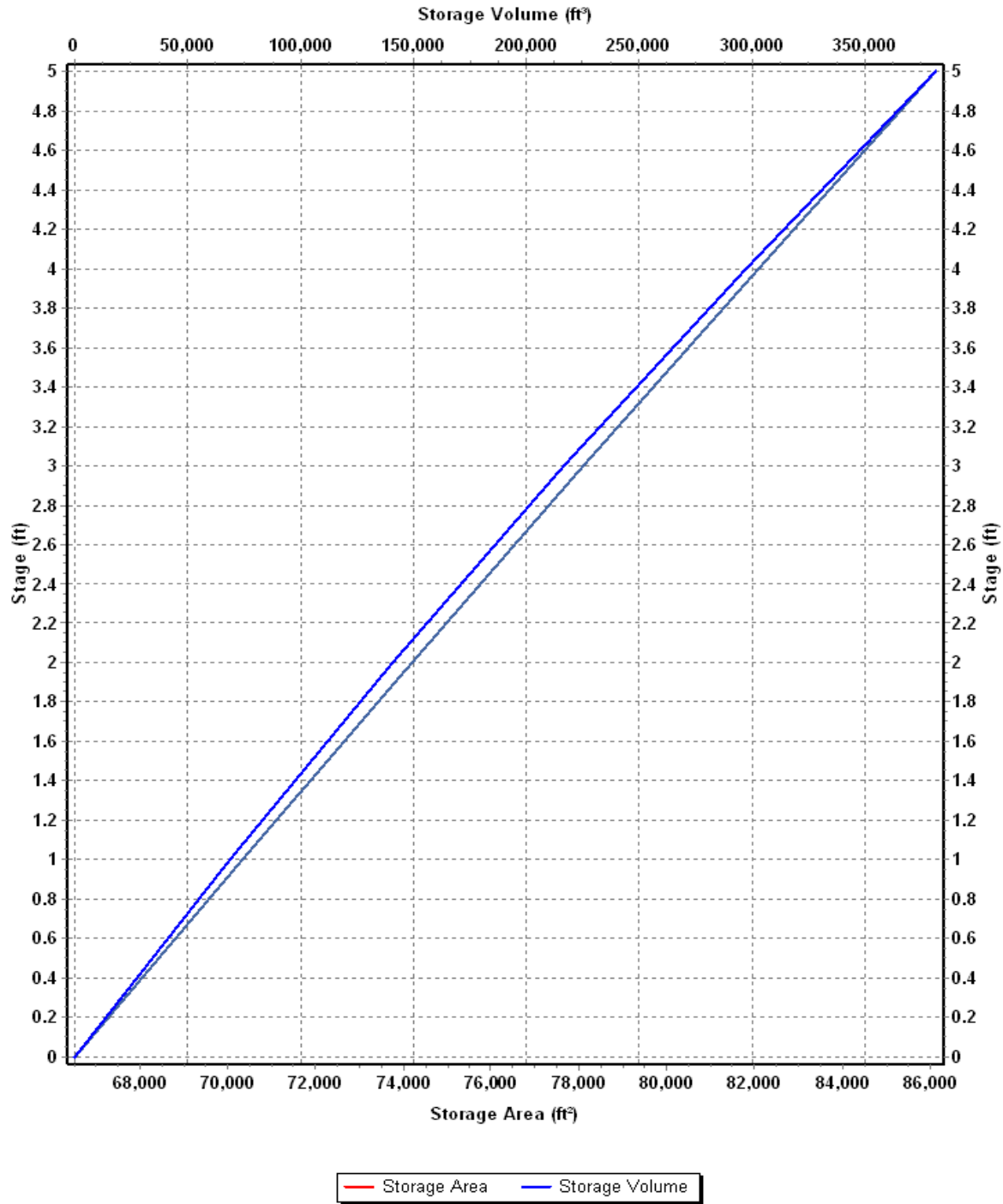
Invert Elevation (ft)	817.00
Max (Rim) Elevation (ft)	822.00
Max (Rim) Offset (ft)	5.00
Initial Water Elevation (ft)	817.00
Initial Water Depth (ft)	0.00
Ponded Area (ft²)	81364.00
Evaporation Loss	0.00

Storage Area Volume Curves

Storage Curve : DETENTION POND

Stage	Storage Area	Storage Volume
(ft)	(ft²)	(ft³)
0	66526	0.000
1	70333	68429.50
2	74197	140694.50
3	78117	216851.50
4	82093	296956.50
5	86126	381066.00

Storage Area Volume Curves

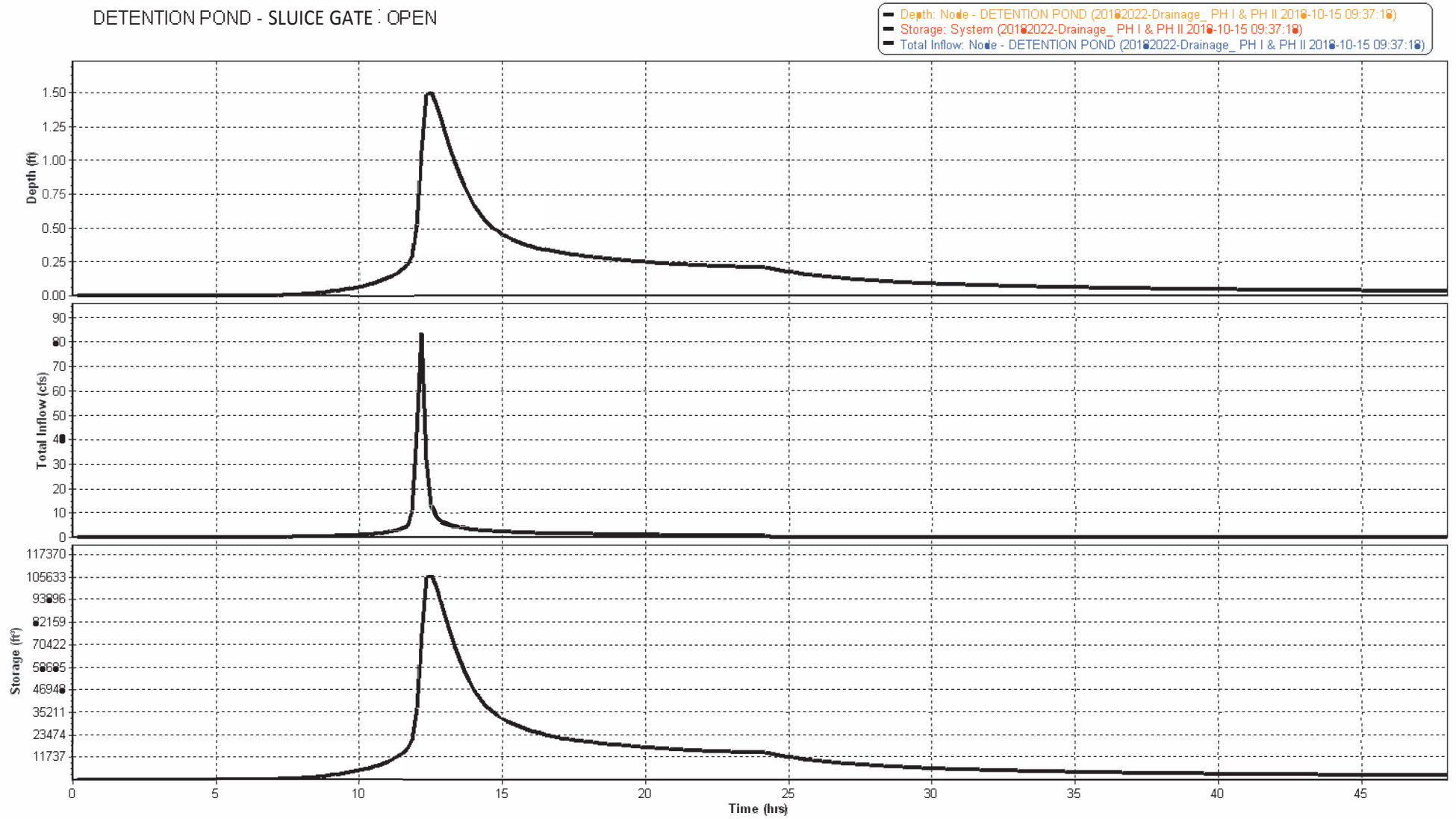


Storage Node : DETENTION POND (continued)

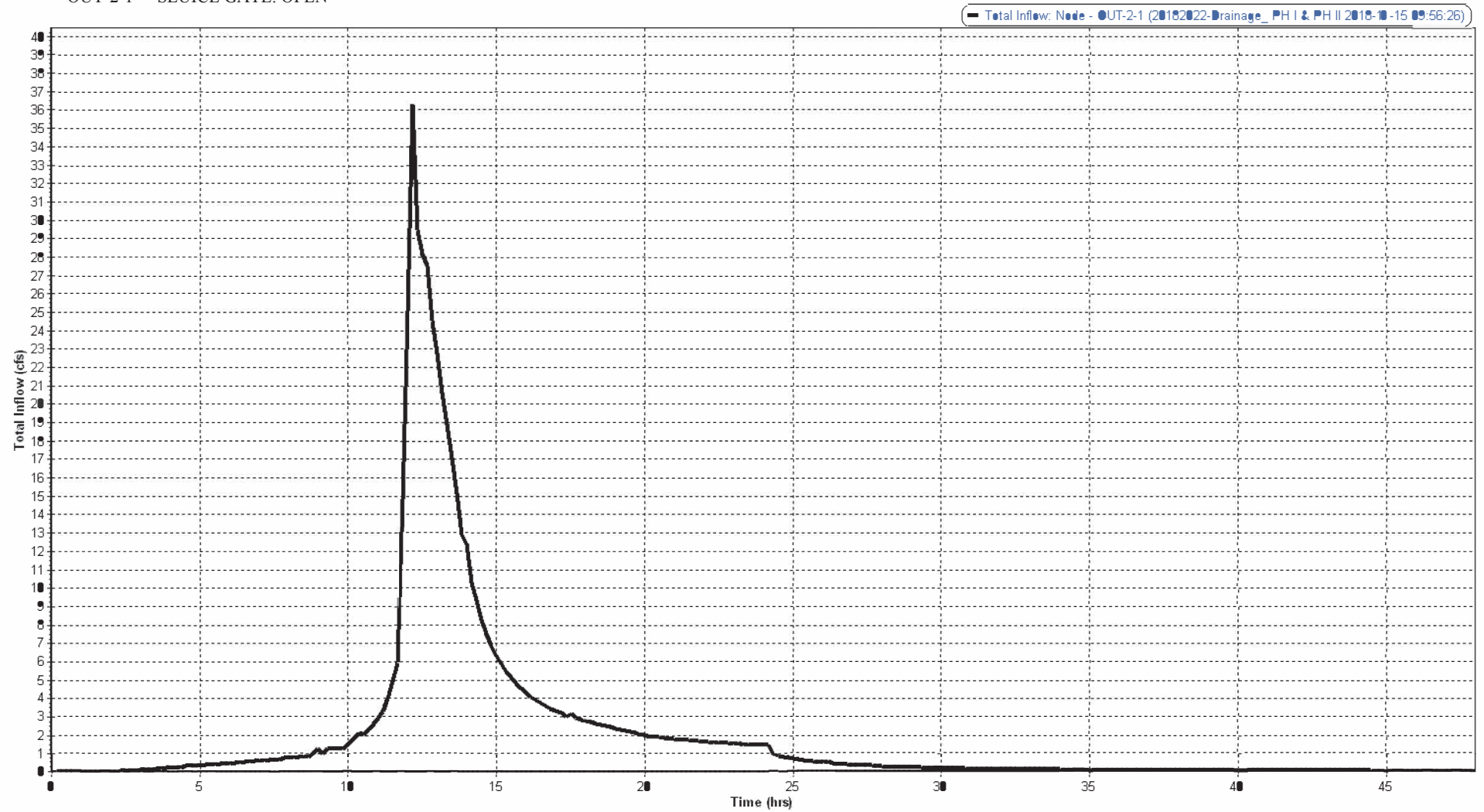
Output Summary Results

Peak Inflow (cfs)	120.00
Peak Lateral Inflow (cfs)	70.56
Peak Outflow (cfs)	0.00
Peak Exfiltration Flow Rate (cfm)	0.00
Max HGL Elevation Attained (ft)	821.52
Max HGL Depth Attained (ft)	4.52
Average HGL Elevation Attained (ft)	820.15
Average HGL Depth Attained (ft)	3.15
Time of Max HGL Occurrence (days hh:mm)	1 07:11
Total Exfiltration Volume (1000-ft³)	0.000
Total Flooded Volume (ac-in)	0
Total Time Flooded (min)	0
Total Retention Time (sec)	0.00

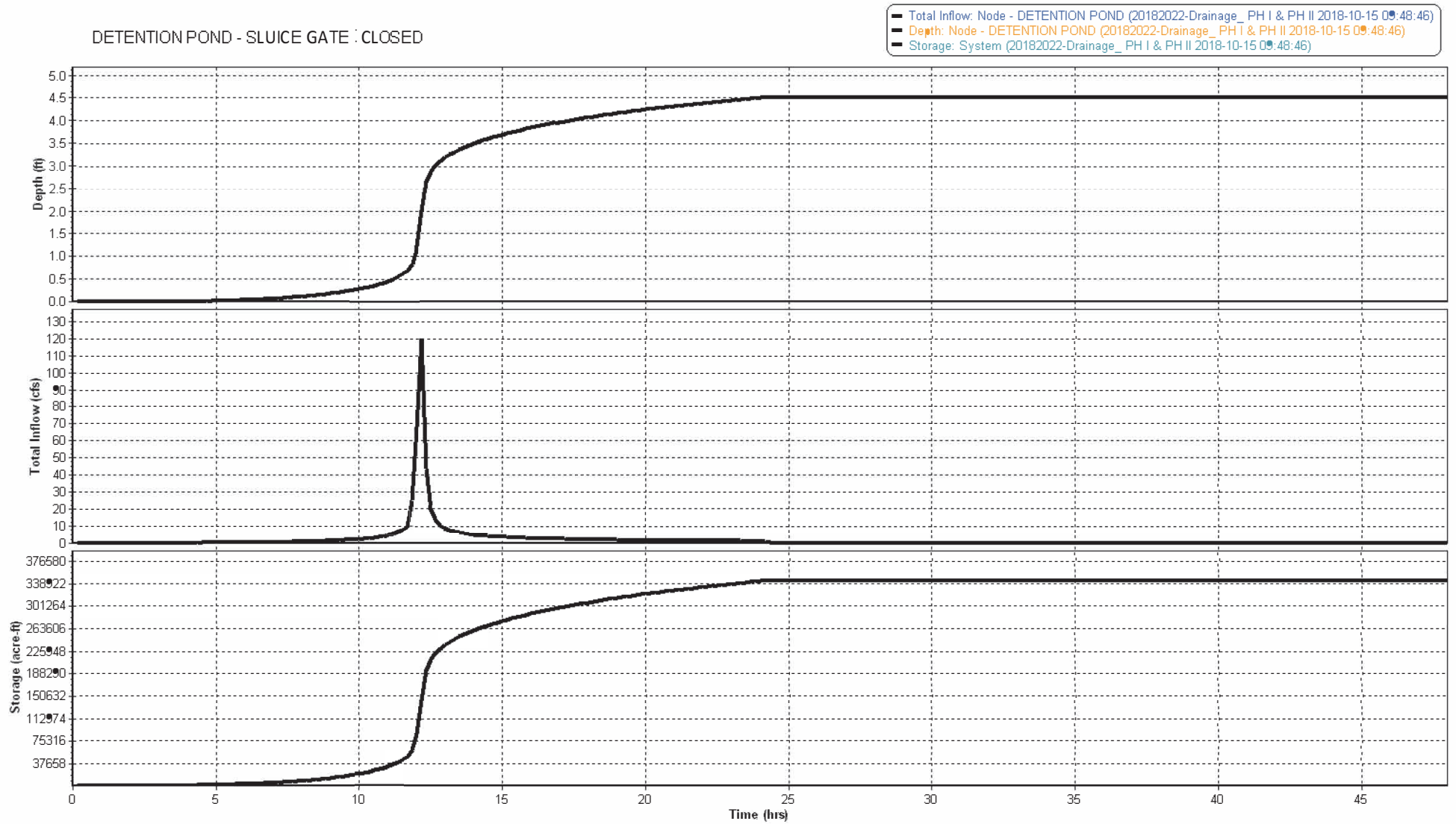
DETENTION POND - SLUICE GATE : OPEN



OUT-2-1 - SLUICE GATE: OPEN



DETENTION POND - SLUICE GATE : CLOSED



APPENDIX C
100-YEAR STORM EVENT
POST CONDITION
FULL DEVELOPMENT (PHASE I,II,III)

Project Description

File Name 20182022-Drainage_ PH I&II& III.SPF

Project Options

Flow Units CFS
Elevation Type Elevation
Hydrology Method SCS TR-55
Time of Concentration (TOC) Method Kirpich
Link Routing Method Hydrodynamic
Enable Overflow Ponding at Nodes YES
Skip Steady State Analysis Time Periods .. NO

Analysis Options

Start Analysis On Oct 05, 2018 00:00:00
End Analysis On Oct 07, 2018 00:00:00
Start Reporting On Oct 05, 2018 00:00:00
Antecedent Dry Days 0 days
Runoff (Dry Weather) Time Step 0 01:00:00 days hh:mm:ss
Runoff (Wet Weather) Time Step 0 00:05:00 days hh:mm:ss
Reporting Time Step 0 00:10:00 days hh:mm:ss
Routing Time Step 30 seconds

Number of Elements

Qty
Rain Gages 1
Subbasins..... 6
Nodes..... 6
 Junctions 1
 Outfalls 4
 Flow Diversions 0
 Inlets 0
 Storage Nodes 1
Links..... 2
 Channels 0
 Pipes 2
 Pumps 0
 Orifices 0
 Weirs 0
 Outlets 0
Pollutants 0
Land Uses 0

Rainfall Details

SN	Rain Gage ID	Data Source	Data Source ID	Rainfall Type	Rain Units	State	County	Return Period (years)	Rainfall Depth (inches)	Rainfall Distribution
1		Time Series	TS-100	Year Intensity	inches	Kansas	Douglas	100	8.16	SCS Type II 24-hr

Subbasin Summary

SN	Subbasin ID	Area	Weighted Curve Number	Average Slope	Flow Length	Total Rainfall	Total Runoff	Total Runoff Volume	Peak Runoff	Time of Concentration
		(ac)		(%)	(ft)	(in)	(in)	(ac-in)	(cfs)	(days hh:mm:ss)
1	A1-1	0.58	88.10	0.5000	500.00	8.16	6.74	3.91	5.82	0 00:05:06
2	A1-2	0.51	88.70	0.5000	500.00	8.16	6.81	3.47	5.00	0 00:06:12
3	A1-3	1.43	94.80	0.5000	500.00	8.16	7.54	10.78	13.77	0 00:08:30
4	A2-1	4.49	90.40	0.5000	500.00	8.16	7.01	31.48	45.13	0 00:05:48
5	A2-2&B2	3.61	96.70	0.5000	500.00	8.16	7.76	28.03	38.60	0 00:05:00
6	A3&B3	10.21	88.00	0.5000	500.00	8.16	6.73	68.66	94.18	0 00:08:00

Node Summary

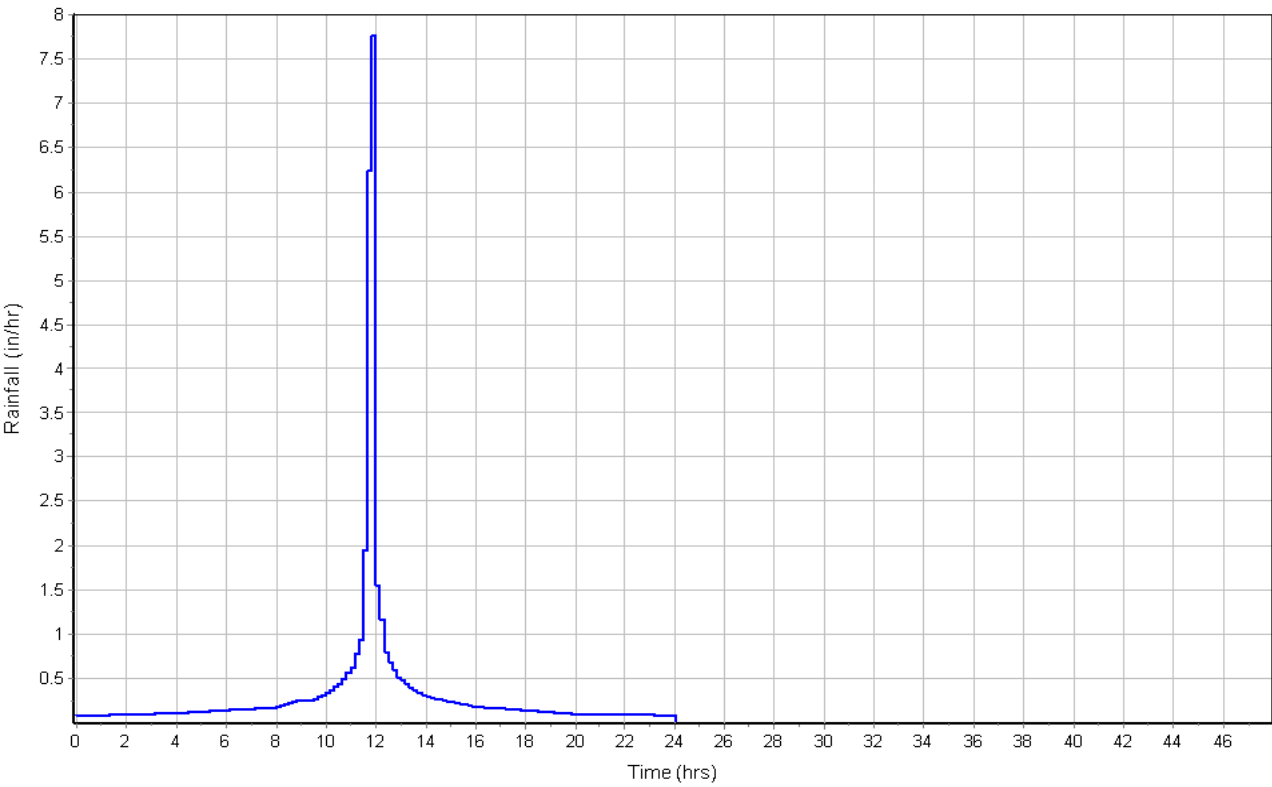
SN	Element ID	Element Type	Invert Elevation	Ground/Rim (Max) Elevation	Initial Water Elevation	Surcharge Elevation	Ponded Area	Peak Inflow	Max HGL Elevation Attained	Max Surcharge Depth Attained	Min Freeboard Attained	Time of Peak Flooding Occurrence	Total Flooded Volume	Total Time Flooded
			(ft)	(ft)	(ft)	(ft)	(ft²)	(cfs)	(ft)	(ft)	(ft)	(days hh:mm)	(ac-in)	(min)
1	Jun-01	Junction	816.00	822.00	0.00	822.00	20.00	49.12	821.89	0.00	0.11	0 00:00	0.00	0.00
2	Out-2-1	Outfall	813.00					0.00	827.70					
3	Out-2-2	Outfall	0.00					29.56	0.00					
4	Out-A1-1	Outfall	0.00					4.54	0.00					
5	Out-A1-2	Outfall	0.00					4.16	0.00					
6	DETENTION POND	Storage Node	817.00	822.00	817.00		93568.00	134.08	821.88				0.00	0.00

Link Summary

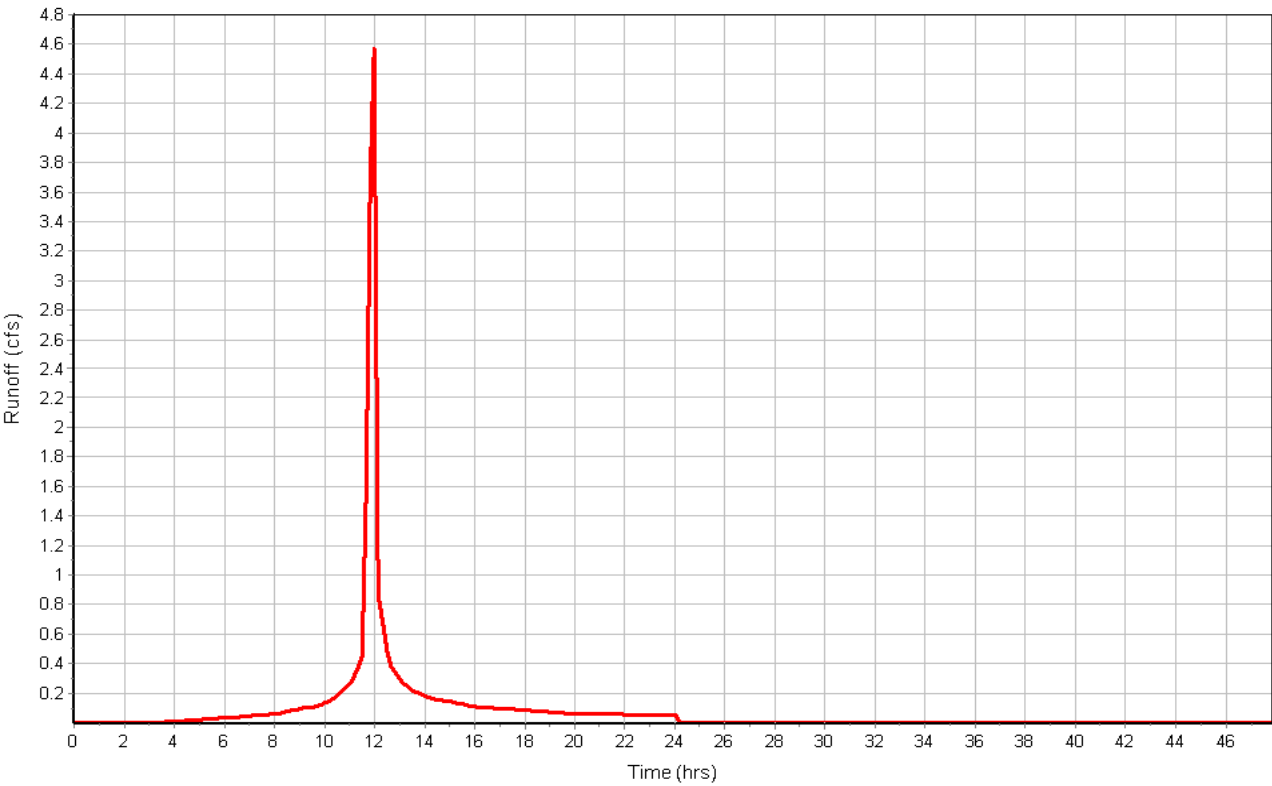
SN	Element ID	Element Type	From (Inlet) Node	To (Outlet) Node	Length	Inlet Invert Elevation	Outlet Invert Elevation	Average Slope	Diameter or Height	Manning's Roughness	Peak Flow	Design Flow Capacity	Peak Flow/ Design Flow Ratio	Peak Flow Velocity	Peak Flow Depth	Peak Flow Depth/ Total Depth Ratio	Total Time Reported Surcharged	Condition
					(ft)	(ft)	(ft)	(%)	(in)		(cfs)	(cfs)		(ft/sec)	(ft)		(min)	
1	EXISTIN-48INCH	Pipe	Jun-01	Out-2-1	225.00	816.00	813.70	1.0200	48.000	0.0130	0.00	145.23	0.00	0.00	4.00	1.00	2160.00	SURCHARGED
2	Link-02	Pipe	DETENTION POND	Jun-01	120.00	816.60	816.00	0.5000	24.000	0.0130	48.69	41.30	1.18	7.75	2.00	1.00	2156.00	SURCHARGED

Subbasin : A1-1

Rainfall Intensity Graph

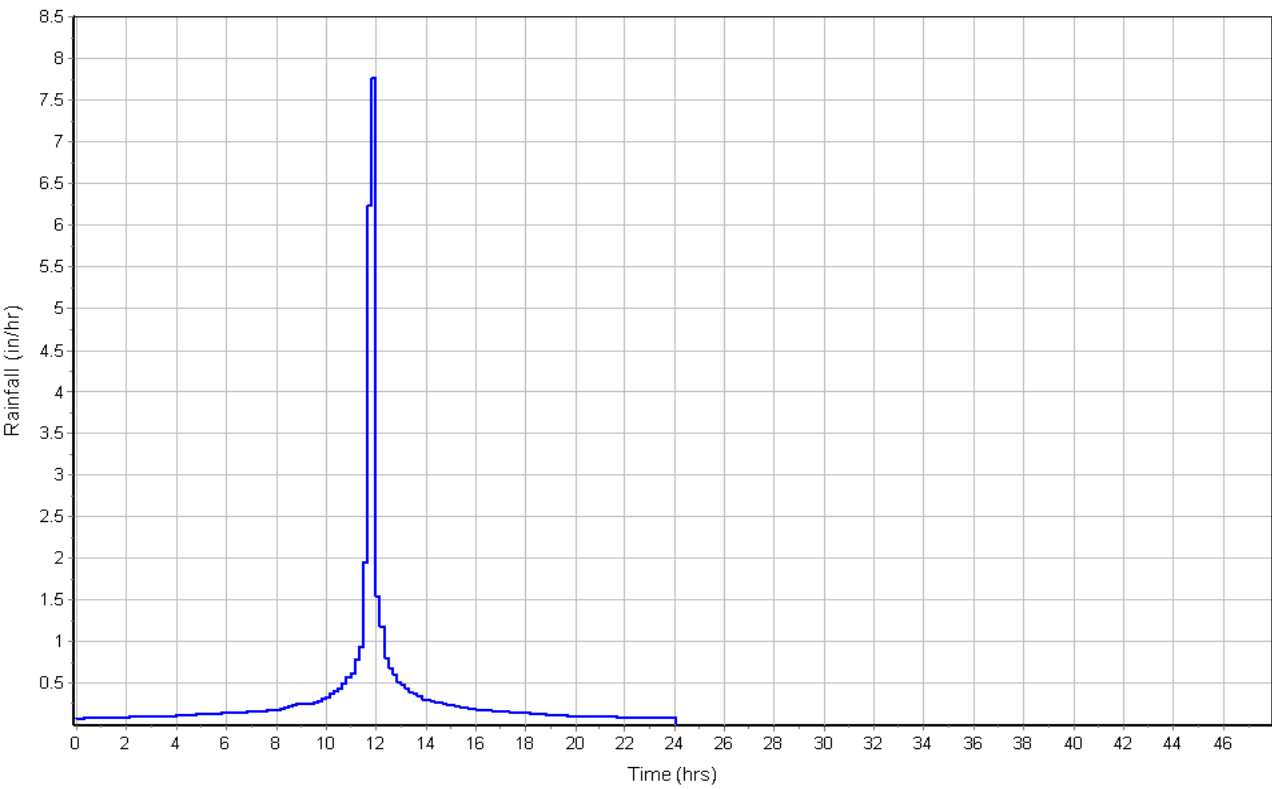


Runoff Hydrograph

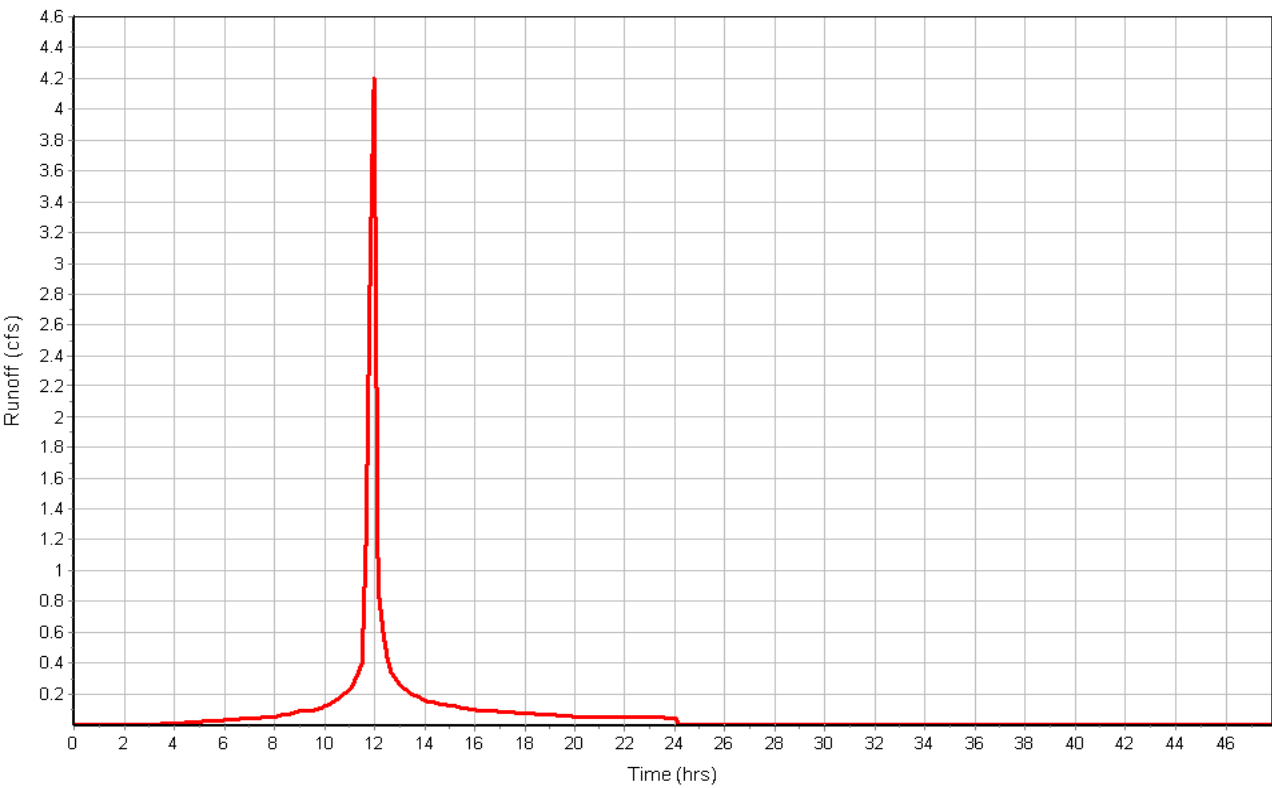


Subbasin : A1-2

Rainfall Intensity Graph

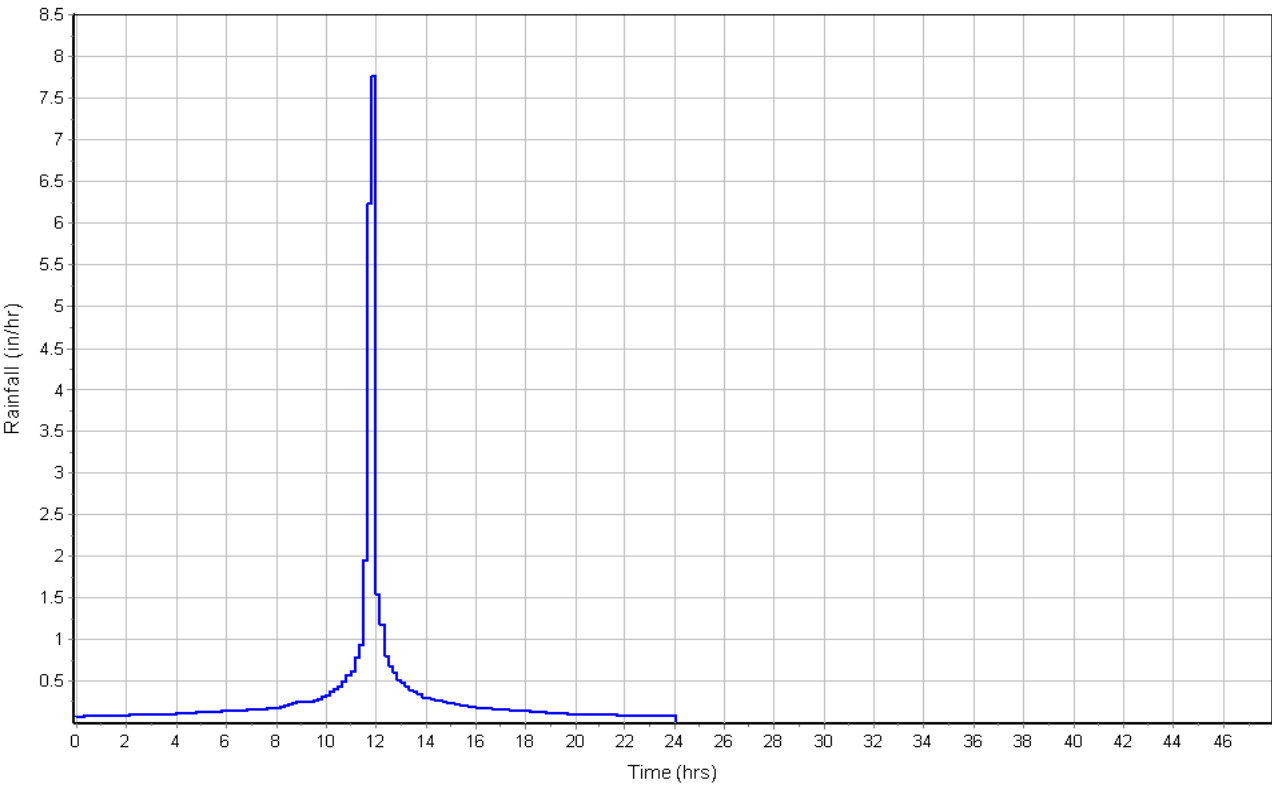


Runoff Hydrograph

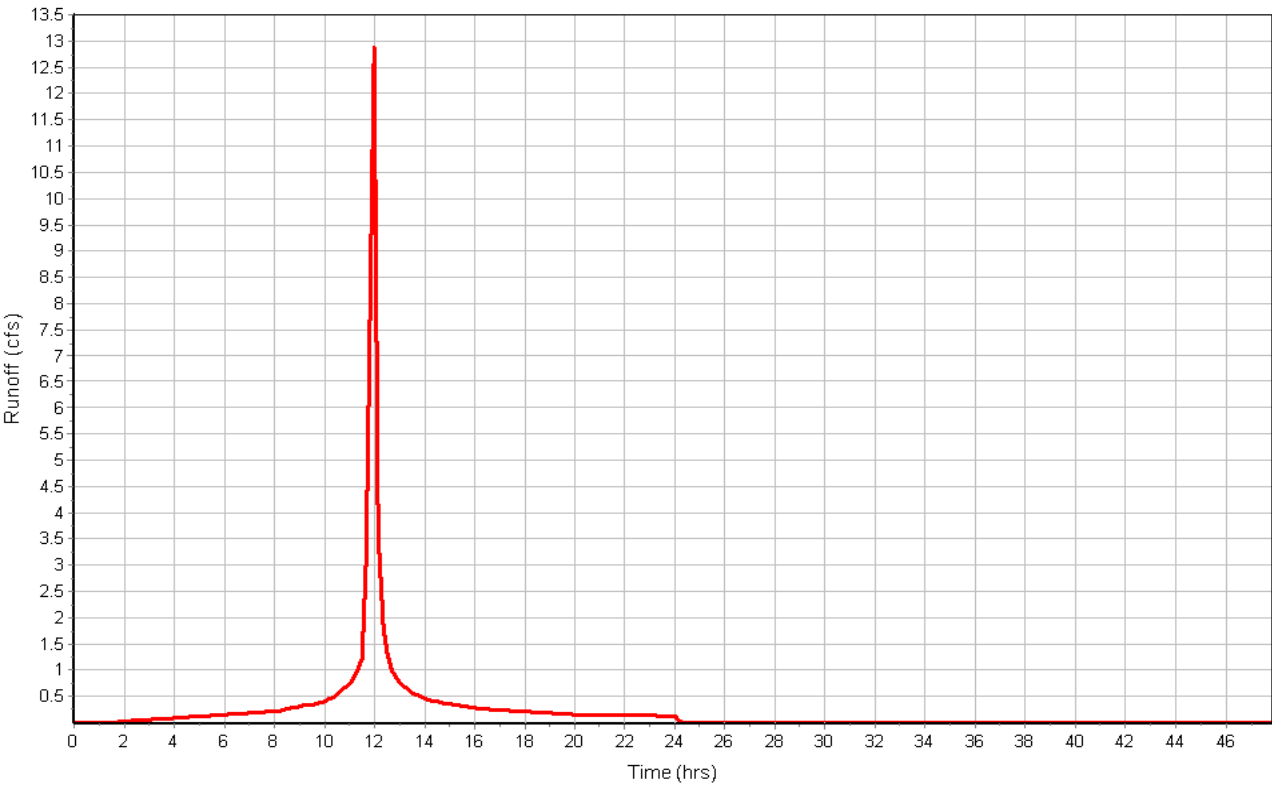


Subbasin : A1-3

Rainfall Intensity Graph

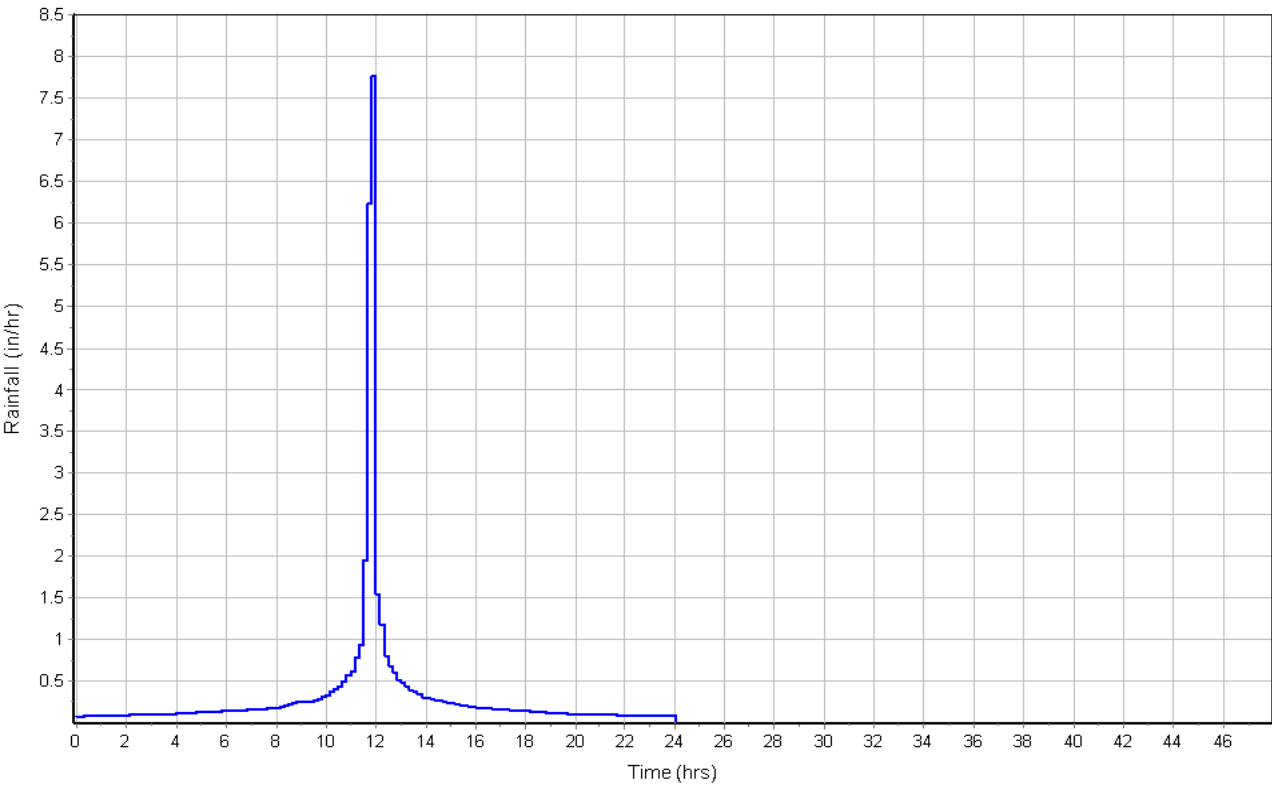


Runoff Hydrograph

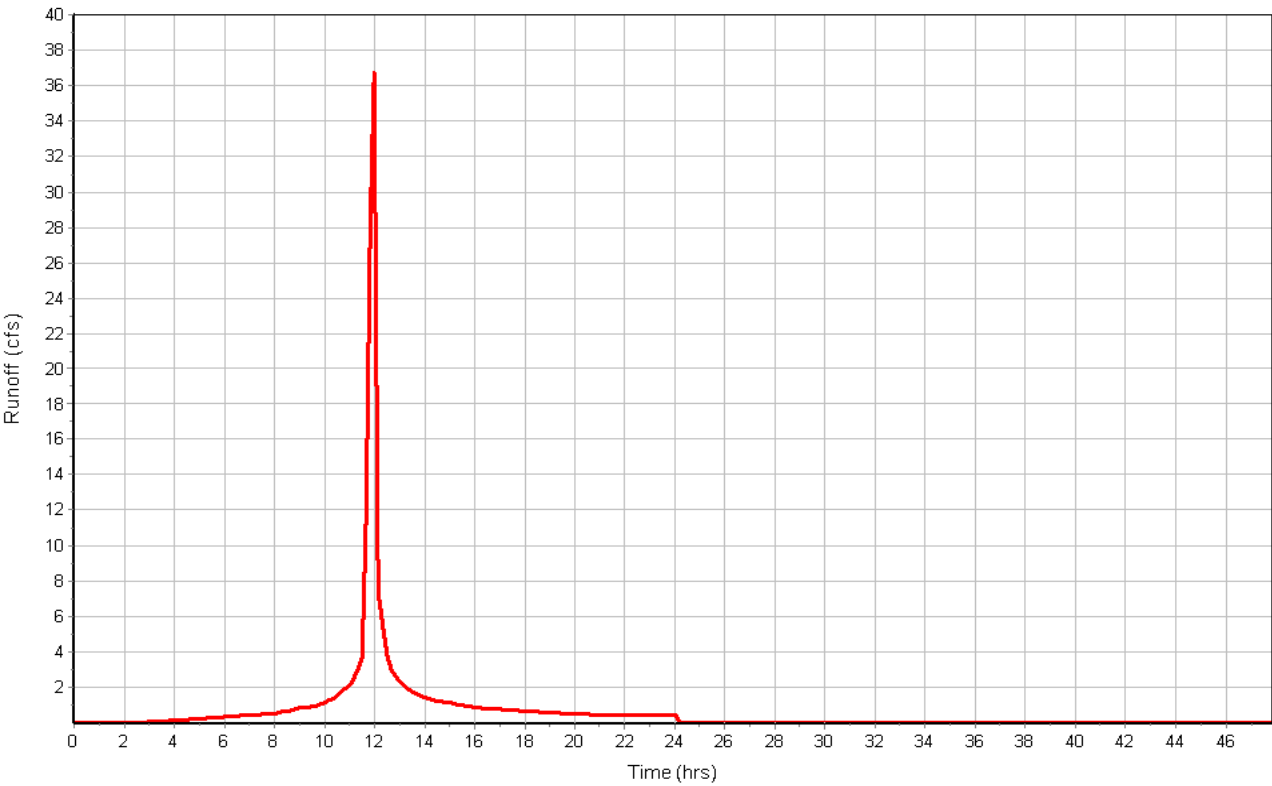


Subbasin : A2-1

Rainfall Intensity Graph

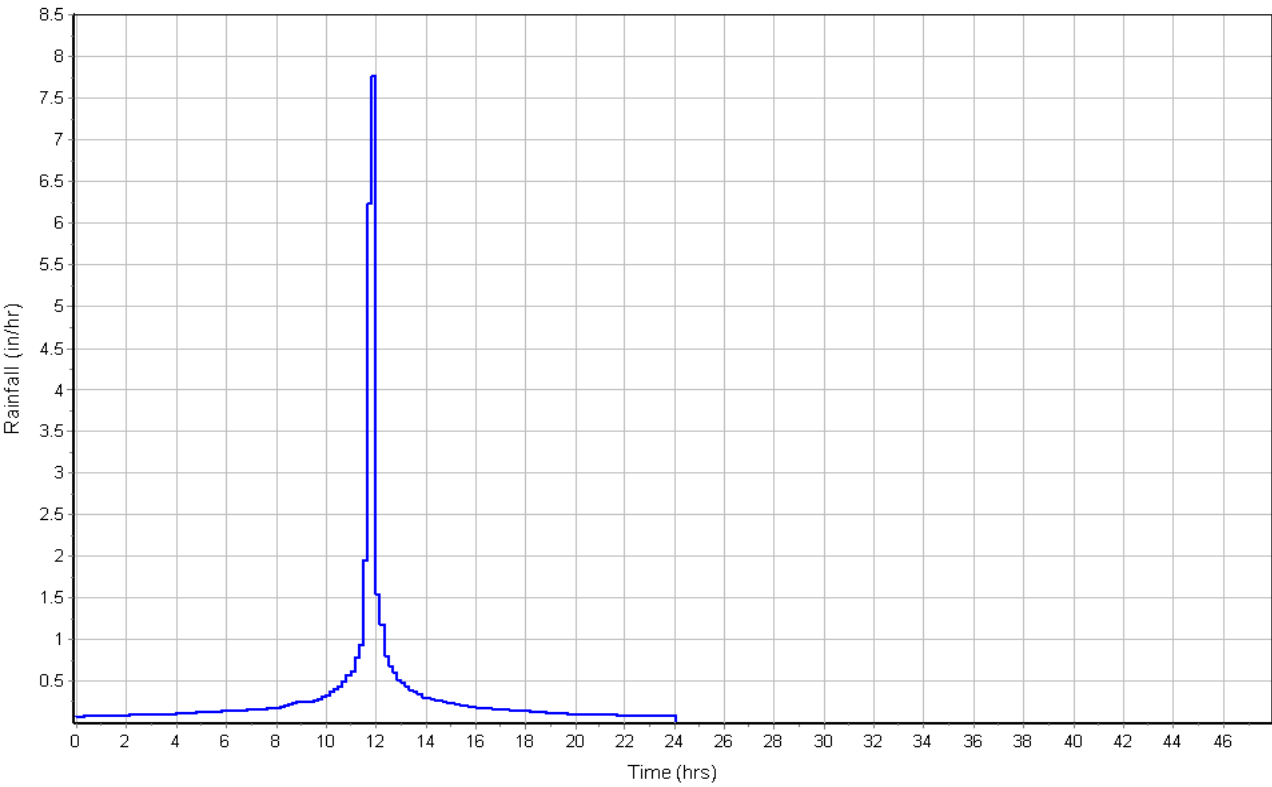


Runoff Hydrograph

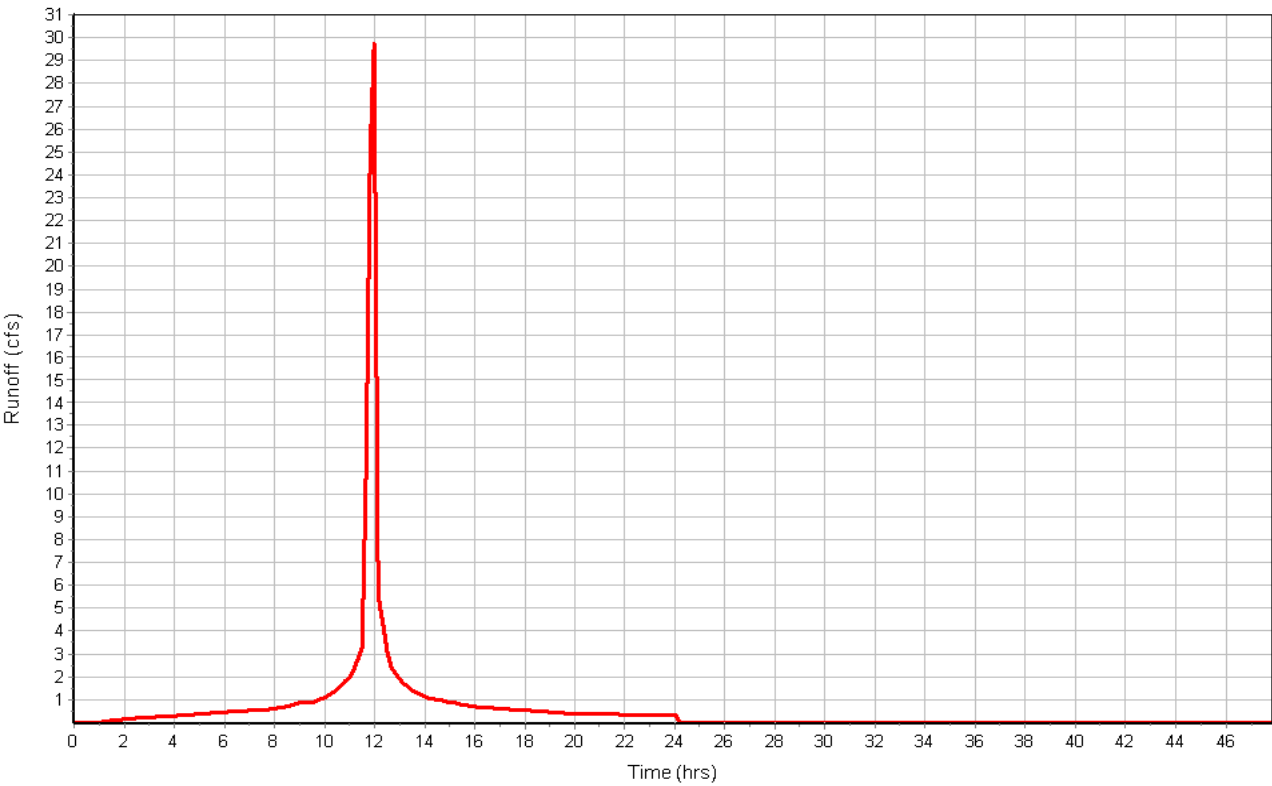


Subbasin : A2-2&B2

Rainfall Intensity Graph

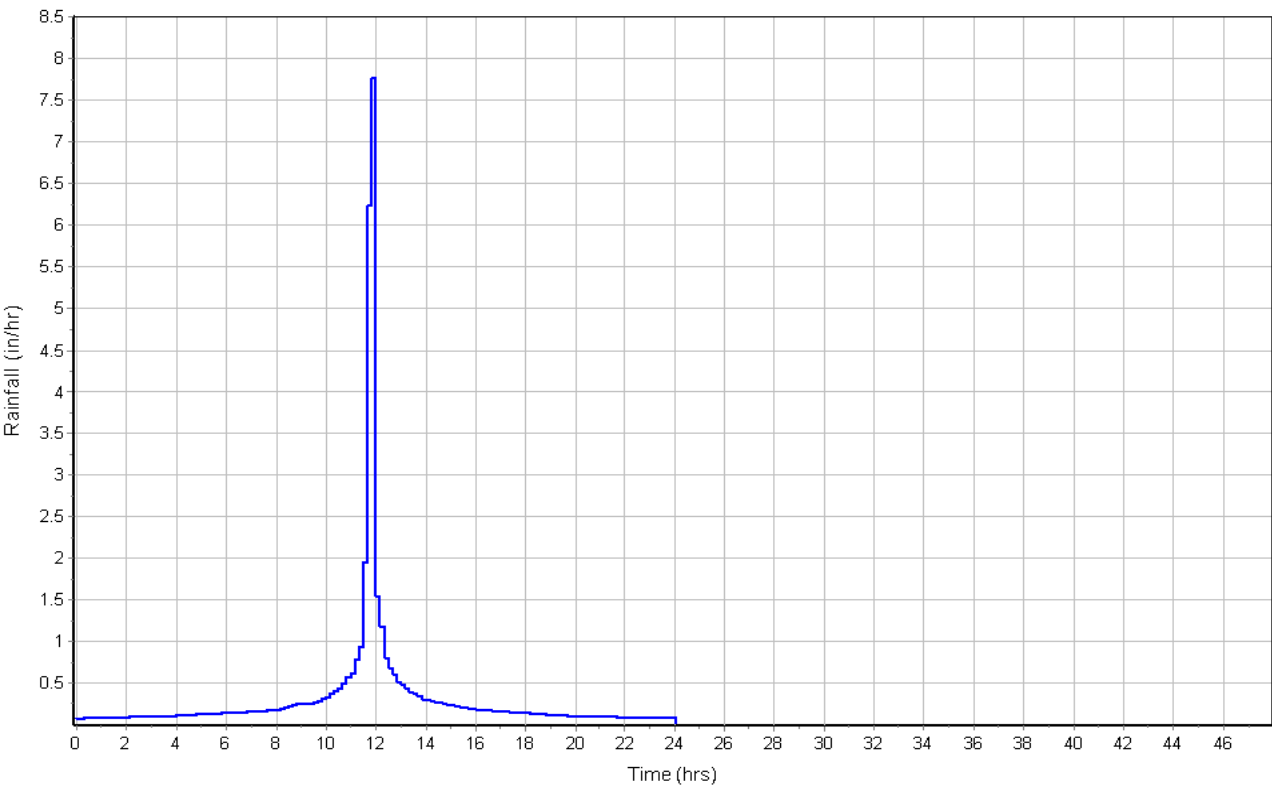


Runoff Hydrograph

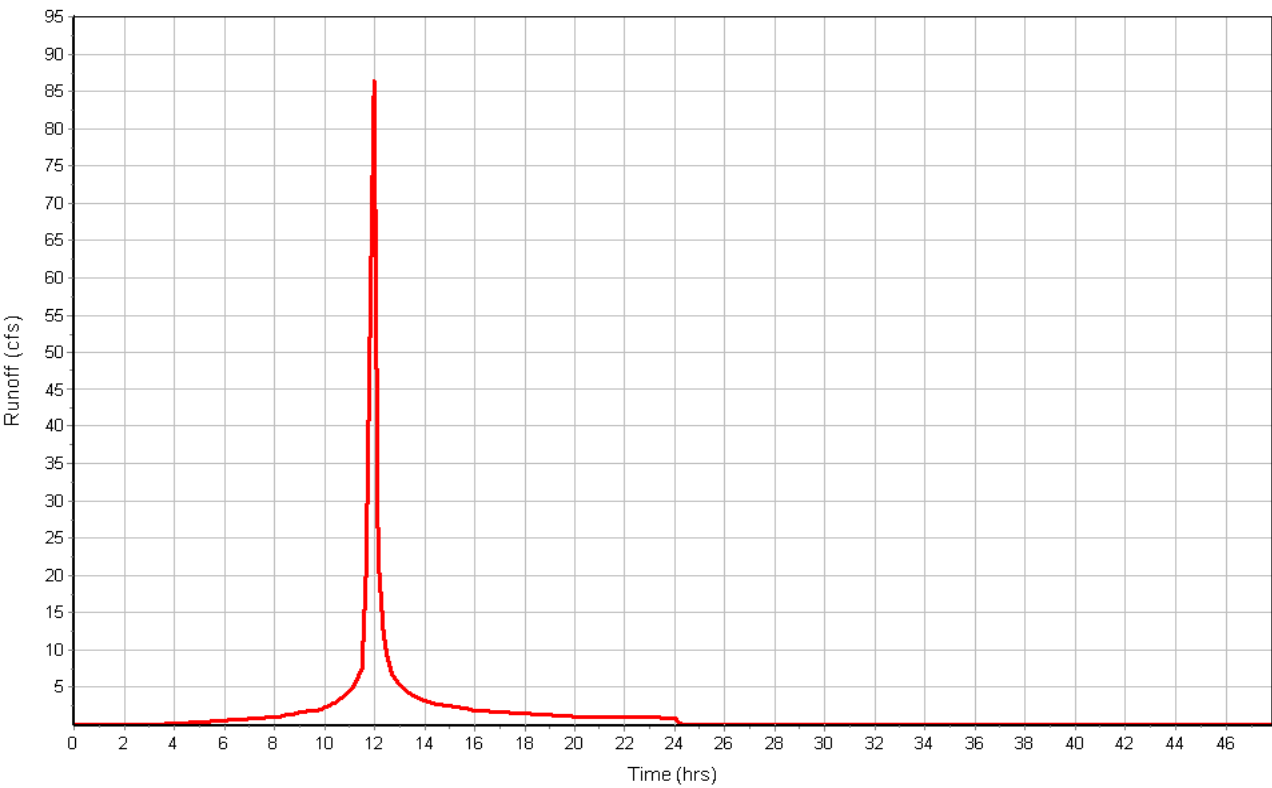


Subbasin : A3&B3

Rainfall Intensity Graph



Runoff Hydrograph



Junction Results

SN	Element ID	Peak Inflow	Peak Lateral Inflow	Max HGL Elevation Attained	Max HGL Depth Attained	Max Surge Depth Attained	Min Freeboard Attained	Average HGL Elevation Attained	Average HGL Depth Attained	Time of Max HGL Occurrence	Time of Peak Flooding Occurrence	Total Flooded Volume	Total Time Flooded
		(cfs)	(cfs)	(ft)	(ft)	(ft)	(ft)	(ft)	(ft)	(days hh:mm)	(days hh:mm)	(ac-in)	(min)
1	Jun-01	49.12	49.12	821.89	5.89	0.00	0.11	820.43	4.43	0 12:10	0 00:00	0.00	0.00

Pipe Results

SN Element ID	Peak Flow	Time of Peak Flow Occurrence	Design Flow Capacity	Peak Flow/ Design Flow Ratio	Peak Flow Velocity	Travel Time	Peak Flow Depth	Peak Flow Depth/ Total Depth Ratio	Total Time Surcharged	Froude Number	Reported Condition
	(cfs)	(days hh:mm)	(cfs)		(ft/sec)	(min)	(ft)		(min)		
1 EXISTIN-48INCH	0.00	0 00:00	145.23	0.00	0.00		4.00	1.00	2160.00		SURCHARGED
2 Link-02	48.69	0 12:10	41.30	1.18	7.75	0.26	2.00	1.00	2156.00		SURCHARGED

Storage Nodes

Storage Node : DETENTION POND

Input Data

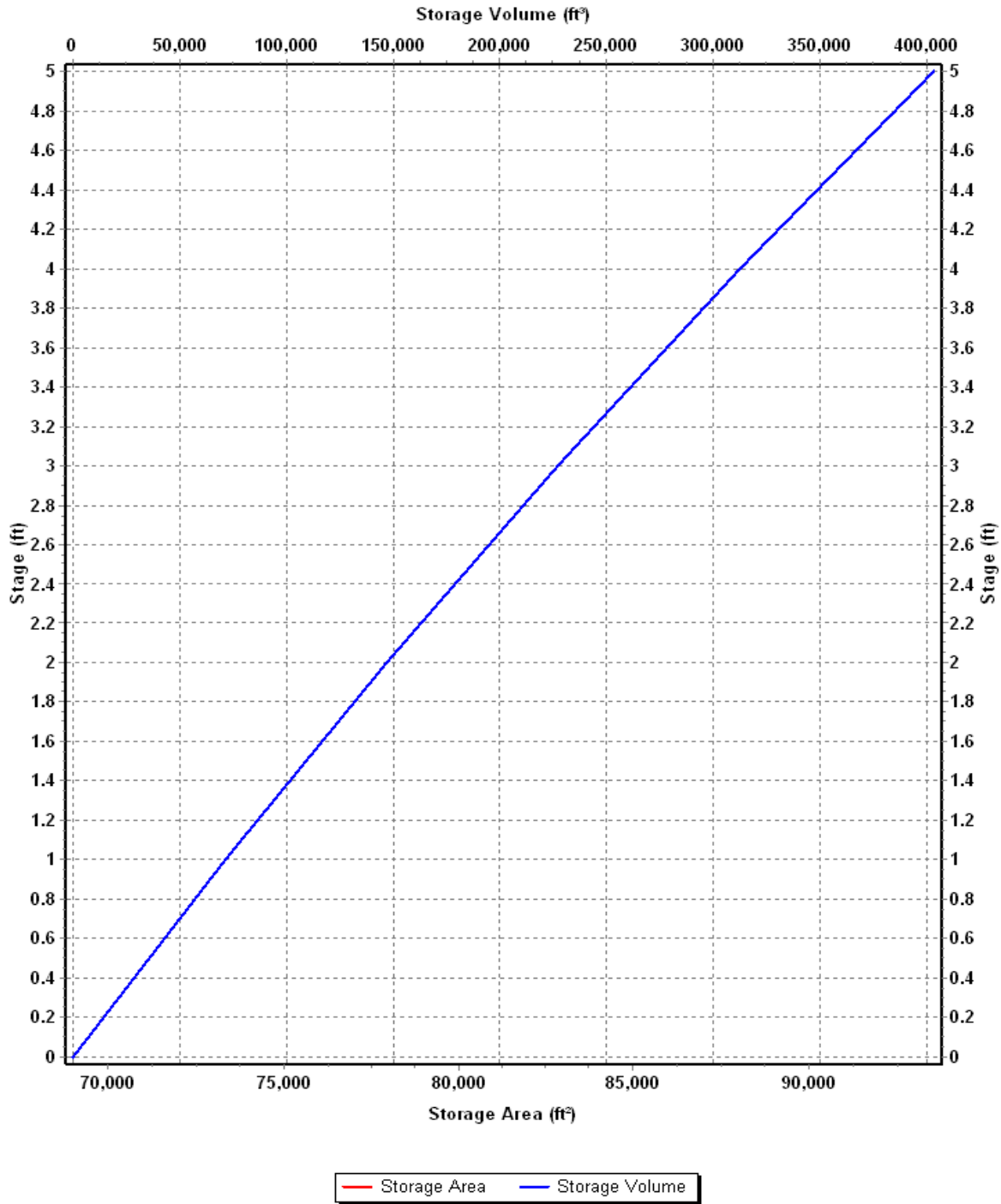
Invert Elevation (ft)	817.00
Max (Rim) Elevation (ft)	822.00
Max (Rim) Offset (ft)	5.00
Initial Water Elevation (ft)	817.00
Initial Water Depth (ft)	0.00
Ponded Area (ft²)	93568.00
Evaporation Loss	0.00

Storage Area Volume Curves

Storage Curve : DETENTION POND

Stage	Storage Area	Storage Volume
(ft)	(ft²)	(ft³)
0	69000	0.000
1	73334	71167.00
2	77940	146804.00
3	82835	227191.50
4	88039	312628.50
5	93568	403432.00

Storage Area Volume Curves

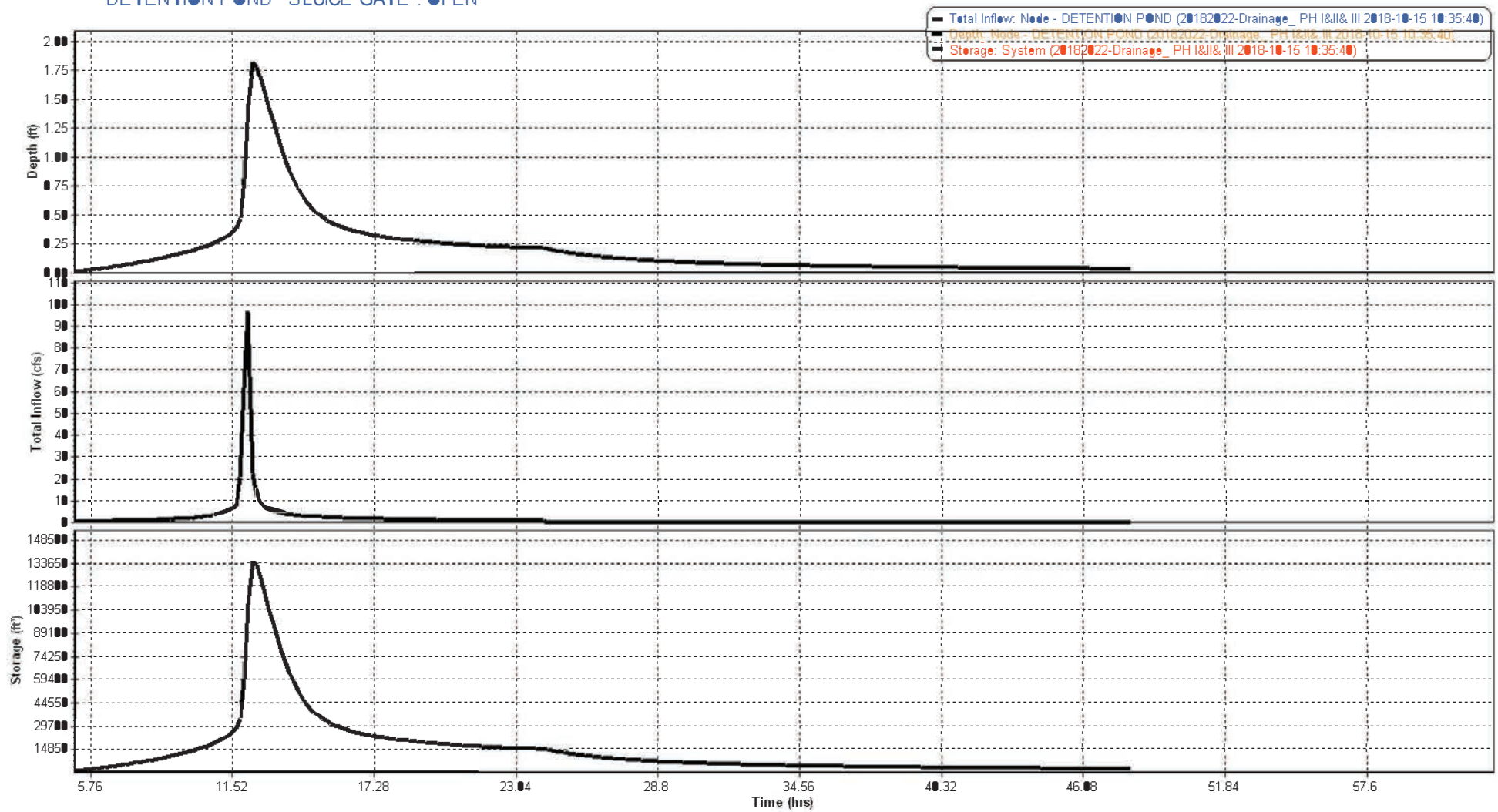


Storage Node : DETENTION POND (continued)

Output Summary Results

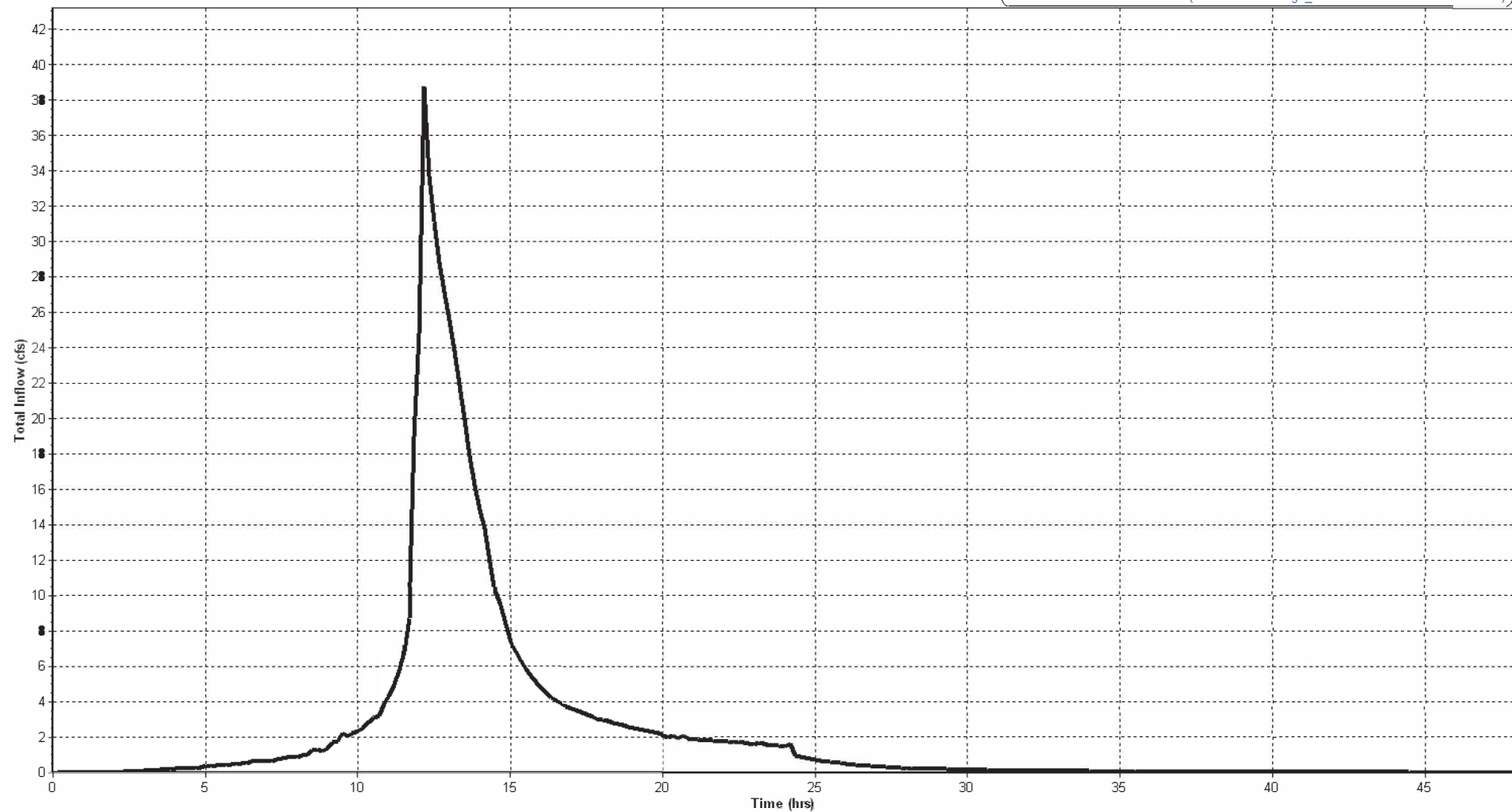
Peak Inflow (cfs)	134.08
Peak Lateral Inflow (cfs)	85.39
Peak Outflow (cfs)	0.00
Peak Exfiltration Flow Rate (cfm)	0.00
Max HGL Elevation Attained (ft)	821.88
Max HGL Depth Attained (ft)	4.88
Average HGL Elevation Attained (ft)	820.45
Average HGL Depth Attained (ft)	3.45
Time of Max HGL Occurrence (days hh:mm)	1 18:10
Total Exfiltration Volume (1000-ft³)	0.000
Total Flooded Volume (ac-in)	0
Total Time Flooded (min)	0
Total Retention Time (sec)	0.00

DETENTION POND : SLUICE GATE : OPEN

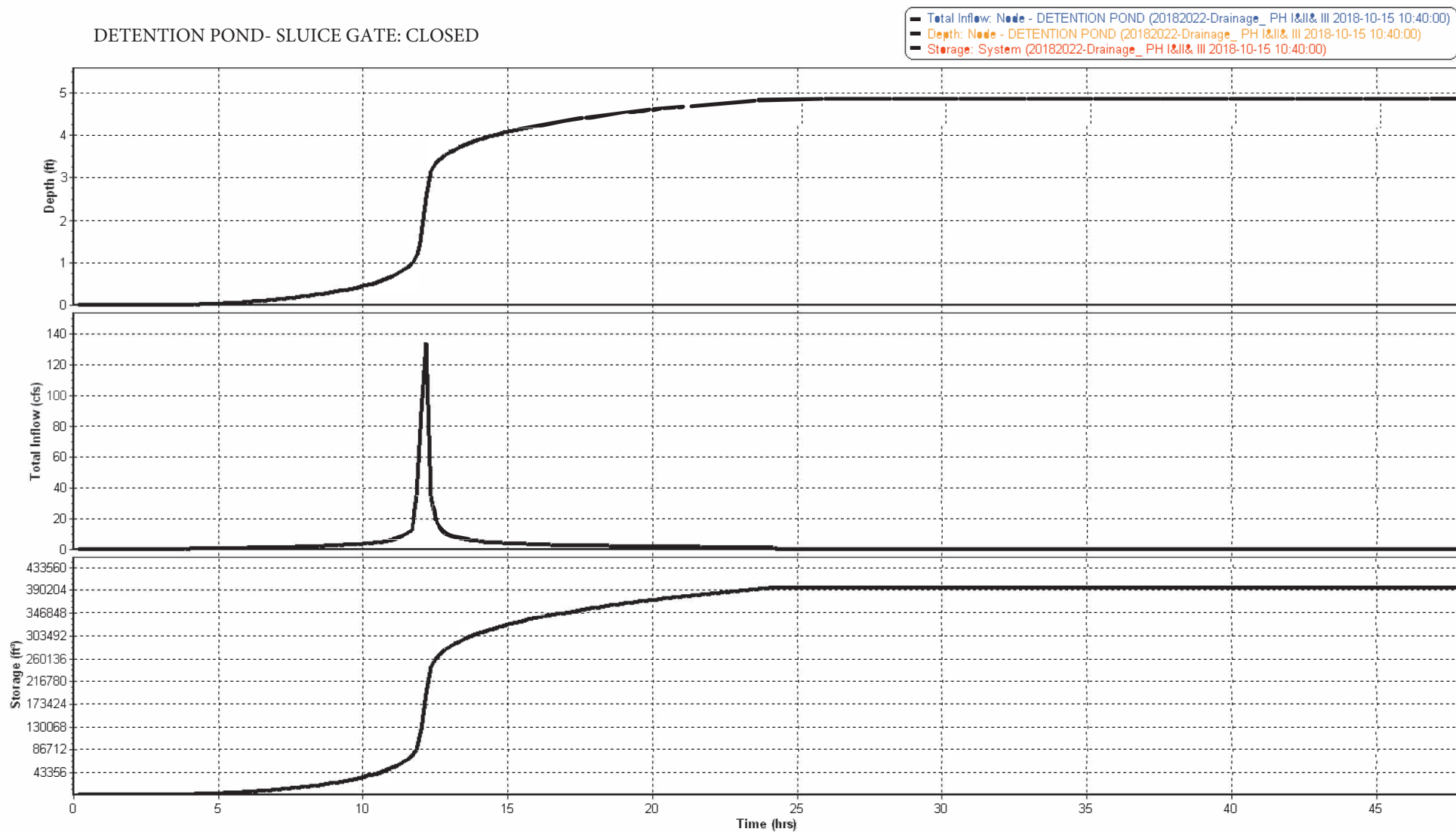


DETENTION POND- SLUICE GATE : OPEN

— Total Inflow: Node - Out-2-1 (2018-2022) Drainage_ PH I&I& III 2018-15 10:37:40)



DETENTION POND- SLUICE GATE: CLOSED


























APPENDIX D
FEMA FIRM PANEL




Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes, Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
OTHER FEATURES		Levee, Dike, or Floodwall
		Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature
	MAP PANELS	
		No Digital Data Available
		Unmapped

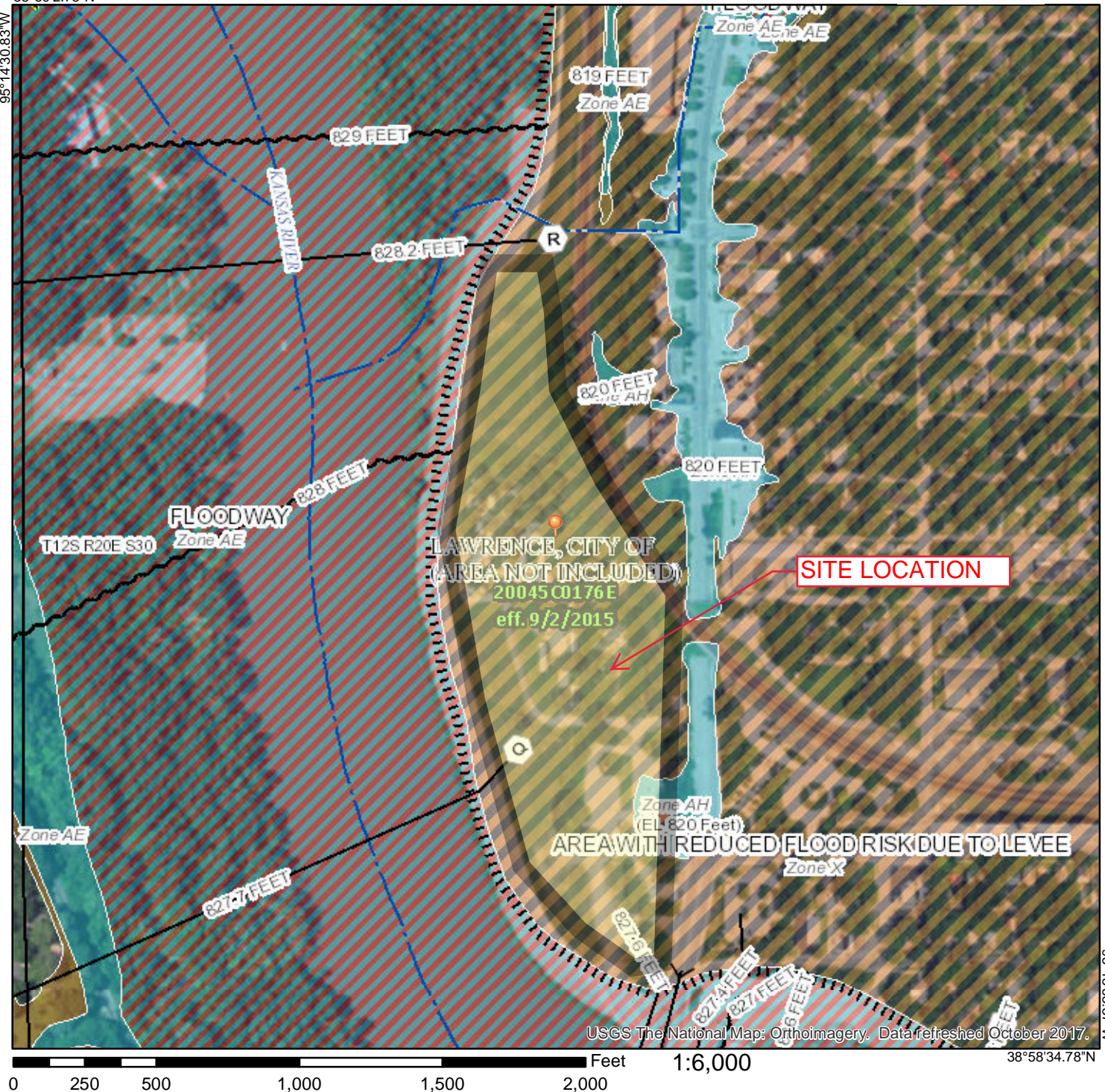


 The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 10/15/2018 at 11:11:51 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.





Civil Engineering
Landscape Architecture
Community Planning
Surveying

1310 WAKARUSA DRIVE | LAWRENCE, KANSAS 66049 | 785.843.7530 (p) | 785.843.2410 (f) | info@landplan-pa.com

October 12, 2018

City of Lawrence
Utilities Department
Attn: Andy Ensz, Utilities Engineer
P.O. Box 708
Lawrence, KS 66044

Subject: North Lawrence Riverfront Development – Sanitary Sewer Design

Mr. Ensz,

This letter is to address the sanitary sewer flows for the above referenced project. Originally, the Utilities Department spoke with Chris Storm, PE with Landplan Engineering back in 2016 about the same proposed development. The project consists of a 16.1 acre tract located west of North 2nd Street and east of the Kansas River Levee System. Two of the existing building located along North 2nd Street will remain with all the other buildings on-site being removed. The new development has increased in the number of units/ square footage being constructed, since the 2016 conversation and will have the following land use properties.

1. Commercial area – 97,500 sq. ft.
2. Office Space - 42,000 sq. ft.
3. Apartments - 520 units
4. Hotel - 200 units

The proposed construction will be phased with a combination of apartments and mix use being constructed in phase 1 (one building) next to North 2nd Street. Phase 2 will be the remaining units adjacent to the levee, except for the hotel which is proposed to be constructed in phase 3. Originally, the sanitary sewer flows for the project were going to be split with 34% being discharged to the north within the Lincoln Street system and the remaining 66% being discharged within the Locust- Elm System. The proposed development will be designed to discharge 100% of the flow into the Locust-Elm System, specifically manhole SW301220-001, with an 8" PVC pipe.

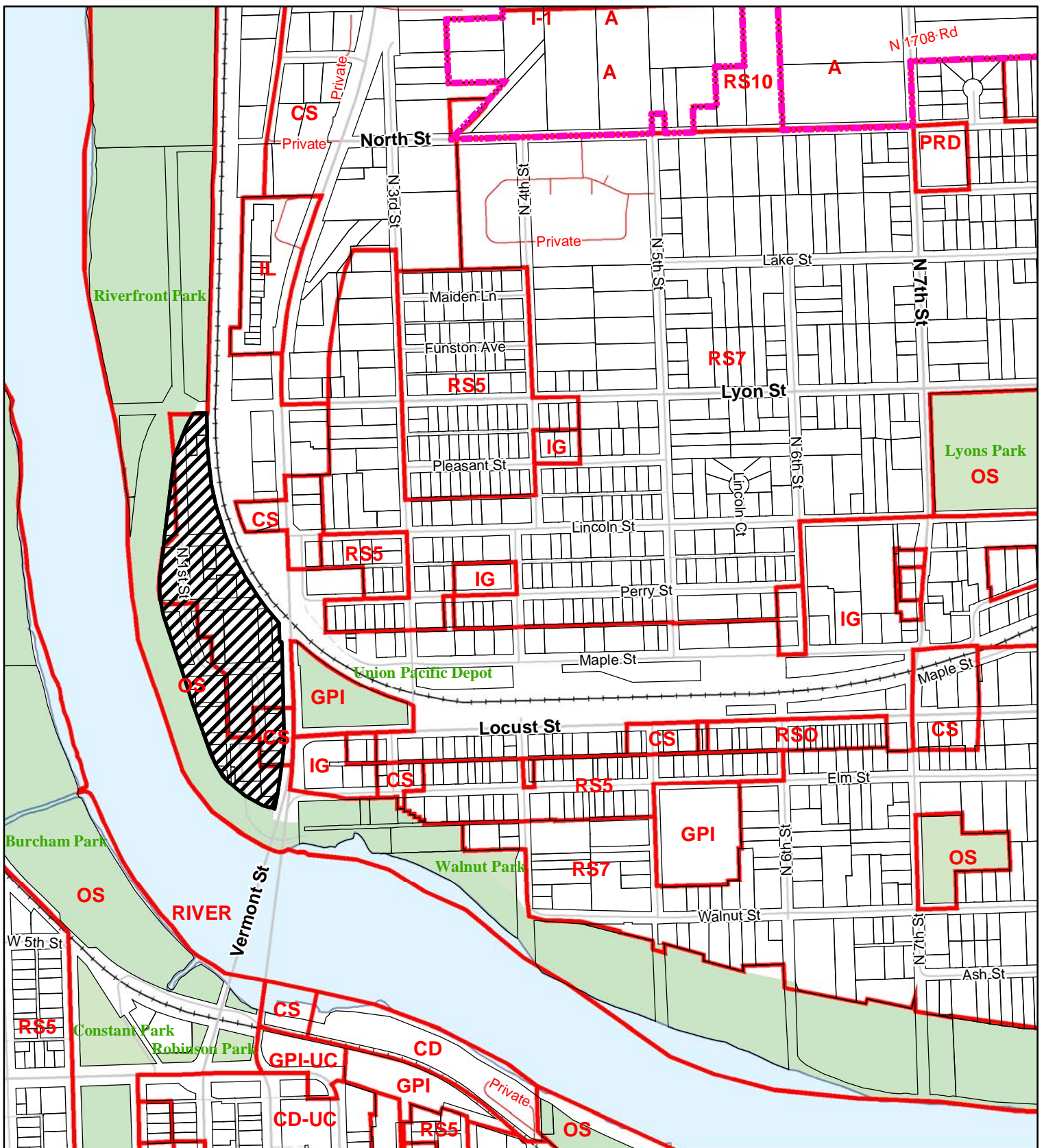
The original design that was discussed added an additional 0.326 MGD to the system, which was split into the two systems (North & South). The proposed design will add an additional 0.436 MGD to the South or the Locust-Elm system and isn't proposed to be split. The calculations for the proposed sewer flow are attached to this letter for your use.

If you have any questions feel free to contact me at 785-843-7530.

Thank you,


Shawn R. Bryan, P.E. CFM
Senior Project Engineer





PP-18-00504 & PDP-18-00506:
 Consider a Preliminary Plat and Preliminary Development Plan for
 approximately 16.116 acres
 located at 311, 317, 401, 409, 415, 501, & 505 N. 2nd STREET.



-----Original Message-----

From: Madison Mater <madisonrmater@gmail.com>

Sent: Friday, December 14, 2018 10:23 AM

To: Denny Ewert <dewert@lawrenceks.org>

Subject: Planning Commissioners

Hello there!

I just wanted to write to express that as a property owner and hopefully long term resident of North Lawrence, I am so very excited and supportive of the proposed development along the river levee. I feel that it will improve the value of my property as well as provide more retail options for myself and my neighbors of North Lawrence.

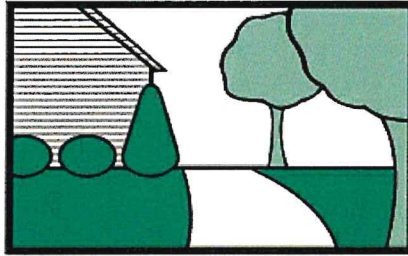
I personally walk along the levee often and am saddened by how under-utilized one of the most beautiful places of Lawrence is. As a city that was founded along the river I wonder why it is not more appreciated and enjoyed.

I bought my house with the idea that someday my elderly mother will live there and I would love for her to have such easy access to more retail and a place she can sit and enjoy watching the eagles in the winter as she so loves to do.

I hope that you plan to move forward with this project as it would personally mean a great deal to me. Thank you for your time and your consideration.

Respectfully,

Madison R. Mater



LAWRENCE
Landscape

Design, Installation, Maintenance, and Sprinkler Systems

December 17, 2018

Douglas County Planning Commission members,

I wish to offer my support to the professional staff's recommendation for approval of the rezoning and preliminary development plan for the North Lawrence Riverfront project. My interest is more than casual, being a business owner, as well as industrial and residential property owner for over 20 years in this neighborhood. As North Lawrence goes, so goes my future.

It seems this project makes sense on many levels.

Capitalizing on a defining natural feature, The Mighty Kaw.

Infill development- repurposing the collection of properties to their highest & best use

Continuing the charm & allure of downtown without expansion into the neighborhoods

Creating an attractive gateway into downtown from the North.

Leveraging the existing infrastructure.

An attractive project that will increase sales tax, and property tax revenue.

Increased residents in North Lawrence, hopefully enough for a viable grocery store.

Updated mechanical systems to Johnny's while preserving its charm.

Local ownership of this project, by successful Lawrencians, reinvesting here.

This seems to be the kind of project that the community should embrace.

It checks all the boxes.

Thanks for considering my comments.

A handwritten signature in blue ink, reading "Frank D. Male". The signature is fluid and cursive, with a long, sweeping underline.

Frank Male

785.979.4129

frank@lawrencelandscape.com

600 Lincoln Lawrence, Kansas. 66044
785.843.4370 info@lawrencelandscape.com

PLANNING COMMISSION REPORT Regular Agenda – Public Hearing Item

PC Staff Report
01/23/19

ITEM NO. 3: PCD TO CS; .18 ACRES; 3235 OUSDAHL RD (MKM)

Z-18-00508: Consider a request to rezone approximately 0.18 acres from PCD (Planned Commercial Development) District to CS (Strip Commercial) District, located at 3235 Ousdahl. Submitted by Paul Werner Architects, for Fraternal Investors LLC, property owner of record.

STAFF RECOMMENDATION:

Staff recommends approval of the rezoning request for approximately 0.18 acres from PCD-[Pine Ridge Plaza] (Planned Commercial Development) District to CS (Commercial Strip) District and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report.

REASON FOR REQUEST

Applicant's Response:

"A portion of 3235 Ousdahl is being purchased by the property owner to the north. The property to the north is zoned CS, and that property owner would like to expand their mini-storage facility, so the new piece of property they are purchasing needs to match the zoning of the property they currently own."

KEY POINTS

- The subject property is currently included in the Pine Ridge Plaza Planned Commercial Development Zoning District. This rezoning will remove the property from the Planned Development District; however, the property is not needed to meet any density or open space requirements of the planned development.
- The subject property is located within the boundaries of the *Revised Southern Development Plan*. The proposed zoning is consistent with the recommendations of the plan.
- Infrastructure to accommodate the proposed development is immediately available.
- A minor subdivision to consolidate the lots to the north to accommodate the mini-storage/boat and RV storage use, MS-17-00071, was approved on April 10, 2017. A revision was submitted in August of 2018 to add the subject property to the lot. This rezoning application was submitted for the subject property to avoid creating a lot with split zoning.

OTHER ACTION REQUIRED

- City Commission approval of rezoning request and adoption/publication of ordinance.
- Administrative review and approval of the revised Minor Subdivision, MS-17-00071, placement on the City Commission's agenda for acceptance of dedication and vacation of easement, and recording with the Douglas County Register of Deeds.
- Submittal and approval of a site plan application for any proposed site improvements.

- Submittal of construction plans to Development Services for processing of building permits. Building permits must be obtained prior to construction activity.

ASSOCIATED CASES

- Z-16-00545; Rezoning of approximately 1.07 acres north of, and adjacent to, the subject property from PCD-[Pine Ridge Plaza] to the CS (Commercial Strip) District to remove the property from the planned development and provide uniform zoning across the lot being proposed with Minor Subdivision, MS-17-0071. Approved by City Commission on April 4, 2017 with adoption of Ordinance 9337. (Parcel marked with X in Figure 1.)
- MS-17-00071; Minor Subdivision to consolidate three lots, including the 1.07 acre lot in the rezoning noted above, into one lot to accommodate development of a mini-storage/boat & RV storage use. The minor subdivision was administratively approved on April 10, 2017 and dedication of easements and right-of-way were accepted by the City Commission at their May 12, 2017 meeting. This minor subdivision is in the process of being revised to include the 0.18 acres that are included in this rezoning request. (The area outlined in bold in Figure 1 was included in the minor subdivision originally. This lot will be expanded with the revised minor subdivision to include yellow highlighted area.)

PUBLIC COMMENT

No public comment was received prior to the printing of this staff report.

Project Summary

The subject property is currently a part of the Pine Ridge Plaza Planned Commercial Development District. Approval of this rezoning request will remove this property from the planned development district and assign it a current base zoning designation. This removal should not impact the planned development as the property does not provide any required open space needed to meet any density requirements.

The owner of the property to the north intends to purchase the 0.18 acres that are the subject of this rezoning request and combine that with



Figure 1. Subject property, highlighted. If approved this parcel will be added to the property to the north which is being replatted as one lot, outlined.

the property he owns to the north (outlined in Figure 1). The rezoning will provide a uniform zoning designation for the consolidated lot and accommodate the proposed storage use on that property. The City Stormwater Engineer approved the conversion of the open channel drainage at this location into an underground main so the existing 60 foot wide drainage easement in this location will be vacated and replaced with a 25 foot wide drainage easement. These changes will result in additional developable area for the storage use.

The property was zoned PCD-2 prior to the adoption of the 2006 Land Development Code. This zoning designation converted to PCD-[Pine Ridge Plaza] with the adoption of the 2006 code; however, the standards of the PCD District remain applicable.

A final development plan for the lot containing the subject property was approved in 2008 for a “commercial office/warehouse” use, which has been developed. The development plan noted the specific uses which were permitted. The uses proposed for the new northern lot, *RV and Boats Storage* and *Mini-Warehouse* were included in Use Groups 13 and 14 in the pre-2006 code. The boat and RV storage use was not listed explicitly in the pre-2006 code, but a similar use: ‘boat and marine sales, rental, and repair’ was permitted in Use Group 13. The *Mini-Warehouse* use was listed as a permitted use in Use Group 14. The development plan notes that all uses in Use Group 14 are permitted on the subject property; however, the *RV and Boats Storage* / ‘boat and marine sales, rental and repair’ use is not listed as a permitted use. Therefore, the rezoning will alter the permitted uses to allow the *RV and Boats Storage* use on the subject property, which is approximately the northern 30 feet of 3235 Ousdahl Road.

The rezoning to the CS District would provide one consistent zoning district for the property included in the project and will allow the boat and RV storage use to extend approximately 30 feet further to the south. The minor subdivision will combine the three existing lots to the north and the subject parcel into one lot and create a smaller lot for the developed property to the south which will keep the planned commercial development zoning.

REVIEW & DECISION-MAKING CRITERIA

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response:

“All of the property in the area is zoned “commercial use”. This particular piece of property is part of a development plan, and the property to the north is a site plan. We are planning to include this small portion of the property from the development plan into the site plan for 3215 Ousdahl. We feel that this conforms with Horizon 2020.”

Recommendations in *Horizon 2020* are discussed below, with staff comments in red.

The land use recommendations for this area are provided in the *Revised Southern Development Plan*, which will be discussed in Section 4 of this report.

While the South Iowa area contains a variety of commercial zonings, it functions as a regional commercial center. The comprehensive plan notes, “*S. Iowa Street is considered an existing Regional Commercial Center. S. Iowa is a strip development that is intensely developed between 23^d Street and K-10. ...*”

Horizon 2020 identifies commercial development goals in Chapter 6. Goals for established commercial areas include the retention, redevelopment and expansion of established commercial areas in the community. (Page 6-24)

The area is identified for commercial zoning and land uses in the comprehensive plan and is currently zoned for commercial uses. The request is to update the zoning to a current commercial zoning district so it can develop under the standards of the 2006 Land Development Code.

Staff Finding – The proposed rezoning and development of the property is compliant with the recommendations of the comprehensive plan.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING ANY OVERLAY ZONING

Current Zoning and Land Use:	PD-[Pine Ridge Plaza] (Planned Commercial Development) District; the lot is developed with commercial office/warehouse uses, the subject property is a drainage way
Surrounding Zoning and Land Use:	<p>To the north: CS (Commercial Strip) District; <i>Mini-Warehouse</i> and <i>Undeveloped</i>. These lots are included in the proposed development.</p> <p>To the west: PD-[Pine Ridge Plaza] (Planned Commercial Development) District: <i>General Retail Sales</i></p> <p>To the east: RM15 (Multi-Dwelling Residential) and OS (Open Space) Districts; <i>Multi-Dwelling Structures</i> and vegetated drainage-way</p> <p>To the south: PD-[Pine Ridge Plaza] (Planned Commercial Development) District: commercial office/warehouse use which is classified in the 2006 Land Development Code as <i>Office</i> and <i>Wholesale Storage and Distribution, Light</i> (Figure 2)</p>

Staff Finding – The area contains a mix of uses with retail, warehousing, and apartments being the predominate uses. The retail uses are located west of the subject property and are oriented toward the west, away from the subject property. The properties along Ousdahl Road are primarily office and warehouse uses which serve as a buffer between the more intense retail uses to the west and the multi-dwelling residential use to the east. The proposed rezoning and uses are compatible with the zoning and land uses in the area.



Figure 2a. Zoning in the area. The general location of the property within the rezoning request is shown in black.



Figure 2b. Land use/development pattern in the area. General location of property within the rezoning request is outlined.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response:

"The area contains many commercial uses such as contractor shops, mini-storage, and multi-family."

The term 'neighborhood' refers to the area surrounding the subject property. The neighborhood is located near the southern border of the city and contains a mix of uses. Iowa Street and W. 31st Street have been developed with intense commercial uses such as a shopping center, department stores, and car dealerships. Ousdahl Road marks the eastern boundary of the commercial development south of W. 31st Street. The west side of Ousdahl Road is partially developed with a mini-storage facility and an electrical equipment supply company. The area east of Ousdahl is partially developed with apartment uses, with a multi-dwelling development currently under construction. Open space, in the form of a vegetated stream, is also located to the east of Ousdahl Road. The proposed rezoning would maintain the commercial nature of the



Figure 3. Neighborhood Area. Subject property shown in black.

property but would remove the restriction on the uses applied with the PCD Zoning from the subject property, which is the north 30 feet of 3532 Ousdahl Road. This minor change should not affect the character of the neighborhood.

The neighborhood is well served by a network of higher classification roads. (Figure 4) Iowa Street, on the west, is classified as a 'Principal Arterial/Freeway' in the City Future Thoroughfares Map. W. 31st Street, a principal arterial, provides connection through the area. The subject property is located on Ousdahl Road, a collector road.

Staff Finding – This is a mixed use neighborhood with the predominate uses being retail, warehouse, and apartments. In addition, the neighborhood contains a mix of higher classification roads. The proposed rezoning and use is compatible with the character of the neighborhood.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

The property is located within the boundaries of the *Revised Southern Development Plan*. The plan recommends 'Commercial' as the future land use on the subject property. (Figure 5) The plan notes that the commercial area is intended to "allow for retail and service type uses geared toward the community as a whole and auto-related uses geared toward traffic from Hwy 10." (Page 20, *Revised Southern Development Plan*) The plan recommends CC and PD zoning for the commercial uses; however, as the adjacent properties are zoned CS and PCD, the subject property would not be a suitable candidate for CC zoning. The CC zoning is intended for development of a community commercial center and is expected to occur at the intersection of collector/arterial streets or arterial/arterial streets. The subject property is not suited for CC zoning, but the CS zoning complies with the intent of the plan to have commercial uses in this area. The overall commercial development, with the mix of commercial zonings, functions as a commercial center. The proposed rezoning and



Figure 4. Major thoroughfares in the area: blue-freeway; red-principal arterial; green-minor arterial; and gold- collector. Subject property shown in black. Yellow highlighted area is property within the development project to which the rezoned property will be added.

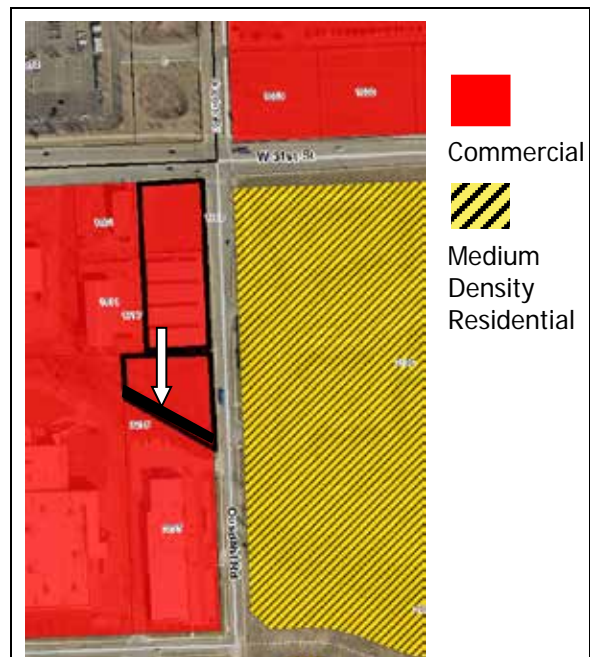


Figure 5. Future land use recommendations from *Revised Southern Development Plan*. Properties included in the project are outlined, with the subject property shown in black.

development is compliant with the recommendations in the *Revised Southern Development Plan*.

Staff Finding – The proposed rezoning and development comply with the land use recommendations of the *Revised Southern Development Plan*.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response:

"The suitability of the property will not be changing with the rezoning."

The subject property, a 30 foot wide strip along the north boundary of 3235 Ousdahl Road, is currently not well suited to development as it is encumbered with a drainage easement and contains a drainage way. The City Stormwater Engineer approved the request to vacate the drainage easement and change the stormwater management from over-ground flow to underground through this area. The minor subdivision will vacate the drainage easement and expand the southern boundary of the lot to include this rezoned parcel. The rezoning will provide one zoning designation for the new lot rather than having planned development zoning on the southern portion and commercial strip zoning on the northern portion. The unified zoning designation will provide one set of standards for the development of the property.

Staff Finding – The subject property is not well suited to the uses to which it is restricted with the current zoning due to the location of the drainage easement and drainage way. The vacation of the drainage easement and the installation of underground stormwater infrastructure will result in land that is suitable to the uses in either the PCD or CS District. One of the proposed uses is a not a permitted use in this phase of the development plan; however, this rezoning will remove the parcel from the planned development and include it in the CS District in which it is permitted. With the rezoning and the changes proposed to the stormwater management measures, the property will be suitable for the uses to which it is restricted.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response:

"It is currently a drainage ditch. By enclosing the drainage ditch, the land will become more usable."

Staff Finding – The subject property is part of a lot that is developed with commercial/warehouse uses but itself has not been developed due to the location of the drainage way. With this rezoning and the removal of the drainage easement, the subject property will be able to be developed.

7. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicants Response:

"This will not detrimentally affect the nearby properties. The drainage ditch will remain in place, it will just become enclosed. Being able to improve the existing mini-storage facility should increase the value of the neighboring properties."

The removal of restrictions will allow the subject property to develop under the design standards of the 2006 Land Development Code, rather than the standards of the planned

development included in the previous code and will allow the proposed *RV and Boats Storage* use to extend to this portion of the lot, if that is the developer's intent.

The 2006 Land Development Code provides additional measures to insure compatibility with adjacent properties; therefore, planned development overlays are needed less frequently. Features included in the Land Development Code to insure compatibility between commercial development and nearby properties include:

- A landscaped bufferyard between properties that are zoned commercially and those that are zoned residentially; therefore, a bufferyard will be required along the Ousdahl Road frontage.
- Compliance with the commercial design guidelines which are intended to insure aesthetically pleasing developments in the commercial districts.
- Additional measures may be applied by the Planning Director as determined to be necessary to insure compatibility with nearby residential properties.

Staff Finding – The removal of the restrictions will allow the property to be developed with site plan approval in compliance with the design standards of the 2006 Land Development Code. The design standards and other requirements in the Land Development Code, implemented through the site plan review should result in compatible development which would not detrimentally affect nearby properties.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicants Response:

"The gain to the public will be a more visually appealing piece of property by enclosing the drainage ditch."

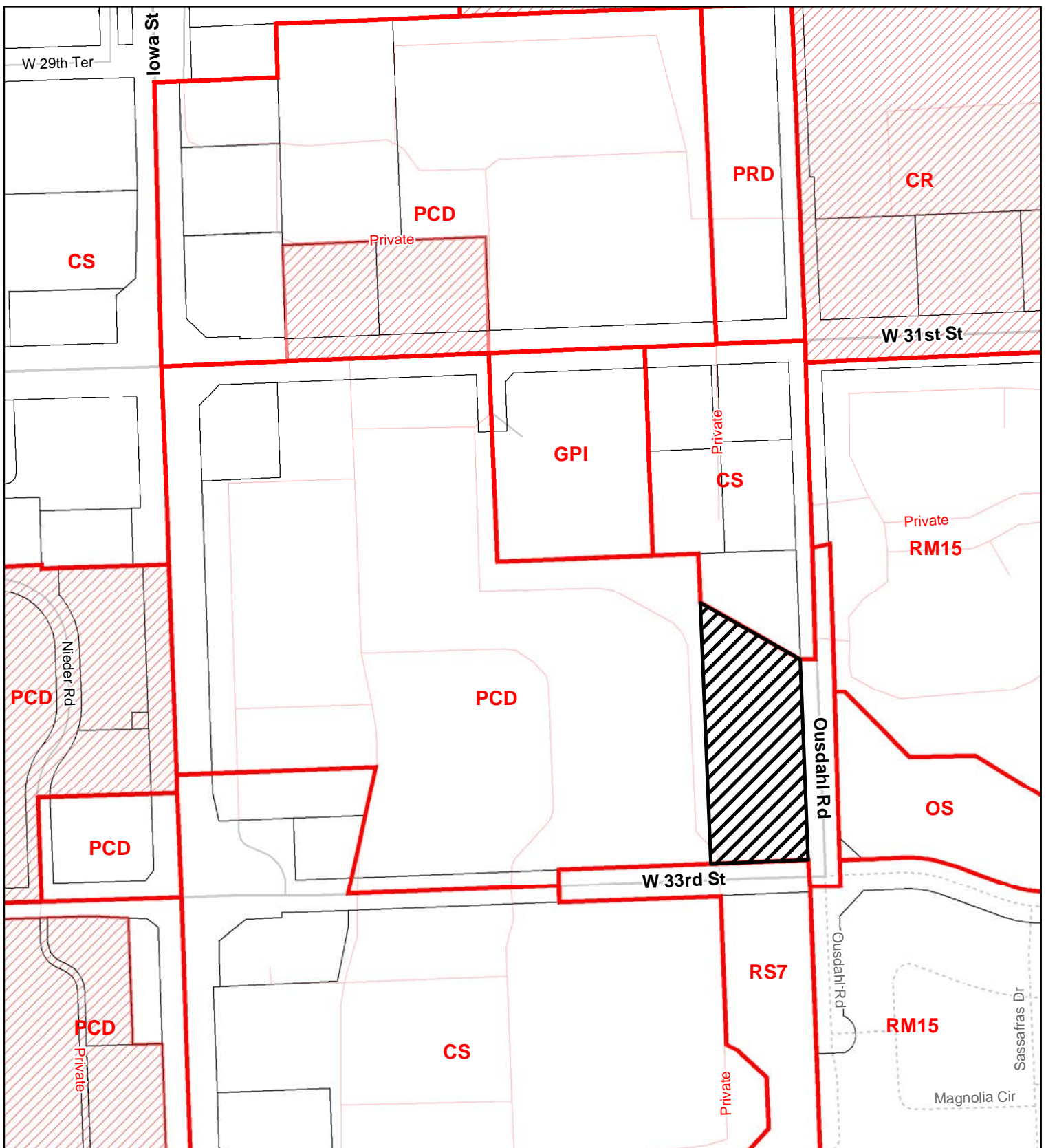
When determining the benefit to the health, safety, and welfare of the public by the denial of a rezoning request the negative impacts that would be avoided are evaluated. This rezoning will allow the lot to the north to be expanded 30 feet to the south, encompassing the entire width of the 60 foot wide drainage easement. When the drainage easement is vacated and underground stormwater measures are installed, the development can extend further to the south. Green infrastructure is typically preferred, such as the tree lined stream to the east; however, the City Stormwater Engineer noted that extending the box culvert will ease a maintenance issue with the ditch and that the length of the improvement doesn't adversely affect the stream since it is coming from a closed conduit immediately upstream. Public improvement plans for the new stormwater infrastructure have been submitted and are under review.

Staff Finding – Denial of the request would not provide any benefit to the public health, safety, or welfare as the rezoning would revise the use restrictions on a small portion of property, the northern 30 feet, and the stormwater management measures will be installed per the approval of the City Stormwater Engineer. Denial would impact the applicant by limiting the development to that proposed with the original zoning and minor subdivision.

PROFESSIONAL STAFF RECOMMENDATION

This staff report reviews the proposed rezoning request for its compliance with the comprehensive plan, adopted plans for the area, the Golden Factors, and compatibility with surrounding development.

Staff recommends approval of the rezoning request for approximately 0.18 acres from the PCD-[Pine Ridge Plaza] (Planned Commercial Development) District to the CS (Commercial Strip) District based on the findings of fact listed in this report.



Z-18-00508: Rezone Approximately 0.18 acres
from PCD (Planned Commercial Development)

District District to CS (Commercial Strip) for the property located at 3235 OUSDAHL ROAD.

NOTE: Proposed area to be rezoned is located along the northern property line of the subject parcel.



PLANNING COMMISSION REPORT
Regular Agenda -- Public Hearing Item

PC Staff Report
12/19/2018

ITEM NO. 4 TEXT AMENDMENT TO LAND DEVELOPMENT CODE; CONDITIONAL ZONING (SLD)

TA-18-00430: Consider a Text Amendment to the City of Lawrence Land Development Code, Articles 13 to define and clarify the use of conditional zoning. *Initiated by Planning Commission on August 22, 2018.*

RECOMMENDATION: Staff recommends the Planning Commission forward the proposed amendment, TA-18-00430, amending Article 13 of the Lawrence Land Development Code to the City Commission with a recommendation to approve the amended text.

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

- None received

ATTACHMENTS

- A. Lesser Change Table Exhibit
- B. Section 20-1301 General Development Procedures - Existing Text
- C. Section 20-1303 Zoning Map Amendments (Rezoning) – Existing Text
- D. Proposed Text Amendment Articles 20-1301 and 20-1303

BACKGROUND

On March 20, 2018, the City Commission considered a rezoning request (Z-17-00689) which included conditions. At that meeting, the City Commission expressed disfavor in the use of conditional zoning and directed staff to review the use of this tool with the intent of removing conditional zoning from the Land Development Code.

On May 23, the Lawrence-Douglas County Metropolitan Planning Commission conducted a public hearing to discuss the merits of conditional zoning related to text amendment TA-18-00154. Staff reviewed how conditional zoning has been applied since 2006 using specific density restrictions, commercial retail caps, processing requirements, and use restrictions to implement specific land use goals of *Horizon 2020* and other applicable area and neighborhood plans.

The tool has been valuable and flexible addressing sensitive infill development and redevelopment projects that could have significant neighborhood impacts. On May 23, 2018, The Planning Commission passed a motion, by an 8-1 vote, to retain the existing code language maintaining conditional zoning in the Land Development Code.

The City Commission received this recommendation on July 17, 2018 and cautiously concurred with the recommendation that directs staff to discuss the topic again with the Planning Commission to determine if the language could be. The topic was further considered by the Planning Commission on August 22, 2018. The Planning Commission voted unanimously to initiate a text amendment to the City of Lawrence Land Development Code to define and clarify the use of conditional zoning.

For the purpose of this discussion, conditional zoning refers to conditions placed on conventional base zoning districts. That is to say, a rezoning or zoning request that is not for a special use permit, a planned development, or part of a site plan. Conditional zoning excludes previous Planned Development overlays that include restrictions or conditions to either the plan or the associated zoning district.

Frequently, conditions included in zoning applications are self-identified by an applicant as part of a proposal to be transparent about the intended use of the property.

ANALYSIS

Since 2006, the City of Lawrence has approved 35 zoning map amendments (rezoning) applications that included a condition or restriction. Common restrictions include limitations on the maximum residential density or commercial retail square footage. Others prohibit or limit certain uses that would otherwise be permitted by right in a specific district. In each case, the restriction was directly related to the context of the surrounding area with an intent to preserve the character of the area, to implement goals and policies of *Horizon 2020* or incorporated specific plans, or provide additional public processes such as City Commission review and approval of a site plan. An example of this conditional zoning requiring an extraordinary approval exists in the Inverness Park Plan Area. Both 3905 Clinton Parkway and 4300 W. 24th Place require City Commission approval of any site plan for otherwise conventional zoning. This condition was established through the neighborhood planning process and implemented in the rezoning applications providing additional notice to the neighborhood of any development application.

The Land Development Code currently includes certain provisions that may be conditions or restrictions of an application that a review body may consider:

- Less intense use or zoning district than indicated in the application (20-1301 (i) (3)).
- Reduce the impact of the development.
- Reduce the amount of land area included in the application.
- Execution of a Development Agreement (20-1301 (i) (4)).
- Compliance with the Access Management Standards.
- Compliance with the Community Design Manual.
- Reduce maximum density or intensity. (20-1301 (i) (6)).

These same provisions are granted to the decision making body in Section 20-1301 (j) (3-6).

Conditional zoning is intended to mitigate adverse impacts that can be reasonably expected to occur without imposing such conditions on an otherwise appropriate land use development project. Conditions must be reasonable and further the public health, safety, and welfare of the community and be consistent with the community's comprehensive plan, or further a community or neighborhood goal or policy.

When reviewing any rezoning application there is a unique set of facts associated with that specific property. The analysis of the rezoning includes the assessment of specific criteria (findings of fact) through a regimented process. Adherence to this process prevents spot zoning. Disregard of these findings and process could result in spot zoning.

Section 20-1303 (g) states,

*In reviewing and making decisions on proposed zoning map amendments, review and decision – making bodies shall **consider at least the following factors.** (Emphasis added)*

The review of a conditional zoning request would be considered with these criteria and the additional considerations.

OVERVIEW OF PROPOSED AMENDMENT

This section of the report includes the existing and revised conditional zoning language for consideration. There are two sections of the Land Development Code that are key to the foundation of this land use entitlement tool. Section 20-1301 provides general provisions for all processes and application processes governed by the Land Development Code. Section 20-1303 is specific to zoning map amendments (rezoning applications). Changes to specific text are constrained to only address rezoning applications. The intent of the proposed language is to add criteria for evaluating requested conditional zoning.

Affected Section:

Article 13 Development Review Procedures

20-1301	General <i>[this section applies to all procedures of the Land Development Code]</i>
20-1303	Zoning Map Amendments (Rezoning)

PROPOSED DEFINITION

***Conditional Zoning:** The attachment of special conditions or restrictions to a rezoning. Conditions can include, but are not limited to restrictions as to use, size, design, density or intensity of development and/or development timing as a means to mitigate potential adverse impacts that could be expected to occur without imposing such conditions.*

CRITERIA FOR REVIEW AND DECISION-MAKING

Section 20-1302(f) provides review and decision-making criteria on proposed text amendments. It states that review bodies shall consider at least the following factors:

1) Whether the proposed text amendment corrects an error or inconsistency in the Development Code or meets the challenge of a changing condition;

This text amendment to the Land Development Code is proposed in response to a need identified by the City Commission and Planning Commission to define and clarify the use of conditional zoning. This amendment refines the Land Development Code and is not required due to an error or inconsistency.

2) Whether the proposed text amendment is consistent with the Comprehensive Plan and the stated purpose of this Code (Sec. 20-104)

Specific goals of the *Horizon 2020* state the intent to protect and preserve neighborhood character, provide compatible infill development, and encourage appropriate commercial development scaled in size and intensity to the surrounding area. These overall goals are found in the principal strategies listed in Chapter 3, General Plan Overview; Chapter 5, Residential Land Use; Chapter 6 Commercial Land Use; and Chapter 7 Industrial and Employment-Related Land Use. Conditional zoning, when used judiciously and thoughtfully, is an appropriate tool to implement the goals and policies of the comprehensive plan.

Conclusion

The application of conditional zoning should be used judiciously, when all other options have been exhausted, when a clear neighborhood goal can be achieved, and/or when a clear harm to the surrounding area can be demonstrated. This proposed text amendment is intended to articulate a definition of conditional zoning, as well as establish criteria for its application. Minor changes to the format of Sections 20-1301 and 20-3013 are proposed to improve readability and to formally incorporate conditional zoning.

The Lesser Change Table is provided for the Planning Commission to determine “hierarchy of Zoning Districts” and for determining when public notification or re-notification is required (20-1301 (K)). The table below is intended to graphically represent the density hierarchy of the residential zoning districts and the intensity of the non-residential zoning districts.

The Lesser Change Table does not require a new public hearing from the one advertised. Any action to rezone property to a higher intensity zoning district shall require a new public hearing after applicable notice.

Example: Applied Lesser Change

1. Proposed Multi-Dwelling Residential Zoning (RM15). Maximum density is 15 dwelling units per acre

The commission recommend and the City Commission may approve a less dense residential district including RM12, RS3, RS5, RS7, RS10, RS20, or RS40.

Density Condition: If condition would result in density of 12 dwelling units per acre then the appropriate action would be to rezone to RM12. A condition may not permit density greater than 15 dwelling units per acre; but may restrict density to 14 or 13 dwelling units per acre.

- Review bodies are not required to recommend approval of the maximum density requested (20-1301 ((i)(6))).
- Review bodies may recommend conditions or modifications (20-1301 (i)(3)).

2. Proposed Single-Dwelling Residential Zoning (RS-7). Minimum lot area requirement is 7,000 Square feet. District does not allow duplex.

- Condition may not otherwise permit a duplex use since it is not an allowed use in the base district. (20-1301 (i)(5)).

Condition could potentially require development of lots larger than 7,000 square feet, to result in a lower density or protect a particular natural resource. This standard could be implemented through the Subdivision Regulations by reviewing lot and block size as part of the preliminary plat. (20-1301 ((i)(6))).

3. Proposed intensive commercial zoning (CC200). Maximum gross floor area shall not exceed 65,000 square feet for a *General Retail Sales use* (single use).

- Condition may not otherwise permit an *Extended Care, General use*. The use is not permitted in the base district (20-1301 (i)(5)).

Condition could establish a lower maximum cap of gross floor area permitted per building such as “ *A General Retail Sales establishment shall not exceed 45,000 square feet of gross floor areas*”. (20-1301 ((i)(6))).

RESIDENTIAL DEVELOPMENT													
RS40	RS20	RS10	RS7	RS5	RS3	RSO	RM12, RM12D	RM15	RM24	RM32	RMG	RMO	
-	RS40	RS20	RS10	RS7	RS5	-	-	RM12 RM12D?	RM15	RM24	RM32	-	
		RS40	RS20	RS10	RS7	RS5	RS3	-	RM12 RM12D?	RM15	RM24	-	
			RS40	RS20	RS10	RS7	RS5	RS3	-	RM12, RM12D	RM15	RM15	
				RS40	RS20	RS10	RS7	RS5	RS3	-	RM12, RM12D	RM12	
					RS40	RS20	RS10	RS7	RS5	RS3	RSO	RSO	
						RS40	RS20	RS10	RS7	RS5	RS3	RS3	
							RS40	RS20	RS10	RS7	RS5	RS5	
								RS40	RS20	RS10	RS7	RS7	
									RS40	RS20	RS10	RS10	
										RS40	RS20	RS20	
											RS40	RS40	

COMMERCIAL DEVELOPMNET									
CO	CN1	CN2	CD	CC200	CC400	CC600	CR	CS	
	-	CN1	CN1	CN1	CC200	CC400	CC600	CN1	
		RSO	CN2	CN2	CN2	CC200	CC400	CN2	
		RMO	CC200			CN2	CC200	CO	

INDUSTRIL DEVELOPMENT				
IBP	IL	IM	IG	
	IBP	IL	IM	
	CN2	IBP	IL	
			IBP	
			CN2	

ARTICLE 13 DEVELOPMENT REVIEW PROCEDURES

20-1301	General
20-1302	Text Amendments
20-1303	Zoning Map Amendments (Rezoning)
20-1304	Planned Developments
20-1305	Site Plan Review
20-1306	Special Uses
20-1307	Institutional Development Plan
20-1308	Floodplain Development Permit
20-1309	Zoning Variances
20-1310	Written Interpretations
20-1311	Appeals of Administrative Orders, Requirements, Decisions, or Determinations

20-1301 GENERAL

(a) Summary of Procedures

The following table provides a summary of the procedures in this Article. In the event of conflict between this summary table and the detailed procedures in this Development Code, the detailed procedures govern.

Procedure	Review and Decision-Making Bodies				Notice
	Staff	PC	BZA	CC	[2]
Text Amendments (§0)	R	<R>		DM	N
Zoning Map Amendments (§0) [3]	R	<R>		DM	N/P/M
Planned Developments (§ 20-1303(l)(2)(v))					
Preliminary Development Plan	R	<R>		DM	N/P/M
Final Development Plan	DM			<A>	M
Site Plan Review (§0)	DM			<A> [4]	P/M
Special Uses (§Article 12. 20-1305(o)(3))	R	<R>		DM	N/P/M
Zoning Variances (§0)	R		<DM>		N/M
Written Interpretations (§0)	DM		<A> [5]		
Appeals of Administrative Decisions (§0)			<DM>		N/M
PC = Planning Commission BZA = Board of Zoning Appeals CC = City Commission <>= Public Hearing Required					
[1] R = Review Body (Responsible for Review and Recommendation); DM = Decision-Making Body (Responsible for Final Decision to Approve or Deny); A = Authority to hear and decide appeals of Decision-Making Body's action.					
[2] Notices: N = Newspaper (published); P = Posted (signs); M = Mailed (See sub-section (q)(3) of this section)					
[3] See Section 20-308(d) for special procedures applicable to UC, Urban Conservation District zoning map amendments.					
[4] City Commission is authorized to hear and decide appeals of Planning Director's decision on Site Plans.					
[5] Appeals processed as "Appeals of Administrative Decisions."					

(b) Authority to File Applications

Unless otherwise expressly stated, applications for review and approval under this article may be initiated by (1) all the **Owner** of the property that is the subject of the application; (2) the **Landowners'** authorized **Agent**; or (3) any review or decision-making body.

(c) Form of Application

Applications required under this Development Code shall be submitted in a form and in such numbers as required by the official responsible for accepting the application. Officials responsible for accepting applications shall develop checklists of submittal requirements and make those checklists available to the public. Application forms and checklists of required submittal information are available in the office of the [Planning Director](#).

(d) Pre-application Meetings

- (1) All applicants for matters that require a public hearing are required to attend a pre-application meeting with staff. Pre-application meetings are also required whenever the provisions of this Article expressly state that they are required. Pre-application meetings shall be scheduled by the applicant to allow adequate time to review and respond to issues raised at the pre-application meeting. The meeting shall occur at least 7 [Working Days](#) before submitting an application.
- (2) All other applicants are encouraged to arrange a pre-application meeting with City staff. The [Planning Director](#) will provide assistance to applicants and ensure that appropriate City staff members are involved in pre-application meetings.

(e) Application Processing Cycles

The [Planning Director](#) may, after consulting with review and decision-making bodies, promulgate processing cycles for applications. Processing cycles may establish:

- (1) deadlines for receipt of complete applications;
- (2) dates of regular meetings;
- (3) the scheduling of staff reviews and staff reports on complete applications; and
- (4) any required time-frames for action by review and decision-making bodies.

(f) Application Filing Fees

Applications shall be accompanied by the fee amount that has been established by the City Commission. Fees are not required with applications initiated by review or decision-making bodies. Application fees are nonrefundable.

(g) Application Completeness, Accuracy and Sufficiency

- (1) An application will be considered complete and ready for processing only if it is submitted in the required number and form, includes all required information and is accompanied by the required filing fee.
- (2) Within 5 [Working Days](#) of application filing, the [Planning Director](#) shall determine whether the application includes all information required for processing (See Section 20-1301(c)). If an application does not include all of the required information it will be deemed incomplete. If an application includes all of the required information it will be deemed complete. If the application is deemed incomplete, written notice shall be provided to the applicant and the applicant's [Agent](#). The notice shall include an explanation of the application's deficiencies.

- (3) No further processing of incomplete applications will occur and incomplete applications will be pulled from the processing cycle. When the deficiencies are corrected, the application will be placed in the next processing cycle. If the deficiencies are not corrected by the applicant within 60 days, the application will be considered withdrawn. If an application is deemed withdrawn because of failure to correct application deficiencies, notice shall be sent to the applicant and the applicant's [Agent](#).
- (4) Applications deemed complete will be considered to be in the processing cycle and will be reviewed by staff and other review and decision-making bodies in accordance with the procedures of this Article and the processing cycles established under Section 20-1301(e).
- (5) The [Planning Director](#) may require that applications or plans be revised before being placed on the agenda of the [Planning Commission](#) or City Commission if the [Planning Director](#) determines that:
 - (i) the application or plan contains one or more significant inaccuracies or omissions that hinder timely or competent evaluation of the plan's/application's compliance with Development Code standards;
 - (ii) the application contains multiple minor inaccuracies or omissions that hinder timely or competent evaluation of the plan's/application's compliance with Development Code standards;
 - (iii) the application or plan cannot be approved without a variance or some other change or modification that the decision-making body for that application or plan does not have the authority to make.
- (6) Applications that contain the aforementioned types of inaccuracies or that substantially fail to comply with Development Code standards shall be revised before they will be placed on agenda of the [Planning Commission](#) or City Commission.
- (7) Action or inaction by the [Planning Director](#) under this section may be appealed to the Board of Zoning Appeals.

(h) Continuation of Public Hearings

- (1) A public hearing for which proper notice was given may be continued by the Board of Zoning Appeals or [Planning Commission](#) to a later date without providing additional notice as long as the continuance is set for specified date and time and that date and time is announced at the time of the continuance.
- (2) If a public hearing is tabled or deferred by the Board of Zoning Appeals or [Planning Commission](#) for an indefinite period of time or postponed more than three (3) months from the date of the originally scheduled public hearing, new public notice shall be given, in accordance with the notice requirements of the respective procedure, before the rescheduled public hearing.

- (3) The applicant or [Landowner](#) who requests the postponement is responsible for paying the cost of re-notification per the adopted schedule of fees for publication, and payment of re-notification costs shall be made before the item is placed on the agenda.

(i) Action by Review Bodies

- (1) Review bodies may take any action that is consistent with:
 - (i) the regulations of this Article;
 - (ii) the City's adopted Development Policy;
 - (iii) any by-laws that may apply to the review body; and
 - (iv) the notice that was given.
- (2) The review body's action may include recommending approval of the application, recommending approval with modifications or conditions, or recommending disapproval of the application.
- (3) The review body may recommend conditions, modifications or amendments if the effect of the condition, modification or amendment is to allow a less intensive use or [Zoning District](#) than indicated in the application, reduce the impact of the development, or reduce the amount of land area included in the application.
- (4) The review body may recommend that the application be approved conditionally upon the execution of a development agreement acceptable to the City Attorney and/or compliance with the [Access Management Standards](#) and the Community Design Manual adopted by the City Commission from time to time.
- (5) Review bodies may not recommend a greater [Density](#) of development; a more intensive use or a more intensive [Zoning District](#) than was indicated in the public notice.
- (6) Review bodies are not required to recommend approval of the maximum [Density](#) or intensity of use allowed.

(j) Action by Decision-Making Bodies

- (1) Decision-making bodies may take any action that is consistent with:
 - (i) the regulations of this Article;
 - (ii) the City's adopted development policy;
 - (iii) any by-laws that may apply to the decision-making body; and
 - (iv) the notice that was given.
- (2) The decision-making body's action may include approving the application, approving the application with modifications or conditions, or denying the application. A denial of application may be accompanied with a remand to the review body, if any, for further consideration.

- (3) The decision-making body may impose conditions on the application or allow modifications or amendments if the effect of the condition, modification or amendment is to allow a less intensive use or [Zoning District](#) than indicated in the application or to reduce the impact of the development or to reduce the amount of land area included in the application.
- (4) The decision-making body may approve the application upon the condition that the applicant executes a development agreement acceptable to the City Attorney and/or compliance with the [Access Management](#) Standards and the Community Design Manual adopted by the City Commission from time to time.
- (5) Decision-making bodies may not approve a greater [Density](#) of development; a more intensive use or a more intensive [Zoning District](#) than was specified in the public notice.
- (6) Decision-making bodies are not required to approve the maximum [Density](#) or intensity of use allowed.

(k) Lesser Change Table

Pursuant to K.S.A. 12-757, the [Planning Commission](#) may adopt a “Lesser Change Table.” The Lesser Change Table is for the use of the [Planning Commission](#) in determining the hierarchy of [Zoning Districts](#) and for determining when public notification or re-notification is required. Such a table lists zoning classifications, by category, in ascending order from the least intense to the most intense. The [Planning Commission](#)’s Lesser Change Table shall identify only the hierarchy of [Zoning Districts](#) within each of the three categories of [Base Districts](#)—Residential, Commercial and Industrial. It is not intended to identify hierarchical arrangements among Districts in different categories. For example, the Lesser Change Table may classify the RS40 District as less intense than the RS20 District, but it may not classify (R) Residential Districts as less intense than (C) Commercial Districts, or vice-versa. The Lesser Change Table shall be filed with the [Planning Director](#).

(l) Burden of Proof or Persuasion

In all cases, the burden is on the applicant to show that an application complies with applicable review or approval criteria.

(m) Conditions of Approval

When the procedures of this Article allow review bodies to recommend or decision-making bodies to approve applications with conditions, the conditions shall relate to a situation created or aggravated by the proposed use or development. When conditions are imposed, an application will not be deemed approved until the applicant has complied with all of the conditions.

(n) Deferred Items

Once a staff report is included in a posted agenda packet, [Planning Commission](#) action is required to defer an item. If an application is requested for deferral from the next [Planning Commission](#) agenda prior to inclusion of a staff report in a posted agenda packet, the applicant may defer an item by submitting a written request to the [Planning Director](#). For [Deferred Items](#), the [Landowner](#) or applicant shall provide an updated property Ownership list from the County Clerk’s office for items that have been deferred from an agenda for 3 or more months. If deferred at the applicant or [Landowner](#)’s request, the cost of republication of legal notice in the newspaper shall be paid by the applicant or [Landowner](#). If an item is deferred by the [Planning Commission](#), no republication fee will be charged.

(o) Inactive Files

For **Inactive Files**, the **Planning Director** may notify the applicant and applicant's **Agent** in writing that a file has been closed when the file has been inactive for a period of time equal to or exceeding 12 months. Requests for action after a file has been declared inactive and the applicant has been notified require resubmittal as a new application. Review fees and cost of publication are required to be paid as part of the resubmittal.

(p) Inaction by Review/Decision-Making Bodies

- (1) When a review or decision-making body fails to take action on an application within any time limit that is specified in or under this Article (as with an application processing cycle), that inaction will be interpreted as a recommendation of approval or a decision to approve, respectively. The **Effective Date** of such a “non-action” approval or recommendation of approval will be the date that action was required to have occurred under the required time limit.
- (2) Time limits for action may be extended if the applicant gives written consent to the extension or the applicant submits a written request for a deferral and agrees in writing to an extension of the time for action.
- (3) When a review body fails to take action on an application within any time limit that is specified in this Article, the decision-making body is free to proceed with its own action on the matter without awaiting a recommendation.

(q) Notices

The notice provisions of this section apply except as otherwise expressly stated.

(1) Content**(i) Newspaper and Mailed Notice**

All Newspaper and Mailed Notices shall:

- a. indicate the date, time and place of the public hearing or date of action that is the subject of the notice;
- b. describe the property involved in the application by Street address or by general description;
- c. describe the nature, scope and purpose of the application or proposal; and
- d. indicate where additional information on the matter can be obtained.

(ii) Posted Notice

All Posted Notices shall:

- a. indicate the date, time and place of the public hearing or date of action that is the subject of the notice;
- b. state the language “**Development Activity** Proposed”, and
- c. indicate where additional information on the matter can be obtained.

(2) Newspaper Notice

When the provisions of this Development Code require that “Newspaper Notice” be provided, the City is responsible for ensuring that notice is published in the official newspaper of the City of Lawrence. The notice shall appear in the newspaper at least 20 days before the date of the public hearing.

(3) Mailed Notice

When the provisions of this Development Code require that “Mailed Notice” be provided:

(i) Owner Notice; Radius

The official responsible for accepting the application shall mail notice to the record Owner of the subject property and all Owners of property located within 400 feet of the subject property. If the subject property abuts the City limits, the area of notification shall be extended to at least 1,000 feet into the unincorporated area.

(ii) Notice to Registered Neighborhood Associations

The official responsible for accepting the application shall mail notice to any Registered Neighborhood Associations whose boundaries include or are contiguous to the subject property.

(iii) Ownership Information

The applicant is responsible for providing certified ownership information. Current ownership information shall be obtained from the Douglas County Clerk. Ownership information will be considered current if, at the time of submission, it is no more than 30 days old.

(iv) Timing of Notice

Required notices shall be deposited in the U.S. mail at least 20 days before the public hearing, meeting, or date of action that is the subject of the notice. When required notices have been properly addressed and deposited in the mail, failure of a party to receive such notice will not be grounds to invalidate any action taken.

(v) Mailing Fee Established

From time to time, in order to recover mailing and notification costs incurred by the City hereunder, the Governing Body may establish a reasonable mailing fee, which fee shall be paid by the applicant.

(4) Posted Notice

(i) When the provisions of this Development Code require that “Posted Notice” be provided, the applicant shall ensure that notice is posted on the subject property.

(ii) Posted notice shall be in the form of official signs provided by the City.

(iii) Posted notice shall be clearly visible to neighboring residents and passers-by from each Public Street bordering the subject property. At least one sign shall be posted on each Street Frontage. The Planning Director is authorized to require the posting of additional signs when deemed necessary for effective public notice, but not more than one sign per 300 feet of Street Frontage may be required.

- (iv) Posted notice shall remain in place for at least 20 days before the public hearing, meeting, or date of action that is the subject of the notice.
- (v) During the required notice period, the applicant shall periodically check the condition of the sign and shall replace it if it is no longer legible for any reason, whether through Act of God, vandalism, defect in installation or vegetative growth.
- (vi) For any application requiring posted notice, the applicant shall supplement the application with an affidavit of posting and notice no sooner than the date the sign is posted but no later than seven (7) days prior to the scheduled public hearing, meeting, or date of action that is the subject of the notice. Failure to make timely delivery of such affidavit to the [Planning Director](#) shall render the application incomplete and subject it to removal from the agenda on the hearing date, at the discretion of the [Planning Commission](#).
- (vii) The applicant shall remove notice signs required by this section within 10 days of the date that the decision-making body takes action or the date that the application is withdrawn. Failure to properly post or maintain such signs is grounds for deferral or denial of the application.
- (viii) For applications that do not abut [Public Streets](#), the [Planning Director](#) is authorized to approve an alternative form of posted notice that will be visible to passers-by.
- (ix) The public may submit written statements regarding a specific development proposal that, when the written statement is submitted by the published deadline for receiving public comment, will become a part of the official record in the planning department.
- (x) Parties affected by the actions of a decision making body have the right to appeal the action taken in accordance with the procedures set out in Article 13 of this Chapter.

(r) Written Findings

Unless otherwise specifically provided in this ordinance, written findings are not required for a final decision on any application. Provided, however, that any decision may be expressly made subject to the subsequent adoption of written findings and, in such cases, the decision shall not be considered final until such findings are adopted. Provided further, that where an appeal of any quasi-judicial decision has been filed in the District Court of Douglas County pursuant to K.S.A. 12-760 or K.S.A. 60-2101(d) in cases where written findings have not been adopted, written findings shall be adopted by the approving authority within 45 days of service of the appeal on the City and thereafter shall be certified to the District Court as part of the administrative record. The 45-day time period for adoption and certification of findings may be extended with the permission of the District Court.

(s) Where Ordinance Required

Adoption of an ordinance is required in the case of a zoning text amendment, rezoning and special use permit. In such instances, the decision approving the application shall not be deemed to be final until the ordinance has been published in an official City newspaper.

(t) Planning Director as Administrative Official

Except where otherwise specifically provided in the Development Code, the Planning Director shall be the administrative official charged with interpreting and enforcing the provisions of the Development Code.

20-1302 TEXT AMENDMENTS**(a) Initiation**

An amendment to the text of the Development Code may be initiated by the City Commission, the [Planning Commission](#), or, as to provisions affecting Urban Conservation Districts, by the [Historic Resources Commission](#); and adopted in accordance with the rules of that body. Applications for text amendments may also be initiated by private parties and shall be filed with the [Planning Director](#). The application shall be in writing and shall include the proposed text and the reasons for proposing the amendment. The [Planning Director](#) shall forward the application to the City Commission for review and consideration of initiating the amendment taking into consideration the need for the amendment. Any proposed amendment shall follow the process set forth in this section after initiation.

(b) Public Hearing Notice

Newspaper notice of the [Planning Commission](#)'s public hearing shall be provided in accordance with Section 20-1301(q).

(c) Staff Review/Report

The [Planning Director](#) will review each proposed text amendment in accordance with the review and decision-making criteria of subsection (f) of this Section and, if deemed necessary, distribute the proposed amendment to other agencies and reviewers. Based on the results of those reviews, the [Planning Director](#) will provide a report on the proposed amendment to the [Planning Commission](#) and City Commission.

(d) Planning Commission's Review/Recommendation

The [Planning Commission](#) shall hold a public hearing on the proposed text amendment, review the proposed text amendment in accordance with the review and decision-making criteria of subsection (f) of this Section and recommend in writing that the City Commission approve, approve with modifications or deny the proposed amendment. The [Planning Commission](#) is also authorized to forward the proposed amendment to the City Commission with no recommendation.

(e) City Commission Decision

After receiving the [Planning Commission](#)'s recommendation, the City Commission shall take one of the following actions on the proposed text amendment:

- (1) approve, approve with modifications, or deny; or
- (2) return the application to the [Planning Commission](#) for further consideration, together with a written explanation of the reasons for the City Commission's failure to approve or disapprove.
 - (i) The [Planning Commission](#), after considering the explanation by the City Commission, may resubmit its original recommendations with its reasons for doing so or submit a new or amended recommendation.

20-1303 ZONING MAP AMENDMENTS (REZONINGS)**(a) Initiation**

An amendment to the zoning map may be initiated by the City Commission, the [Planning Commission](#), or, as to Urban Conservation district, by the Historic Resource Commission; and adopted in accordance with the rules of that body. Applications for zoning map amendments initiated by the [Landowner](#) shall be filed with the [Planning Director](#). Any proposed amendment shall follow the process set forth in this section after initiation.

(b) Application Contents

- (1) An application for amendment shall be accompanied by a conceptual plan and data necessary to demonstrate that the proposed amendment is in general conformance with the [Comprehensive Plan](#) and that the public necessity and convenience; and general welfare require the adoption of the proposed amendment.
- (2) The application shall include a General Location Map, which shall show the location of the property in relation to at least one intersection of two streets shown as [Collector](#) or [Arterial Streets](#) on the City's Major Thoroughfares Map of the [Comprehensive Plan](#).
- (3) Each application for an amendment to the [Zoning Districts](#) map shall be accompanied by a certified list of all property [Owner](#) within the notification area. If such proposed amendment is not a general revision of the existing regulations and affects specific property, the property shall be designated by legal description or a general description sufficient to identify the property under consideration. In addition to a published notice, written notice of such proposed amendment shall be mailed at least 20 days before the hearing to all [Owners](#) of record of lands located within at least 400 feet of the area proposed to be altered for regulations of the city. If the city proposes a zoning amendment to property adjacent to the city's limits, the area of notification of the city's action shall be extended to at least 1,000 feet in the unincorporated area. All notices shall include a statement that a complete legal description is available for public inspection and shall indicate where such information is available.

(c) Public Hearing Notice

- (1) Newspaper, posted and mailed notice of the [Planning Commission's](#) public hearing shall be provided in accordance with Section 20-1301(q), except as noted in subsection (c)(2) of this Section. For purposes of K.S.A. §12-757, any [Zoning District](#) listed in the right-hand column of the Lesser Change Table that follows shall be considered a "lesser change" than a change to the [Zoning District](#) listed in the left-hand column of the same row of the table; in accordance with the cited section, a recommendation or action to amend the zoning map to assign the "lesser change" [Zoning District](#) to the land, rather than the [Zoning District](#) advertised in the notice, shall not require further notice. A recommendation or action to amend the Zoning Map to assign any [Zoning District](#) other than the one advertised in the notice or one included in the corresponding right-hand column of the Lesser Change Table will be inconsistent with the advertised hearing and shall require re-advertising and the holding of a new hearing, after proper notice. Such recommendation or action by the [Planning Commission](#) or the City

Commission shall be construed as an instruction to the [Planning Director](#) to set a new hearing and to give notice of the proposed hearing, including the new [Zoning District](#) in the notice.

Table of Lesser Changes	
Advertised/Proposed Zoning District	Districts to be Considered a “Lesser Change”
RS40	None
RS20	RS40
RS10	RS20 or RS40
RS7	RS10, RS-20 or RS40
RS5	Any other RS except RS3 or RSO
RS3	Any other RS except RSO
RSO	Any other RS except RS-3
RM12, RM12D	Any RS except RSO
RM15	RM12 or any RS except RSO
RM24	RM15, RM12 or any RS except RSO
RM32	Any RM or any RS
RMG	Any RM or any RS
RMO	RM15, RM12 or any RS
CN1	None
CN2	CN1, RSO or RMO
CD	CN1, CN2 or CC200
CC200	CN1 or CN2
CC400	CC200 or CN2
CC600	CC400, CC200 or CN2
CR	CC600, CC400 or CC200
CS	CN1, CN2 or CO
IBP	None
IL	IBP or CN2
IM	IBP or IL
IG	IL, IM, IBP, or CN2
Other Zoning Districts	Not Applicable

- (2) Applications for Urban Conservation Overlay District zoning amendments shall only require newspaper notice and mailed notice of the Planning Commission’s public hearing in accordance with Section 20-1301(q)(2) and (q)(3). Any posted notice under Section 20-1301(q)(4) shall be at the discretion of the Planning Director.

(d) Staff Review/Report

The [Planning Director](#) will review each proposed zoning map amendment in accordance with the review and decision-making criteria of Subsection (g) of this Section and, if deemed necessary, distribute the proposed amendment to other agencies and reviewers. Based on the results of those reviews, the [Planning Director](#) will provide a report on the proposed amendment to the [Planning Commission](#) and City Commission. The report will include documentation proof of posting and other required notice.

(e) Planning Commission's Review/Recommendation

The **Planning Commission** shall hold a public hearing on the proposed zoning map amendment, review the proposed amendment in accordance with the review and decision-making criteria of Subsection (g) of this Section and recommend that the City Commission approve, approve with modifications or deny the proposed amendment. The **Planning Commission** is also authorized to forward the proposed amendment to the City Commission with no recommendation.

(f) City Commission Decision

After receiving the **Planning Commission's** recommendation, the City Commission shall take one of the following actions on the proposed zoning map amendment:

- (1) approve, approve with conditions or modifications, or deny; or
- (2) return the application to the **Planning Commission** for further consideration, together with a written explanation of the reasons for the City Commission's failure to approve or disapprove.
 - (i) The **Planning Commission**, after considering the explanation by the City Commission, may resubmit its original recommendations with its reasons for doing so or submit a new or amended recommendation.
 - (ii) Upon the receipt of such recommendation, the City Commission may, by a simple majority vote, approve the proposed zoning map amendment, approve it with modifications, or deny it.
 - (iii) If the **Planning Commission** fails to deliver its recommendations to the City Commission following the **Planning Commission's** next regular meeting after receipt of the City Commission's report, the City Commission will consider such course of inaction on the part of the **Planning Commission** as a resubmission of the original recommendations and proceed accordingly.
- (3) The City Commission may act by a simple majority vote, except for the following cases:
 - (i) action that is contrary to the **Planning Commission's** recommendations, in which case the decision shall be by a 2/3 majority vote of the full membership of the City Commission; or
 - (ii) approval, or approval with modifications, when a valid protest petition has been submitted in accordance with subsection (h) of this Section, in which case a decision approving the application shall be effective only if supported by the votes of at least 3/4 of the members of the entire City Commission.
- (4) The City Commission shall:
 - (i) State the reasons for its decision on the minutes or official record; and
 - (ii) notify the applicant, and all other parties who have made a written request for notification, in writing of its decision and the reasons for its decision.

(g) Review and Decision-Making Criteria

In reviewing and making decisions on proposed zoning map amendments, review and decision-making bodies shall consider at least the following factors:

- (1) conformance with the [Comprehensive Plan](#);
- (2) zoning and use of nearby property, including any overlay zoning;
- (3) character of the neighborhood;
- (4) plans for the area or neighborhood, as reflected in adopted area and/or sector plans including the property or adjoining property;
- (5) suitability of the subject property for the uses to which it has been restricted under the existing zoning regulations;
- (6) length of time the subject property has remained vacant as zoned;
- (7) the extent to which approving the rezoning will detrimentally affect nearby properties;
- (8) the gain, if any, to the public health, safety and welfare due to denial of the application, as compared to the hardship imposed upon the [Landowner](#), if any, as a result of denial of the application; and
- (9) the recommendation of the City's professional staff.
- (10) For proposals that will create more than 100,000 square feet of retail space within the city: the impact of the proposed project on the retail market. Staff will provide an analysis based on the addition of the square footage to the retail market, vacancy rate trends, square footage per capita trends, and current demand trends, including but not limited to population, income, pull factors, and retail sales using the latest available city-wide retail market report.

(h) Protest Petitions

A valid protest petition opposing a zoning map amendment may be submitted to the City Clerk within 14 days of the conclusion of the [Planning Commission's](#) public hearing.

- (1) A protest petition will be considered "valid" if it is signed by the [Owner](#) of 20% or more of:
 - (i) any real property included in the proposed amendment; or
 - (ii) the total area of real property located within 200 feet of any real property included in the proposed amendment (or 1,000 feet into the unincorporated area when the real property included in the proposed amendment abuts the city limits), excluding streets and public ways.
- (2) In the case of joint Ownership, all [Owners](#) shall sign the petition.
- (3) For the purpose of determining the sufficiency of a protest petition, if the proposed rezoning was requested by the [Owner](#) of the specific property subject to the rezoning, or the [Owner](#) of the specific property subject to the rezoning does not oppose in writing such rezoning, such property shall be excluded when calculating the total real property within the area required to be notified.

(i) Date of Effect

The zoning map amendment will become effective upon publication of the adopting ordinance.

(j) Limitation on Successive Applications

- (1) Withdrawal of an original application after it has been advertised for public hearing shall constitute denial of the application as if the public hearing had been held and concluded;
- (2) A successive application shall not be accepted for a period of twelve (12) months from the date of City Commission denial of the original application unless a successive application is substantially different from the original application that was denied;
- (3) A successive application shall not be accepted until 120 days after the date of the City Commission denial and then will only be accepted if substantially different from the original application. The threshold for measuring substantially different shall be based on meeting one or more of the following criteria:
 - a. A different [Zoning District](#) category has been applied for;
 - b. The same [Zoning District](#) category has been applied for and the [Density](#) of use is at least 25% greater or less than the original petition;
 - c. The same [Zoning District](#) category has been applied for and the intensity of use is at least 25% greater or less than the original petition; or
 - d. Specific responses to the reasons for denial set forth in the findings of fact by the City Commission are, in the opinion of the [Planning Director](#), addressed in the resubmission.
- (4) A new rezoning application may be submitted after at least twelve (12) months from the date of City Commission denial.

(k) Appeals

Within 30 days of the City Commission's decision on the zoning map amendment, any person aggrieved by such decision may maintain an action in District Court to determine the reasonableness of the final decision.

(l) Plans

- (1) A plan shall be prepared and adopted prior to review of a petition for map amendment when:
 - (i) No water or sanitary sewer mains exist or are planned to serve the proposed site;
 - (ii) The request is not consistent with adopted plans; or,

- (iii) In-fill development is proposed and, at the discretion of the [Planning Commission](#), additional information is needed specific to unanswered questions or concerns related to transportation, compatibility of land use(s), or adequacy of transitions between established and proposed land uses.
- (2) Depending on the size or type of request, the plans to be prepared include:
 - (i) Watershed or Sub-basin Plan. This Plan will encompass an entire watershed or sub-basin.
 - (ii) Sector Plan. This Plan includes approximately one square mile.
 - (iii) Neighborhood Plan. This Plan encompasses a specific neighborhood.
 - (iv) Special Area Plan. This includes a Nodal Plan which plans for an area immediately surrounding an intersection. A Corridor Plan is a type of linear area plan that generally encompasses a roadway or specific feature.
 - (v) Specific Issue/District Plan. Deals with a specific issue or project that does not fall into any of the above listed categories.

NOTE: REVISED LANGUAGE IS HIGHLIGHTED YELLOW. NEW TEXT IS SHOWN IN RED. SOME AFFECTED SECTIONS ARE REFORMATTED FOR EASE OF READING.

ARTICLE 13 DEVELOPMENT REVIEW PROCEDURES

20-1301	General
20-1302	Text Amendments
20-1303	Zoning Map Amendments (Rezoning)
20-1304	Planned Developments
20-1305	Site Plan Review
20-1306	Special Uses
20-1307	Institutional Development Plan
20-1308	Floodplain Development Permit
20-1309	Zoning Variances
20-1310	Written Interpretations
20-1311	Appeals of Administrative Orders, Requirements, Decisions, or Determinations

20-1301 GENERAL

(a) Summary of Procedures

The following table provides a summary of the procedures in this Article. In the event of conflict between this summary table and the detailed procedures in this Development Code, the detailed procedures govern.

Procedure	Review and Decision-Making Bodies				Notice
	Staff	PC	BZA	CC	[2]
Text Amendments (§0)	R	<R>		DM	N
Zoning Map Amendments (§0) [3]	R	<R>		DM	N/P/M
Planned Developments (§ 20-1303(l)(2)(v))					
Preliminary Development Plan	R	<R>		DM	N/P/M
Final Development Plan	DM			<A>	M
Site Plan Review (§0)	DM			<A> [4]	P/M
Special Uses (§Article 12. 20-1305(o)(3))	R	<R>		DM	N/P/M
Zoning Variances (§0)	R		<DM>		N/M
Written Interpretations (§0)	DM		<A> [5]		
Appeals of Administrative Decisions (§0)			<DM>		N/M

Procedure	Review and Decision-Making Bodies				Notice
	Staff	PC	BZA	CC	[2]
PC = Planning Commission BZA = Board of Zoning Appeals CC = City Commission <=> Public Hearing Required					
[1] R = Review Body (Responsible for Review and Recommendation); DM = Decision-Making Body (Responsible for Final Decision to Approve or Deny); A = Authority to hear and decide appeals of Decision-Making Body's action.					
[2] Notices: N = Newspaper (published); P = Posted (signs); M = Mailed (See sub-section (q)(3) of this section)					
[3] See Section 20-308(d) for special procedures applicable to UC, Urban Conservation District zoning map amendments.					
[4] City Commission is authorized to hear and decide appeals of Planning Director 's decision on Site Plans.					
[5] Appeals processed as "Appeals of Administrative Decisions."					

(b) Authority to File Applications

Unless otherwise expressly stated, applications for review and approval under this article may be initiated by (1) all the [Owner](#) of the property that is the subject of the application; (2) the [Landowners'](#) authorized [Agent](#); or (3) any review or decision-making body.

(c) Form of Application

Applications required under this Development Code shall be submitted in a form and in such numbers as required by the official responsible for accepting the application. Officials responsible for accepting applications shall develop checklists of submittal requirements and make those checklists available to the public. Application forms and checklists of required submittal information are available in the office of the [Planning Director](#).

(d) Pre-application Meetings

- (1) All applicants for matters that require a public hearing are required to attend a pre-application meeting with staff. Pre-application meetings are also required whenever the provisions of this Article expressly state that they are required. Pre-application meetings shall be scheduled by the applicant to allow adequate time to review and respond to issues raised at the pre-application meeting. The meeting shall occur at least 7 [Working Days](#) before submitting an application.
- (2) All other applicants are encouraged to arrange a pre-application meeting with City staff. The [Planning Director](#) will provide assistance to applicants and ensure that appropriate City staff members are involved in pre-application meetings.

(e) Application Processing Cycles

The [Planning Director](#) may, after consulting with review and decision-making bodies, promulgate processing cycles for applications. Processing cycles may establish:

- (1) deadlines for receipt of complete applications;
- (2) dates of regular meetings;

- (3) the scheduling of staff reviews and staff reports on complete applications; and
- (4) any required time-frames for action by review and decision-making bodies.

(f) Application Filing Fees

Applications shall be accompanied by the fee amount that has been established by the City Commission. Fees are not required with applications initiated by review or decision-making bodies. Application fees are nonrefundable.

(g) Application Completeness, Accuracy and Sufficiency

- (1) An application will be considered complete and ready for processing only if it is submitted in the required number and form, includes all required information and is accompanied by the required filing fee.
- (2) Within 5 [Working Days](#) of application filing, the [Planning Director](#) shall determine whether the application includes all information required for processing (See Section 20-1301(c)). If an application does not include all of the required information it will be deemed incomplete. If an application includes all of the required information it will be deemed complete. If the application is deemed incomplete, written notice shall be provided to the applicant and the applicant's [Agent](#). The notice shall include an explanation of the application's deficiencies.
- (3) No further processing of incomplete applications will occur and incomplete applications will be pulled from the processing cycle. When the deficiencies are corrected, the application will be placed in the next processing cycle. If the deficiencies are not corrected by the applicant within 60 days, the application will be considered withdrawn. If an application is deemed withdrawn because of failure to correct application deficiencies, notice shall be sent to the applicant and the applicant's [Agent](#).
- (4) Applications deemed complete will be considered to be in the processing cycle and will be reviewed by staff and other review and decision-making bodies in accordance with the procedures of this Article and the processing cycles established under Section 20-1301(e).
- (5) The [Planning Director](#) may require that applications or plans be revised before being placed on the agenda of the [Planning Commission](#) or City Commission if the [Planning Director](#) determines that:
 - (i) the application or plan contains one or more significant inaccuracies or omissions that hinder timely or competent evaluation of the plan's/application's compliance with Development Code standards;
 - (ii) the application contains multiple minor inaccuracies or omissions that hinder timely or competent evaluation of the plan's/application's compliance with Development Code standards;
 - (iii) the application or plan cannot be approved without a variance or some other change or modification that the decision-making body for that application or plan does not have the authority to make.

- (6) Applications that contain the aforementioned types of inaccuracies or that substantially fail to comply with Development Code standards shall be revised before they will be placed on agenda of the [Planning Commission](#) or City Commission.
- (7) Action or inaction by the [Planning Director](#) under this section may be appealed to the Board of Zoning Appeals.

(h) Continuation of Public Hearings

- (1) A public hearing for which proper notice was given may be continued by the Board of Zoning Appeals or [Planning Commission](#) to a later date without providing additional notice as long as the continuance is set for specified date and time and that date and time is announced at the time of the continuance.
- (2) If a public hearing is tabled or deferred by the Board of Zoning Appeals or [Planning Commission](#) for an indefinite period of time or postponed more than three (3) months from the date of the originally scheduled public hearing, new public notice shall be given, in accordance with the notice requirements of the respective procedure, before the rescheduled public hearing.
- (3) The applicant or [Landowner](#) who requests the postponement is responsible for paying the cost of re-notification per the adopted schedule of fees for publication, and payment of re-notification costs shall be made before the item is placed on the agenda.

(i) Action by Review Bodies

- (1) Review bodies may take any action that is consistent with:
 - (i) the regulations of this Article;
 - (ii) the City's adopted Development Policy;
 - (iii) any by-laws that may apply to the review body; and
 - (iv) the notice that was given.

(2) The review body's action may include:

Section (2) changed from paragraph form. New text is not proposed.

- (i) recommending approval of the application,**
- (ii) recommending approval with modifications or conditions, or**
- (iii) recommending disapproval of the application.**

- (3) The review body may recommend conditions, modifications, or amendments if the effect of the condition, modification or amendment if ~~the effect of the of the condition, modification, or amendment:~~**

- (i) allows a less intensive use or [Zoning District](#) than indicated in the application **per Section 20-1301 (k) Lesser Change Table,****

Sections (4) and (5)
changed from paragraph
form.

- (ii) reduces the impact of the development as a means to mitigate potential adverse impacts that could be expected to occur without such conditions and may include restricting or limiting uses,
 - (iii) limits or restricts uses permitted in a base zoning district; or
 - (iv) reduces the amount of land area included in the application.
 - (4) The review body may recommend that the application be approved conditionally:
 - (i) upon the execution of a development agreement acceptable to the City Attorney and/or,
 - (ii) compliance with the Access Management Standards and/or;
 - (iii) the Community Design Manual adopted by the City Commission from time to time.
 - (5) Review bodies may not:
 - (i) recommend a greater Density of development; or
 - (ii) a more intensive use or a more intensive Zoning District than was indicated in the public notice.
 - (6) Review bodies are not required to recommend approval of the maximum Density or intensity of use allowed.
- (j) **Action by Decision-Making Bodies**
- (1) Decision-making bodies may take any action that is consistent with:
 - (i) the regulations of this Article;
 - (ii) the City's adopted development policy;
 - (iii) any by-laws that may apply to the decision-making body; and
 - (iv) the notice that was given.
 - (2) The decision-making body's action may include:
 - (i) approving the application,
 - (ii) approving the application with modifications or conditions, or
 - (iii) denying the application.
 - (iv) remanding to the review body, if any, for further consideration.
 - (3) The decision-making body may impose conditions on the application or allow modifications or amendments if the effect of the condition, modification or amendment:

- (i) is to allow a less intensive use or Zoning District than indicated in the application per Section 20-1301 (k) Lesser Change Table; or
 - (ii) to reduce the impact of the development as a means to mitigate potential adverse impacts that could be expected to occur without such conditions and may include restricting or limiting uses;
 - (iii) limit or restrict uses permitted in a base zoning district; or
 - (iv) is to reduce the amount of land area included in the application.
- (4) The decision-making body may approve the application upon the condition that:
 - (i) the applicant executes a development agreement acceptable to the City Attorney; and/or
 - (ii) the application is in compliance with the Access Management Standards; and/or
 - (iii) the application is in compliance with the Community Design Manual adopted by the City Commission from time to time.
- (5) Decision-making bodies may not:
 - (i) approve a greater Density of development;
 - (ii) a more intensive use; or
 - (iii) a more intensive Zoning District than was specified in the public notice.
- (6) Decision-making bodies are not required to approve the maximum Density or intensity of use allowed.

(k) Lesser Change Table

Pursuant to K.S.A. 12-757, the Planning Commission may adopt a “Lesser Change Table.” The Lesser Change Table is for the use of the Planning Commission in determining the hierarchy of Zoning Districts and for determining when public notification or re-notification is required. Such a table lists zoning classifications, by category, in ascending order from the least intense to the most intense. The Planning Commission’s Lesser Change Table shall identify only the hierarchy of Zoning Districts within each of the three categories of Base Districts—Residential, Commercial and Industrial. It is not intended to identify hierarchical arrangements among Districts in different categories. For example, the Lesser Change Table may classify the RS40 District as less intense than the RS20 District, but it may not classify (R) Residential Districts as less intense than (C) Commercial Districts, or vice-versa. The Lesser Change Table shall be filed with the Planning Director.

(l) Burden of Proof or Persuasion

In all cases, the burden is on the applicant to show that an application complies with applicable review or approval criteria.

(m) Conditions of Approval

When the procedures of this Article allow review bodies to recommend or decision-making bodies to approve applications with conditions, the conditions shall relate to a

situation created or aggravated by the proposed use or development. When conditions are imposed, an application will not be deemed approved until the applicant has complied with all of the conditions.

(n) Deferred Items

Once a staff report is included in a posted agenda packet, [Planning Commission](#) action is required to defer an item. If an application is requested for deferral from the next [Planning Commission](#) agenda prior to inclusion of a staff report in a posted agenda packet, the applicant may defer an item by submitting a written request to the [Planning Director](#). For [Deferred Items](#), the [Landowner](#) or applicant shall provide an updated property Ownership list from the County Clerk's office for items that have been deferred from an agenda for 3 or more months. If deferred at the applicant or [Landowner's](#) request, the cost of republication of legal notice in the newspaper shall be paid by the applicant or [Landowner](#). If an item is deferred by the [Planning Commission](#), no republication fee will be charged.

(o) Inactive Files

For [Inactive Files](#), the [Planning Director](#) may notify the applicant and applicant's [Agent](#) in writing that a file has been closed when the file has been inactive for a period of time equal to or exceeding 12 months. Requests for action after a file has been declared inactive and the applicant has been notified require resubmittal as a new application. Review fees and cost of publication are required to be paid as part of the resubmittal.

(p) Inaction by Review/Decision-Making Bodies

- (1)** When a review or decision-making body fails to take action on an application within any time limit that is specified in or under this Article (as with an application processing cycle), that inaction will be interpreted as a recommendation of approval or a decision to approve, respectively. The [Effective Date](#) of such a “non-action” approval or recommendation of approval will be the date that action was required to have occurred under the required time limit.
- (2)** Time limits for action may be extended if the applicant gives written consent to the extension or the applicant submits a written request for a deferral and agrees in writing to an extension of the time for action.
- (3)** When a review body fails to take action on an application within any time limit that is specified in this Article, the decision-making body is free to proceed with its own action on the matter without awaiting a recommendation.

(q) Notices

The notice provisions of this section apply except as otherwise expressly stated.

(1) Content

(i) Newspaper and Mailed Notice

All Newspaper and Mailed Notices shall:

- a.** indicate the date, time and place of the public hearing or date of action that is the subject of the notice;
- b.** describe the property involved in the application by Street address or by general description;

- c. describe the nature, scope and purpose of the application or proposal; and
- d. indicate where additional information on the matter can be obtained.

(ii) Posted Notice

All Posted Notices shall:

- a. indicate the date, time and place of the public hearing or date of action that is the subject of the notice;
- b. state the language “[Development Activity](#) Proposed”, and
- c. indicate where additional information on the matter can be obtained.

(2) Newspaper Notice

When the provisions of this Development Code require that “Newspaper Notice” be provided, the City is responsible for ensuring that notice is published in the official newspaper of the City of Lawrence. The notice shall appear in the newspaper at least 20 days before the date of the public hearing.

(3) Mailed Notice

When the provisions of this Development Code require that “Mailed Notice” be provided:

(i) [Owner Notice](#); Radius

The official responsible for accepting the application shall mail notice to the record [Owner](#) of the subject property and all Owners of property located within 400 feet of the subject property. If the subject property abuts the City limits, the area of notification shall be extended to at least 1,000 feet into the unincorporated area.

(ii) Notice to [Registered Neighborhood Associations](#)

The official responsible for accepting the application shall mail notice to any [Registered Neighborhood Association](#)s whose boundaries include or are contiguous to the subject property.

(iii) Ownership Information

The applicant is responsible for providing certified ownership information. Current ownership information shall be obtained from the Douglas County Clerk. Ownership information will be considered current if, at the time of submission, it is no more than 30 days old.

(iv) Timing of Notice

Required notices shall be deposited in the U.S. mail at least 20 days before the public hearing, meeting, or date of action that is the subject of the notice. When required notices have been properly addressed and deposited in the mail, failure of a party to receive such notice will not be grounds to invalidate any action taken.

(v) Mailing Fee Established

From time to time, in order to recover mailing and notification costs incurred by the City hereunder, the Governing Body may establish a reasonable mailing fee, which fee shall be paid by the applicant.

(4) Posted Notice

- (i) When the provisions of this Development Code require that “Posted Notice” be provided, the applicant shall ensure that notice is posted on the subject property.
- (ii) Posted notice shall be in the form of official signs provided by the City.
- (iii) Posted notice shall be clearly visible to neighboring residents and passers-by from each [Public Street](#) bordering the subject property. At least one sign shall be posted on each Street [Frontage](#). The [Planning Director](#) is authorized to require the posting of additional signs when deemed necessary for effective public notice, but not more than one sign per 300 feet of Street [Frontage](#) may be required.
- (iv) Posted notice shall remain in place for at least 20 days before the public hearing, meeting, or date of action that is the subject of the notice.
- (v) During the required notice period, the applicant shall periodically check the condition of the sign and shall replace it if it is no longer legible for any reason, whether through Act of God, vandalism, defect in installation or vegetative growth.
- (vi) For any application requiring posted notice, the applicant shall supplement the application with an affidavit of posting and notice no sooner than the date the sign is posted but no later than seven (7) days prior to the scheduled public hearing, meeting, or date of action that is the subject of the notice. Failure to make timely delivery of such affidavit to the [Planning Director](#) shall render the application incomplete and subject it to removal from the agenda on the hearing date, at the discretion of the [Planning Commission](#).
- (vii) The applicant shall remove notice signs required by this section within 10 days of the date that the decision-making body takes action or the date that the application is withdrawn. Failure to properly post or maintain such signs is grounds for deferral or denial of the application.
- (viii) For applications that do not abut [Public Streets](#), the [Planning Director](#) is authorized to approve an alternative form of posted notice that will be visible to passers-by.
- (ix) The public may submit written statements regarding a specific development proposal that, when the written statement is submitted by the published deadline for receiving public comment, will become a part of the official record in the planning department.
- (x) Parties affected by the actions of a decision making body have the right to appeal the action taken in accordance with the procedures set out in Article 13 of this Chapter.

(r) Written Findings

Unless otherwise specifically provided in this ordinance, written findings are not required for a final decision on any application. Provided, however, that any decision may be expressly made subject to the subsequent adoption of written findings and, in such cases, the decision shall not be considered final until such findings are adopted. Provided further, that where an appeal of any quasi-judicial decision has been filed in the District Court of Douglas County pursuant to K.S.A. 12-760 or K.S.A. 60-2101(d) in cases where written findings have not been adopted, written findings shall be adopted by the approving authority within 45 days of service of the appeal on the City and thereafter shall be certified to the District Court as part of the administrative record. The 45-day time period for adoption and certification of findings may be extended with the permission of the District Court.

(s) Where Ordinance Required

Adoption of an ordinance is required in the case of a zoning text amendment, rezoning and special use permit. In such instances, the decision approving the application shall not be deemed to be final until the ordinance has been published in an official City newspaper.

(t) Planning Director as Administrative Official

Except where otherwise specifically provided in the Development Code, the Planning Director shall be the administrative official charged with interpreting and enforcing the provisions of the Development Code.

20-1302 TEXT AMENDMENTS**20-1303 ZONING MAP AMENDMENTS (REZONINGS)****(a) Initiation**

An amendment to the zoning map may be initiated by the City Commission, the [Planning Commission](#), or, as to Urban Conservation district, by the Historic Resource Commission; and adopted in accordance with the rules of that body. Applications for zoning map amendments initiated by the [Landowner](#) shall be filed with the [Planning Director](#). Any proposed amendment shall follow the process set forth in this section after initiation.

(b) Application Contents

- (1) An application for amendment shall be accompanied by a conceptual plan and data necessary to demonstrate that the proposed amendment is in general conformance with the [Comprehensive Plan](#) and that the public necessity and convenience; and general welfare require the adoption of the proposed amendment.
- (2) The application shall include a General Location Map, which shall show the location of the property in relation to at least one intersection of two streets shown as [Collector](#) or [Arterial Streets](#) on the City's Major Thoroughfares Map of the [Comprehensive Plan](#).
- (3) Each application for an amendment to the [Zoning Districts](#) map shall be accompanied by a certified list of all property [Owner](#) within the notification area. If such proposed amendment is not a general revision of the existing regulations and affects specific property, the property shall be designated by legal description or a general description sufficient to identify the property under consideration. In addition to a published notice, written notice of such proposed amendment shall be mailed at least 20 days before the hearing to all [Owners](#) of record of lands located within at least 400 feet of the area proposed to be altered for regulations of the city. If the city proposes a zoning amendment to property adjacent to the city's limits, the area of notification of the city's action shall be extended to at least 1,000 feet in the unincorporated area. All notices shall include a statement that a complete legal description is available for public inspection and shall indicate where such information is available.

(c) Public Hearing Notice

- (1) Newspaper, posted and mailed notice of the [Planning Commission's](#) public hearing shall be provided in accordance with Section 20-1301(q), except as noted in subsection (c)(2) of this Section. For purposes of K.S.A. §12-757, any [Zoning District](#) listed in the right-hand column of the Lesser Change Table that follows shall be considered a "lesser change" than a change to the [Zoning District](#) listed in the left-hand column of the same row of the table; in accordance with the cited section, a recommendation or action to amend the zoning map to assign the "lesser change" [Zoning District](#) to the land, rather than the [Zoning District](#) advertised in the notice, shall not require further notice. A recommendation or action to amend the Zoning Map to assign any [Zoning District](#) other than the one advertised in the notice or one included in the corresponding right-hand column of the Lesser Change Table will be inconsistent with the advertised hearing and shall require

re-advertising and the holding of a new hearing, after proper notice. Such recommendation or action by the [Planning Commission](#) or the City Commission shall be construed as an instruction to the [Planning Director](#) to set a new hearing and to give notice of the proposed hearing, including the new [Zoning District](#) in the notice.

Table of Lesser Changes	
Advertised/Proposed Zoning District	Districts to be Considered a “Lesser Change”
RS40	None
RS20	RS40
RS10	RS20 or RS40
RS7	RS10, RS-20 or RS40
RS5	Any other RS except RS3 or RSO
RS3	Any other RS except RSO
RSO	Any other RS except RS-3
RM12, RM12D	Any RS except RSO
RM15	RM12 or any RS except RSO
RM24	RM15, RM12 or any RS except RSO
RM32	Any RM or any RS
RMG	Any RM or any RS
RMO	RM15, RM12 or any RS
CN1	None
CN2	CN1, RSO or RMO
CD	CN1, CN2 or CC200
CC200	CN1 or CN2
CC400	CC200 or CN2
CC600	CC400, CC200 or CN2
CR	CC600, CC400 or CC200
CS	CN1, CN2 or CO
IBP	None
IL	IBP or CN2
IM	IBP or IL
IG	IL, IM, IBP, or CN2
Other Zoning Districts	Not Applicable

- (2) Applications for Urban Conservation Overlay District zoning amendments shall only require newspaper notice and mailed notice of the Planning Commission's public hearing in accordance with Section 20-1301(q)(2) and (q)(3). Any posted notice under Section 20-1301(q)(4) shall be at the discretion of the Planning Director.

(d) Staff Review/Report

The [Planning Director](#) will review each proposed zoning map amendment in accordance with the review and decision-making criteria of Subsection (g) of this

Section and, if deemed necessary, distribute the proposed amendment to other agencies and reviewers. Based on the results of those reviews, the **Planning Director** will provide a report on the proposed amendment to the **Planning Commission** and City Commission. The report will include documentation proof of posting and other required notice.

(e) Planning Commission’s Review/Recommendation

The **Planning Commission** shall hold a public hearing on the proposed zoning map amendment, review the proposed amendment in accordance with the review and decision-making criteria of Subsection (g) of this Section and shall recommend on one of the following actions on the proposed zoning map amendment:

- (1) approve,
- (2) approve with conditions or modifications, or
- (3) deny the proposed amendment.

The **Planning Commission** is also authorized to forward the proposed amendment to the City Commission with no recommendation.

(f) Conditional Zoning

Conditional zoning may be considered when a clear and specific goal, policy, or recommendation of an approved area, neighborhood, nodal plan, or the Comprehensive Plan is identified, or when a clear harm or aggravation of a negative externality to the surrounding area can be mitigated by a condition.

Conditional Zoning: The attachment of special conditions or restrictions to a rezoning. Conditions can include restrictions as to use, size, design, density or intensity, and development timing as a means to mitigate potential adverse impacts that could be expected to occur without imposing such conditions.

Conditions imposed on a rezoning are intended to mitigate adverse impacts that can be reasonably expected to occur without imposing such conditions on an otherwise appropriate land use development project. Conditions must be reasonable and further the public health, safety, and welfare of the community and consistent with the community’s comprehensive plan, or furthers an articulated community or neighborhood goal or policy expressed in an adopted area, neighborhood or nodal plan or the Comprehensive Plan.

- (1) When the procedures of this Article allow review bodies to recommend or decision-making bodies to approve applications with conditions, the conditions shall relate to a situation created or aggravated by the proposed use or development.

(2) Types of conditions

Conditions must meet and/or exceed established minimum design standards.

- (i) Restrict or prohibit uses permitted by right or allowed by a Special Use Permit in a Base District.
- (ii) Enhance written notice requirements of proposed changes to surrounding property owners for future development, or to require governing body approval of a

Note: Introductory paragraph does not include State legislative reference. Per Randy it is not applicable because of Home Rule.

development application that would otherwise be subject to administrative approval.

(iii) Limit or restrict density when permitted by right in the Base District, but shall not permit residential uses in any district that otherwise prohibits residential uses.

(iv) Restrict intensity of development to include, but not limited to, Article 6 Density and Dimensional Standards and Article 5 Use Regulations.

(g) City Commission Decision

After receiving the [Planning Commission](#)'s recommendation, the City Commission shall take one of the following actions on the proposed zoning map amendment:

- (1) approve, approve with conditions or modifications, or deny; or
- (2) return the application to the [Planning Commission](#) for further consideration, together with a written explanation of the reasons for the City Commission's failure to approve or disapprove.
 - (i) The [Planning Commission](#), after considering the explanation by the City Commission, may resubmit its original recommendations with its reasons for doing so or submit a new or amended recommendation.
 - (ii) Upon the receipt of such recommendation, the City Commission may, by a simple majority vote, approve the proposed zoning map amendment, approve it with modifications, or deny it.
 - (iii) If the [Planning Commission](#) fails to deliver its recommendations to the City Commission following the [Planning Commission](#)'s next regular meeting after receipt of the City Commission's report, the City Commission will consider such course of inaction on the part of the [Planning Commission](#) as a resubmission of the original recommendations and proceed accordingly.
- (3) The City Commission may act by a simple majority vote, except for the following cases:
 - (i) action that is contrary to the [Planning Commission](#)'s recommendations, in which case the decision shall be by a 2/3 majority vote of the full membership of the City Commission; or
 - (ii) approval, or approval with modifications, when a valid protest petition has been submitted in accordance with subsection (h) of this Section, in which case a decision approving the application shall be effective only if supported by the votes of at least 3/4 of the members of the entire City Commission.
- (4) The City Commission shall:
 - (i) State the reasons for its decision on the minutes or official record; and

- (ii) notify the applicant, and all other parties who have made a written request for notification, in writing of its decision and the reasons for its decision.

(h) Review and Decision-Making Criteria

In reviewing and making decisions on proposed zoning map amendments, review and decision-making bodies shall consider at least the following factors:

- (1) conformance with the [Comprehensive Plan](#);
- (2) zoning and use of nearby property, including any overlay zoning;
- (3) character of the neighborhood;
- (4) plans for the area or neighborhood, as reflected in adopted area and/or sector plans including the property or adjoining property;
- (5) suitability of the subject property for the uses to which it has been restricted under the existing zoning regulations;
- (6) length of time the subject property has remained vacant as zoned;
- (7) the extent to which approving the rezoning will detrimentally affect nearby properties;
- (8) the gain, if any, to the public health, safety and welfare due to denial of the application, as compared to the hardship imposed upon the [Landowner](#), if any, as a result of denial of the application; and
- (9) the recommendation of the City's professional staff.
- (10) For proposals that will create more than 100,000 square feet of retail space within the city: the impact of the proposed project on the retail market. Staff will provide an analysis based on the addition of the square footage to the retail market, vacancy rate trends, square footage per capita trends, and current demand trends, including but not limited to population, income, pull factors, and retail sales using the latest available city-wide retail market report.
- (11) The reasonableness of conditions as determined by the following criteria:
 - (i) Conditions shall facilitate a clear and specific goal, policy, or recommendation of an approved area, neighborhood, nodal plan, or the Comprehensive Plan.
 - (ii) Conditions may be applied when a clear harm to the surrounding area or a negative externality aggravated or created by the use.
 - (iii) The Lesser Change Table cannot be employed to achieve the same result as a proposed conditional zoning.
 - (iv) Conditions may not allow an increase in intensity when otherwise restricted by the Base District.

(i) Protest Petitions

A valid protest petition opposing a zoning map amendment may be submitted to the City Clerk within 14 days of the conclusion of the [Planning Commission](#)'s public hearing.

- (1) A protest petition will be considered "valid" if it is signed by the [Owner](#) of 20% or more of:
 - (i) any real property included in the proposed amendment; or
 - (ii) the total area of real property located within 200 feet of any real property included in the proposed amendment (or 1,000 feet into the unincorporated area when the real property included in the proposed amendment abuts the city limits), excluding streets and public ways.
- (2) In the case of joint Ownership, all [Owners](#) shall sign the petition.
- (3) For the purpose of determining the sufficiency of a protest petition, if the proposed rezoning was requested by the [Owner](#) of the specific property subject to the rezoning, or the [Owner](#) of the specific property subject to the rezoning does not oppose in writing such rezoning, such property shall be excluded when calculating the total real property within the area required to be notified.

(j) Date of Effect

The zoning map amendment will become effective upon publication of the adopting ordinance.

(k) Limitation on Successive Applications

- (1) Withdrawal of an original application after it has been advertised for public hearing shall constitute denial of the application as if the public hearing had been held and concluded;
- (2) A successive application shall not be accepted for a period of twelve (12) months from the date of City Commission denial of the original application unless a successive application is substantially different from the original application that was denied;
- (3) A successive application shall not be accepted until 120 days after the date of the City Commission denial and then will only be accepted if substantially different from the original application. The threshold for measuring substantially different shall be based on meeting one or more of the following criteria:
 - a. A different [Zoning District](#) category has been applied for;
 - b. The same [Zoning District](#) category has been applied for and the [Density](#) of use is at least 25% greater or less than the original petition;
 - c. The same [Zoning District](#) category has been applied for and the intensity of use is at least 25% greater or less than the original petition; or

- d. Specific responses to the reasons for denial set forth in the findings of fact by the City Commission are, in the opinion of the [Planning Director](#), addressed in the resubmission.

- (4) A new rezoning application may be submitted after at least twelve (12) months from the date of City Commission denial.

(l) Appeals

Within 30 days of the City Commission's decision on the zoning map amendment, any person aggrieved by such decision may maintain an action in District Court to determine the reasonableness of the final decision.

(m) Plans

- (1) A plan shall be prepared and adopted prior to review of a petition for map amendment when:
 - (i) No water or sanitary sewer mains exist or are planned to serve the proposed site;
 - (ii) The request is not consistent with adopted plans; or,
 - (iii) In-fill development is proposed and, at the discretion of the [Planning Commission](#), additional information is needed specific to unanswered questions or concerns related to transportation, compatibility of land use(s), or adequacy of transitions between established and proposed land uses.
- (2) Depending on the size or type of request, the plans to be prepared include:
 - (i) Watershed or Sub-basin Plan. This Plan will encompass an entire watershed or sub-basin.
 - (ii) Sector Plan. This Plan includes approximately one square mile.
 - (iii) Neighborhood Plan. This Plan encompasses a specific neighborhood.
 - (iv) Special Area Plan. This includes a Nodal Plan which plans for an area immediately surrounding an intersection. A Corridor Plan is a type of linear area plan that generally encompasses a roadway or specific feature.
 - (v) Specific Issue/District Plan. Deals with a specific issue or project that does not fall into any of the above listed categories.

PLANNING COMMISSION REPORT
Regular Agenda - Public Hearing Item

PC Staff Report
02/21/2018

ITEM NO. 5 IBP TO IL; 1900 WAKARUSA DRIVE (KEW)

Z-18-00495: Consider rezoning approximately 2.7 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, located at 1900 Wakarusa Dr. Submitted by Home Sweet Home on behalf of Larry McElwain, property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the request to rezone approximately 2.7 acres from IBP (Industrial/Business Park) District to IL (Limited Industrial) District, with use restrictions, and forwarding it to the City Commission with a recommendation for approval based on the findings of fact found in the body of the staff report, subject to the following condition: Permitted uses are limited to those listed below:

- | | |
|-----------------------------------------------------------|--------------------------------------------------------|
| <i>a. College/University</i> | <i>cc. Business Equipment</i> |
| <i>b. Day Care Center</i> | <i>dd. Business Support</i> |
| <i>c. Event Center, Small</i> | <i>ee. Maker Space, Limited</i> |
| <i>d. Event Center, Large</i> | <i>ff. Maker Space, Intensive</i> |
| <i>e. Postal & Parcel Service</i> | <i>gg. Manufacturing & Production, Ltd.</i> |
| <i>f. Public Safety</i> | <i>hh. Manufacturing & Production, Tech.</i> |
| <i>g. Active Funeral and Interment</i> | <i>ii. Research Service</i> |
| <i>h. Temporary Shelter (Special Use only)</i> | <i>jj. Exterior Storage (Accessory Use only)</i> |
| <i>i. Social Service Agency</i> | <i>kk. Wholesale Storage & Distribution, Light</i> |
| <i>j. Community Meal Program (Special Use only)</i> | <i>ll. Mini-warehouse</i> |
| <i>k. Utilities, Minor (Special Use only)</i> | <i>mm. Agriculture, Crop</i> |
| <i>l. Utilities and Service, major (Special Use only)</i> | <i>nn. Agricultural, small animal (Accessory Use</i> |
| <i>m. Health Care Office, Health Care Clinic</i> | <i>Only)</i> |
| <i>n. Active Recreation</i> | <i>oo. Farmer's Market</i> |
| <i>o. Participant Sports & Recreation, Indoor</i> | <i>pp. On-site agricultural sales (Accessory Use</i> |
| <i>p. Participant Sports & Recreation, Outdoor</i> | <i>only)</i> |
| <i>q. Passive Recreation</i> | <i>qq. Amateur & Receive-Only Antennas</i> |
| <i>r. Nature Preserve/Undeveloped</i> | <i>(Accessory Use Only)</i> |
| <i>s. Veterinary</i> | <i>rr. Broadcasting Tower</i> |
| <i>t. Kennel</i> | <i>ss. Communications Service Establishment</i> |
| <i>u. Sales & Grooming</i> | <i>tt. Wireless Facility - Antenna (Accessory Use</i> |
| <i>v. Accessory Bar (Accessory Use only)</i> | <i>only)</i> |
| <i>w. Restaurant, Quality</i> | <i>uu. Wireless Support Structure (Special Use</i> |
| <i>x. Administrative and Professional (Office)</i> | <i>only)</i> |
| <i>y. Financial, Insurance & Real Estate (Office)</i> | <i>vv. Satellite dish (Accessory Use only)</i> |
| <i>z. Payday Advance, Car Title Loan Business</i> | <i>ww. Recycling Facilities, Small Collection</i> |
| <i>aa. Office, Other</i> | |
| <i>bb. Parking Facility, Commercial</i> | |

Conditional zoning was initiated at the applicant's request.

Reason for Request: *"The current zoning of IBP includes health care office/health care clinic, but does not include the desired uses of kennel and sales & grooming. These desired uses are permitted in IL zoning."*

KEY POINTS <ul style="list-style-type: none">• Property is included in an existing Industrial Business Park zoning district.• District is described in Chapter 7 of <i>Horizon 2020</i> as part of the industrial inventory.• Proposed use is not permitted in the IBP District.• Applicant proposes restricted zoning to maintain IBP character but permit specific uses.
ASSOCIATED CASES/OTHER ACTION REQUIRED <ul style="list-style-type: none">• Final Plat Oread West No. 14.• SP-10-59-93; Site Plan for Carrousel Printing (not developed)• Submission of a site plan for lot development.
PLANS AND STUDIES REQUIRED <ul style="list-style-type: none">• <i>Traffic Study</i> – Not required for rezoning• <i>Downstream Sanitary Sewer Analysis</i> – Not required for rezoning• <i>Drainage Study</i> – Not required for rezoning• <i>Retail Market Study</i> – Not applicable to this request
ATTACHMENTS <ol style="list-style-type: none">1. Area map2. Concept Development3. Neighborhood Map4. Use Table5. Land Use Map
PUBLIC COMMENT RECEIVED PRIOR TO PRINTING <ul style="list-style-type: none">• No communications or inquiries have been received for this rezoning request.

Project Summary:

This application is a pre-development request to change the base zoning district to permit *Kennel and Sales & Grooming* on a specific parcel of land in the Oread West Research Park. The applicant proposes to condition the zoning and restrict uses to those permitted in the IBP District (as it currently exists) and add the *Kennel and Sales & Grooming* use to the list of permitted uses.

1. CONFORMANCE WITH THE COMPREHENSIVE PLAN

Applicant's Response: *"The project creates an infill development project on existing commercially zoned land. It creates the opportunity for an increase in property tax revenue, while providing the opportunity for the expansion of a local business."*

Industrial and Employment-Related Land Uses are discussed in detail in Chapter 7 of *Horizon 2020*. Specific strategies are listed in Chapter 7 and include increasing the number and diversity of jobs available, protect, enhance and retain existing industrial-related land use areas, continue to address the needs of existing businesses and industries to ensure their retention in the community and to help facilitate expansion plans of those businesses and industries for the future.

Horizon 2020 divides this land use into two groups. See Table 1. The community includes both existing and future areas designated for these land uses described in Chapter 7 in detail. *Horizon 2020* identifies the Oread West Research Park as an existing (Employment Related Area) (Page 7-10).

Table 1 INDUSTRIAL LAND USE CATEGORIES (HORIZON 2020)

	Industrial Land Use	Employment Related Land Use
Land Use Categories	<ul style="list-style-type: none">• Warehouse and Distribution• Industrial• Work-live Campus-type Center• Industrial/Business/Research Park	<ul style="list-style-type: none">• Office• Office Research• Work-live Campus-type Center• Industrial/Business/Research park

Applicable policies in *Horizon 2020* are discussed below, with staff comments.

Industrial and Employment-Related Land Use (Chapter 7, *Horizon 2020*)

Goal 1: Development in Established Industrial and Employment-Related Areas

Policy 1.1 Retain Established Development and Encourage New Development in Existing Industrial and Employment Related Areas.

This policy discusses the need to market and develop existing areas designated for industrial and employment related uses. Approval of this request will facilitate development of vacant land within an existing industrial area.

Policy 1.2: Ensure Compatibility of Development

“Encourage best management practices for site planning and design that include, but are not limited to, building placement and orientation, vehicular and pedestrian circulation patterns, open space, landscaping, lighting, stormwater management, and interfacings with adjacent neighborhoods and development, and appropriate accommodation of the design to the site's natural features.”

Staff will review the submitted site plan for compliance with the standards of the Land Development Code, which will include compatibility with the arrangement of buildings, off-street parking, lighting, landscaping, pedestrian walkways and sidewalks, ingress and egress, and stormwater drainage of the surrounding area. Staff will also review a future development application for conformance with the Community Design Manual – Industrial Design Standards. The review will ensure that, by meeting the Land Development Code standards, the development is compatible with the surrounding area.

This application includes a concept plan showing the location of the building with customer parking, rear access and parking, and a shared driveway.

Policy 1.3: Concentrate Industrial and Employment-Related Development

“Maintain an appropriate supply of industrially zoned land so that site choices are available and infrastructure expansion can occur in an efficient and orderly manner.”

The rezoning request maintains the industrially zoned land in the Oread West Research Park in a different form. The restrictions associated with the rezoning align the proposed IL zoning to the surrounding IBP zoning of the research park. The uses permitted in the IBP District would be maintained with this rezoning, but the applicant has proposed to restrict all other IL uses except for *Kennel and Sales & Grooming*.

Goals 2, 3, and 4

The goals and policies included in the rest of Chapter 4 address criteria for the location of new industrial and employment-related development, compatible land use transitions, and

transportation considerations. Only policies specific to site design and access would be applicable to this request since the property is currently included in the industrial land inventory.

Staff Finding – Rezoning of the subject property to the IL District would be in conformance with *Goal 1: Development in Established Industrial and Employment Related Area of Horizon 2020*. The proposed rezoning would conform with the comprehensive plan. Approval of the request does not alter the property's inclusion in the industrial inventory.

2. ZONING AND USE OF NEARBY PROPERTY, INCLUDING OVERLAY ZONING

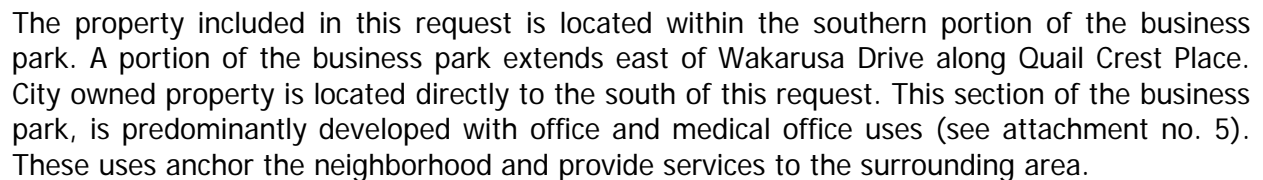
Current Zoning and Land Use:	IBP (Industrial Business Park) District; vacant land.
Surrounding Zoning and Land Use:	<p>IBP (Industrial Business Park) District to the north and west; <i>Healthcare Office/Health Clinic</i> use to the west and <i>Veterinary</i> use and <i>Administrative and Professional Office</i> uses to the north.</p> <p>GPI (General Public and Institutional Use) District to the south; Wastewater Treatment Plant</p> <p>RM12 (Multi-Dwelling Residential) District to the east. Lots to the east are developed with a <i>single-dwelling</i> residential structures.</p>

Staff Finding – The subject property is adjacent to IBP zoning to the north and west, GPI zoning is located to the south. The lots along the east side of Wakarusa Drive are developed with single-dwelling residential structures.

3. CHARACTER OF THE NEIGHBORHOOD

Applicant's Response: *"The neighborhood is comprised of a mix of commercial, public and residential uses. To the north and west, there exists a veterinary clinic and a health care office/clinic; to the south is a City of Lawrence owned property, and to the east across Wakarusa Drive is a mix of multi-family and single-family residential."*

The property is included in the West Lawrence Neighborhood (Figure 1). It is the largest neighborhood in Lawrence. The neighborhood includes area north of W. 6th Street to Peterson Road and south to Clinton Parkway. The neighborhood is located entirely on the west side of Wakarusa Drive. Land uses along Wakarusa Drive include commercial uses at W. 6th Street and Clinton Parkway. Between these two points, the area includes the *Oread West Business Park* (IBP, PID, and IL zoning) and approximately 118 acres zoned GPI (City of Lawrence Water Treatment Plant, Fire Station, Public Works fueling station, and future parkland). Open space as well as back-to-back zoning relationships are used to transition between uses. Figure 2 shows the location of significant land uses in the neighborhood.



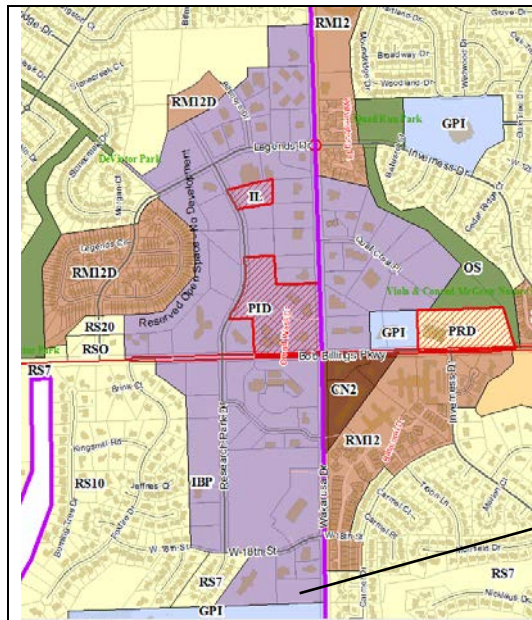


Figure 3: Oread West Research Park

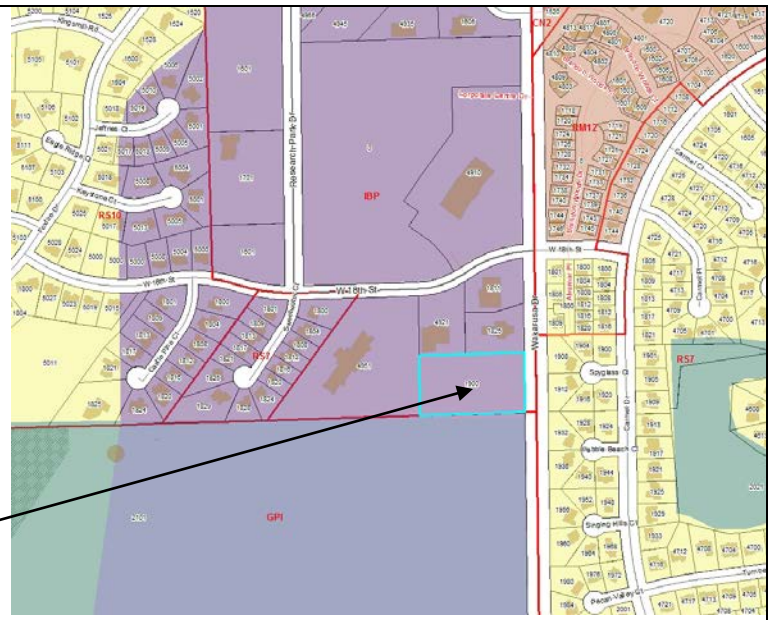


Figure 4: Subject Property/Portion of Oread

Staff Finding – The majority of the neighborhood is residential. Non-residential uses are located along the arterial streets that surround the neighborhood. The Oread West Research Office Park is located in the east-central portion of the neighborhood and includes a mix of office, industrial, and commercial uses.

4. PLANS FOR THE AREA OR NEIGHBORHOOD, AS REFLECTED IN ADOPTED AREA AND/OR SECTOR PLANS INCLUDING THE PROPERTY OR ADJOINING PROPERTY

There are no adopted area plans or sector plans that include this property. Map 3-2 in Horizon 2020 identifies the future land use of this parcel as Office Research Industrial. The development pattern has been established through the annexation, zoning, subdivision approvals, and extensions of infrastructure over time. The original IBP District was much larger but has been reduced over time. A property to the north of the proposed request was similarly modified by Z-16-00215 (IBP to IL with conditions).

Staff Finding – The property included in the request has not been included in a specific area or sector plan. The development pattern of the area has been established through the land use entitlement process.

5. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED UNDER THE EXISTING ZONING REGULATIONS

Applicant's Response: *"The current IBP zoning will allow for a portion of the proposed project (Symbiotic Behavioral Treatment Center), but not for the kennel, sales & grooming use."*

The property is currently zoned IBP which permits certain low-impact employment and manufacturing uses in a planned industrial/business parking setting. This portion of the staff report analyzes the new use that would be permitted and the uses that would be prohibited with the approval of the request. Attachment 4 shows the uses permitted in the two districts. This rezoning proposes restricting all of the uses normally permitted in the IL District that are not permitted in the IBP District to align the subject property to the surrounding area zoned IBP. Only one use,

Kennel and Sales & Grooming, permitted in the IL (not allowed in the IBP District) is proposed as an additional allowed use.

1310 Wakarusa Drive, to the north of the subject property, was restricted to allow *Construction Sales and Service, Kennel, Work/Live Unit, and Building Maintenance* (Z-16-00215). This rezoning allowed the development of the property for a *Construction Sales and Service* use known as Rainbow International, a contractor's shop and restoration business. This current application follows that precedent. The requested rezoning would accommodate the expansion of an existing business to locate/expand business operation in the western portion of the community.

Kennel and Sales & Grooming: The uses are defined in the Land Development Code as:

"Kennel services for dogs, cats, and small animals, including day care and overnight care. Typical uses include boarding kennels and dog training centers." (Section 20-1710 (2)).

Sales, grooming and day time care of dogs, cats, and similar small animals. Typical uses include pet stores, dog bathing and clipping salons and pet grooming shops. No overnight boarding is allowed." (Section 20-1710 (1)).

USES THAT WOULD BE RESTRICTED

Below is a list of the uses that are allowed in the IBP District that are not allowed in the IL District. If this request were approved, the following uses would no longer be an option for development because they are not allowed in the IL District, but are permitted in the IBP District.

- *Cultural Center Library*
- *Extended Care Facility, General* (permitted with approval of a Special Use Permit)
- *Accessory Restaurant* (permitted as an accessory use)¹
- *Private Dining Establishment*²

The uses normally permitted in the IL District that are proposed to be restricted are listed below. Uses shown in blue are permitted in the IL District with the approval of a Special Use Permit.

¹ Typical uses include snack bars, school cafeterias, and supermarket delicatessens.

² Private Dining uses are accessory to owner-occupied residential uses where the residence is the principal use. Residential uses are not permitted in the IBP District so it is unlikely that the use would ever occur.

- *Mobile Home*
- *Detention Facilities*
- *Lodge, Fraternal & Civic Assembly*
- *Passive Funeral and Internment*
- *Campus or Community Institution*
- *Neighborhood Institution*
- *Livestock Sale*
- *Mini-Warehouse*
- *Veterinary*
- *Fast Order Food*
- *Fast Order Food, Drive-in*
- *Accessory Parking Facilities*
- *Agricultural Sales*
- *Building Maintenance*
- *Construction Sales and Services*
- *Food and Beverage*
- *Mixed Media Store*
- *Personal Convenience Services*
- *Personal Improvement Services*
- *Repair Service, Consumer*
- *Retail Sales, General*
- *Hotel, Motel, Extended Stay*
- *Vehicle Sales & Service; Cleaning*
- *Fleet Storage*
- *Gas and Fuel Sales*
- *Heavy Equipment Repair*
- *Heavy Equipment Sales/Rental*
- *Inoperable Vehicles Storage*
- *Light Equipment Repair*
- *Light Equipment Sales/Rental*
- *RV and Boat Storage*
- *Industrial, General*
- *Laundry Service*
- *Scrap and Salvage Operation*
- *Wholesale Storage & Distribution; Heavy*
- *Wholesale Storage and Distribution; Light*
- *Urban Farm*
- *Recycling; Large Collection*
- *Recycling; Processing Center*

Staff Finding – The proposed request does not alter the suitability of the property for future development of uses permitted in the IBP District, with the exception of allowing *Kennel and Sales & Grooming*. The proposed zoning could be described as IBP plus two additional uses – *Kennel and Sales & Grooming*. Approval of the request does not substantively alter the allowed uses or potential for development.

6. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Applicant's Response: *The subject property has remained vacant since its annexation into the City in 1988.*

The property was rezoned to IBP (Industrial Business Park) District in 2006 with the adoption of the Land Development Code. Prior to 2006 the property was zoned M-1 (Research Industrial) District. The M-1 District was established through multiple annexation and rezoning requests from 1983 to 2003.

Staff Finding – The property included in the request has been zoned for industrial development in the current configuration since annexation in 1983. The property is undeveloped.

7. EXTENT TO WHICH APPROVING THE REZONING WILL DETRIMENTALLY AFFECT NEARBY PROPERTIES

Applicant's Response: *"The rezoning will have no detrimental impact on the nearby properties. The surrounding properties include these uses: veterinary clinic, a health care clinic/office and the City of Lawrence services. The new use would be complimentary and beneficial to the existing properties."*

The proposed change will not cause a detrimental effect. The impact of the request is to retain the currently permitted list of uses of the IBP district, with the addition of *Kennel and Sales & Grooming*. Other uses permitted in the IL district would be prohibited.

Staff Finding – Approval of the conditional IL district request mitigates detrimental effects on nearby properties and retains the property as part of the industrial inventory.

8. THE GAIN, IF ANY, TO THE PUBLIC HEALTH, SAFETY AND WELFARE DUE TO THE DENIAL OF THE APPLICATION, AS COMPARED TO THE HARDSHIP IMPOSED UPON THE LANDOWNER, IF ANY, AS A RESULT OF DENIAL OF THE APPLICATION

Applicant's Response: *"The current land owner has had the property on the market for many many years. If this proposal is denied, the land will remain undeveloped and continue to produce a lower property tax. When developed, the property tax will increase significantly , and provide a complimentary use to the area as well as help existing businesses grow."*

Evaluation of this criterion includes weighing the benefits to the public versus the benefit of the owners of the subject property. Benefits are measured based on anticipated impacts of the rezoning request on the public health, safety, and welfare.

This IBP District is unique in the City of Lawrence. This area is the only industrial node with this specific designation. The area was developed incrementally and the district boundary has fluctuated with respect to various development demands. Infrastructure is adjacent to this specific area included in the request.

Staff Finding – Denial of the request defers development opportunity of underutilized land. Approval of the request facilitates infill development within the business park portion of the neighborhood.

9. PROFESSIONAL STAFF RECOMMENDATION

The intent of the applicant's request is to retain the uses currently permitted in the IBP district and to allow the *Kennel and Sales & Grooming* uses. The request allows the uses without modifying the Land Development Code by seeking to change the uses permitted in the base district. The proposed development is for a Symbiotic Behavioral Treatment Center facility (therapy with animals) and kennel with grooming. The activity is predominantly indoors (with outside kennel run area) and compatible with the development pattern of the developing business park. All industrial use districts are included in the industrial inventory for this industrial park. Staff recommends approval of the proposed IL District with restrictions.

CONCLUSION

Staff review for conditional zoning was at the applicant's request. The applicant will be required to submit a site plan for administrative review and approval prior to issuance of a building permit.



ARCHITECTURAL SITE PLAN A1

DATE: 09/24/18
Client Name: [Redacted]
Project No: [Redacted]
Revision: [Redacted]

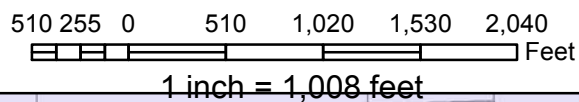
Home Sweet Home Dog Resort
& Symbiotic Behavioral Treatment Center
Street
City, State, Zip

TREANOR^{HL}
1040 Vermont
Lawrence, Kansas 66044
Office: 785.842.4858
Fax: 785.842.7536
www.TreanorHL.com

UNLESS A
PROFESSIONAL SEAL
WITH SIGNATURE AND
STAMP IS PRESENT
THIS DOCUMENT IS
PRELIMINARY AND IS
NOT INTENDED FOR
CONSTRUCTION

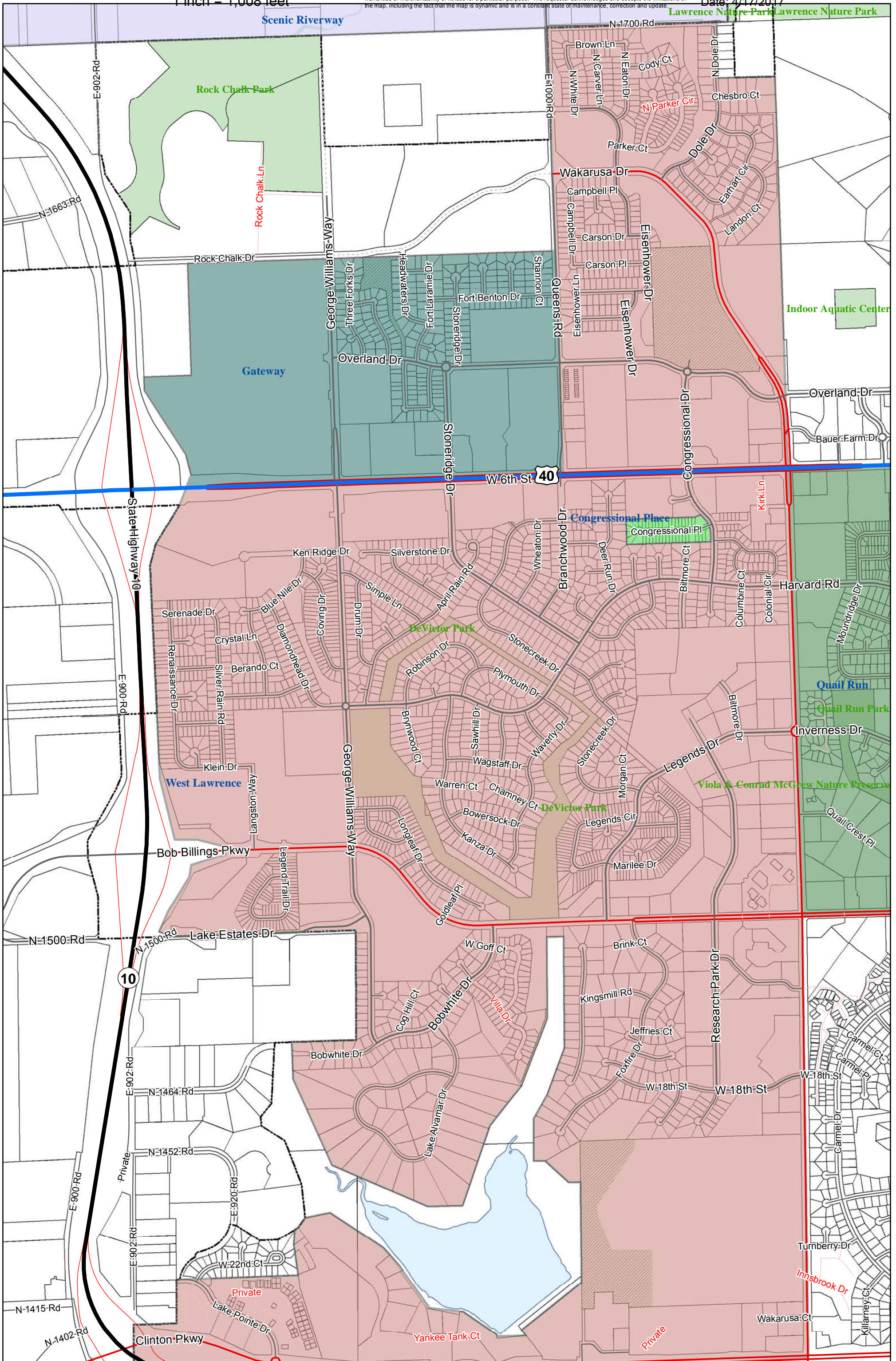
DESIGN BY: [Redacted]
CHECKED BY: [Redacted]
REVISIONS:
NO. DESCRIPTION DATE
DATE: [Redacted]

SITE PLAN
SP101
Treasnor, Inc. DWG: 021-008



DISCLAIMER NOTICE

Date: 4/17/2017



20-403 NONRESIDENTIAL DISTRICT USE TABLE USE TABLE FOR IBP AND IL DISTRICT

Uses proposed to be restricted are shown with a strikethrough.

<i>A = Accessory</i> <i>P = Permitted</i> <i>S = Special Use</i> <i>* = Standard Applies</i> <i>- = Use not allowed</i>			
	<i>IBP</i>	<i>IL</i>	<i>STDS</i>
Household Living			
Accessory Dwelling Unit	-	-	
Attached Dwelling	-	-	
Cluster Dwelling	-	-	
Detached Dwelling	-	-	
Duplex	-	-	
Manufactured Home	-	-	
Manufactured Home, Residential-Design	-	-	
Mobile Home	-	P	
Mobile Home Park	-	-	
Multi-Dwelling Structure	-	-	
Non-Ground Floor Dwelling	-	-	
Work/Live Unit	-	P*	517/541
Zero Lot Line Dwelling	-	-	
Home Occupation, Type A or B	-	-	
Group Living			
Assisted Living	-	-	
Congregate Living	-	-	
Dormitory	-	-	
Fraternity or Sorority House	-	-	
Group Home, General [11 or more]	-	-	
Group Home, Limited [10 or fewer]	-	-	
Community Facilities			
College/University	P	P	
Cultural Center/ Library ¹	P	-	
Day Care Center	P*	P*	507
Day Care Home, Class A	-	-	
Day Care Home, Class B	-	-	
Detention Facilities	-	S	
Event Center, Small	P	P	
Event Center, Large	P	P	
Lodge, Fraternal & Civic Assembly	-	P*	512
Postal Service	P	P	
Public Safety	P	P	
School	-	-	
Active Funeral and Interment	P*	P*	505
Passive Funeral and Internment	-	A*	505
Temporary Shelter	S*	S*/A	544/522
Social Service Agency	P	P	

¹ Use allowed in the IBP but not permitted in the IL District.

Community Meal Program	S	S/A*	522
Utilities, Minor	P*/S*	P*/S*	530
Utilities and Service, Major	S	S	
Medical Facilities			
Community Mental Health Facility	–	–	
Extended Care Facility, General ²	S	–	
Extended Care Facility, Limited	–	–	
Health Care Office, Health Care Clinic	P	P	
Hospital	–	–	
Outpatient Care Facility	–	–	
Recreational Facilities			
Active Recreation	P	P	
Entertainment & Spectator Sports, General	–	–	
Entertainment & Spectator Sports, Limited	–	–	
Participant Sports & Recreation, Indoor	P	P	
Participant Sports & Recreation, Outdoor	P	P	
Passive Recreation	P	P	
Nature Preserve/Undeveloped	P	P	
Private Recreation	–	–	
Religious Assembly			
Campus or Community Institution	–	P*	522
Neighborhood Institution	–	P*	522
Animal Services			
Kennel	–	P*	
Livestock Sale	–	P	
Sales and Grooming	–	P	
Veterinary	P	P	
Eating & Drinking Establishment			
Accessory Bar	A*	A*	509
Accessory Restaurant ³	A	–	
Bar or Lounge	–	–	
Brewpub	–	–	
Fast Order Food	–	P*	511/509
Fast Order Food, Drive-in	–	P	
Nightclub	–	–	
Private Dining Establishments ⁴	P*	–	539
Restaurant, Quality	P*	P*	524
Office			
Administrative and Professional	P	P	
Financial, Insurance & Real Estate	P	P	
Payday Advance, Car Title Loan Business	P	P	
Other	P	P	
Parking Facilities			
Accessory	A*	A*	535
Commercial	P	P	
Retail Sales & Services			

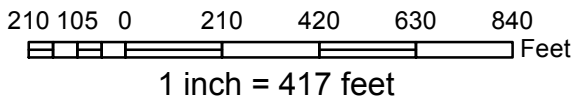
² Use allowed in the IBP but not permitted in the IL District.

³ Use allowed in the IBP but not permitted in the IL District.

⁴ Use allowed in the IBP but not permitted in the IL District.

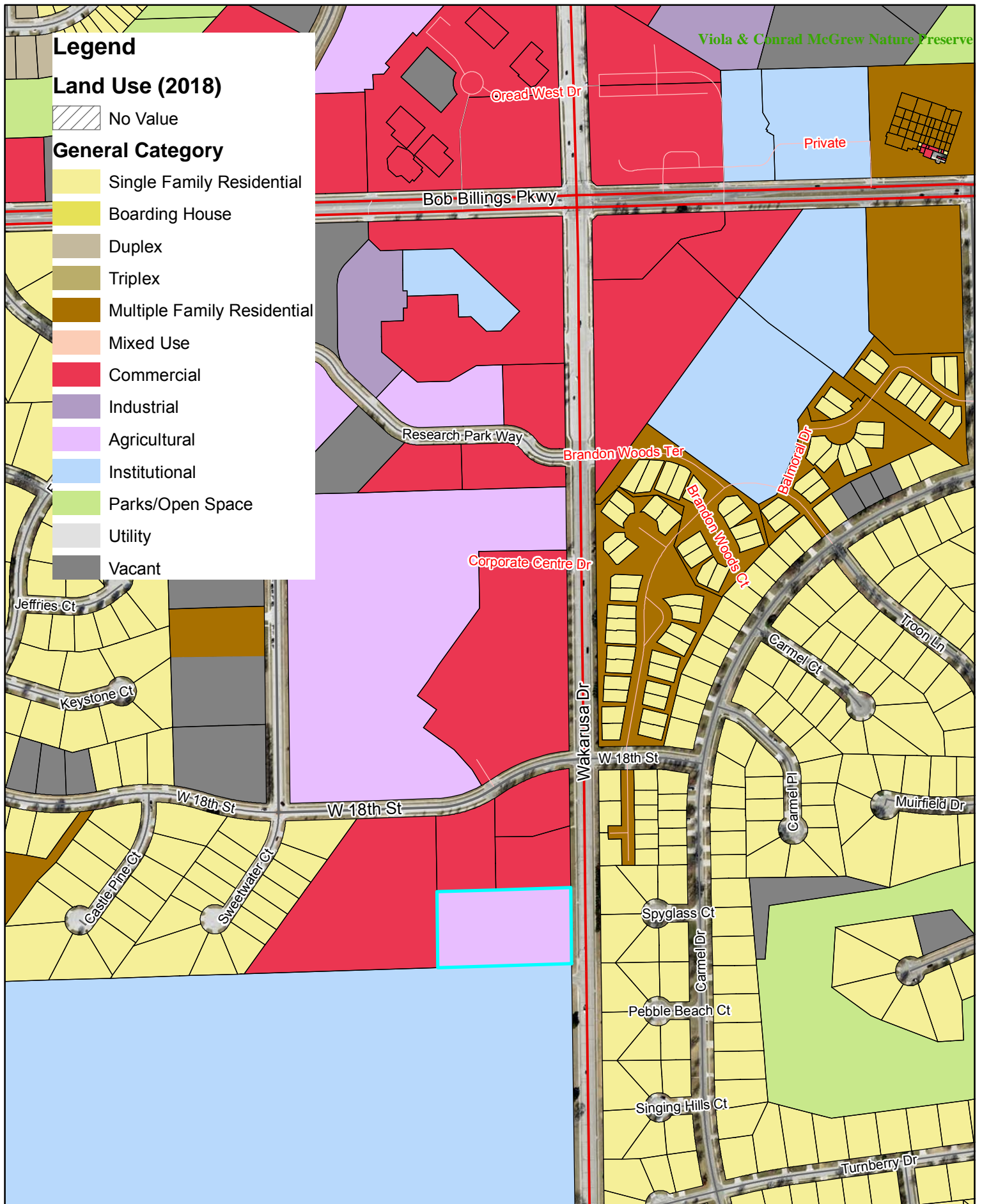
Agricultural Sales	-	P	
Building Maintenance	-	P	
Business Equipment	P	P	
Business Support	P	P	
Construction Sales and Service	-	P	
Food and Beverage	-	P*	511
Mixed Media Store	-	P*	516/528
Personal Convenience	-	P	
Personal Improvement	-	P	
Repair Service, Consumer	-	P*	523
Retail Sales, General	-	P*	525
Retail Establishment, Large	-	-	
Retail Establishment, Medium	-	-	
Retail Establishment, Specialty	-	-	
Sexually Oriented Businesses			
Sexually Oriented Media Store	-	-	
Physical Sexually Oriented Business	-	-	
Sex Shop	-	-	
Sexually Oriented Theater	-	-	
Transient Accommodations			
Bed and Breakfast	-	-	
Campground	-	-	
Hotel, Motel, Extended Stay	-	P	
Vehicle Sales & Services			
Cleaning (e.g., car wash)	-	P	
Fleet Storage	-	P	
Gas and Fuel Sales	-	P	
Truck Stop	-	-	
Heavy Equipment Repair	-	P	
Heavy Equipment Sales/Rental	-	P	
Inoperable Vehicles Storage	-	P	
Light Equipment Repair	-	P	
Light Equipment Sales/Rental	-	P	
RV and Boats Storage	-	P	
Industrial Facilities			
Explosive Storage	-	-	
Industrial, General	-	P	
Industrial, Intensive	-	-	
Laundry Service	-	P	
Maker Space, Limited	P	P	
Maker Space, Intensive	P	P	
Manufacturing & Production, Limited	P	P	
Manufacturing & Production, Tech	P	P	
Research Service	P	P	
Scrap and Salvage Operation	-	S*	527
Wholesale Storage & Distribution			
Exterior Storage	A*	A*	538
Heavy	-	S	
Light	P	P	
Mini-Warehouse	-	P	

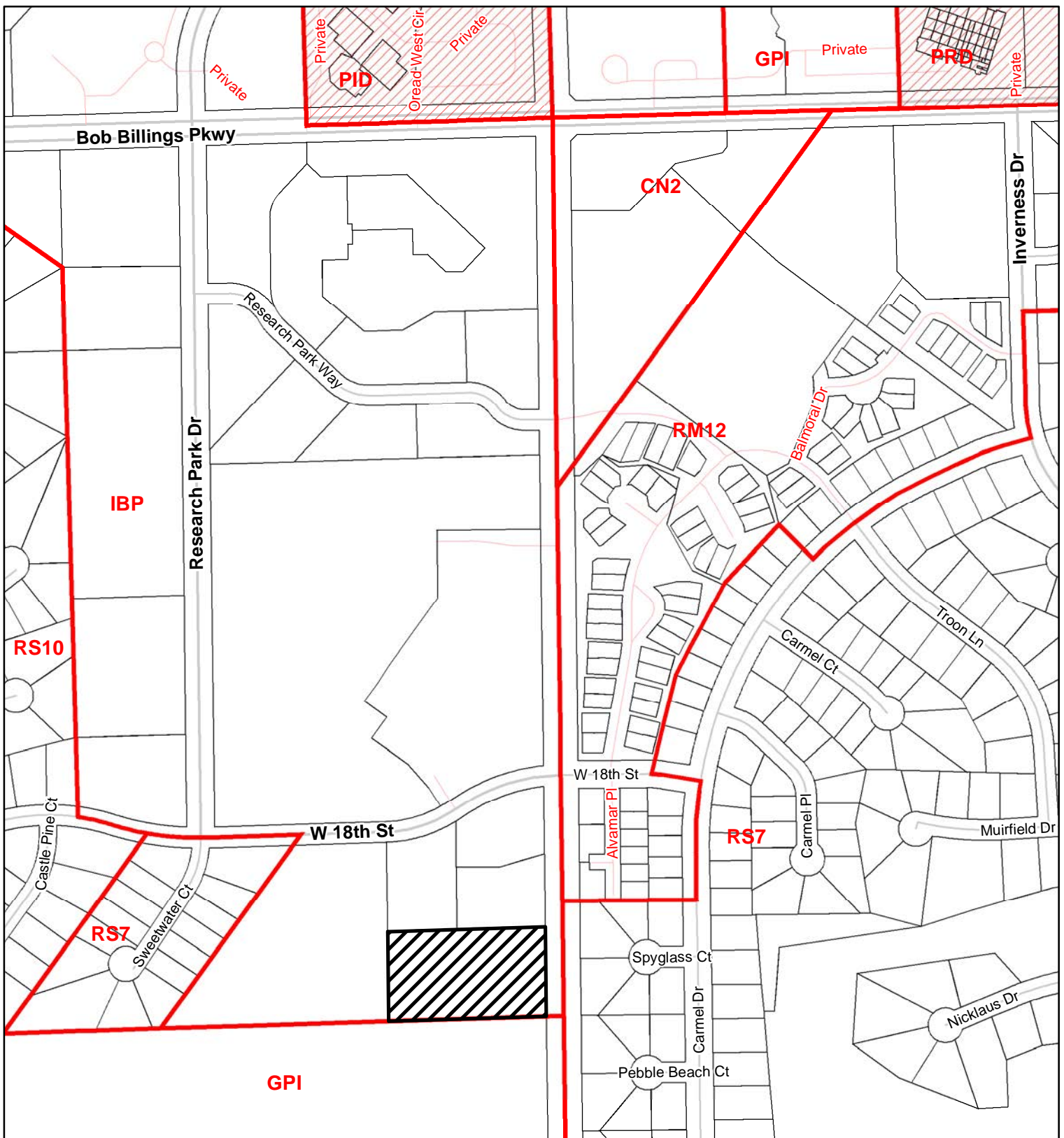
Adaptive Reuse			
Designated Historic Property	S*	S*	501
Greek Housing Unit	-	-	
Urban Agriculture			
Agriculture, Crop	P	P	
Agriculture, Large Animal	-	-	
Agriculture, Small Animal	A*	A*	
Farmers Market	P	P	
On-Site Agricultural Sales	A*	A*	
Urban Farm	-	P*	
Communication Facilities			
Amateur and Receive-Only Antennas	A*	A*	536
Broadcasting Tower	P	P	
Communications Service Establishment	P	P	
Wireless Facility- Antenna	A*	A*	529
Wireless Support Structure	S*	S*	529
Satellite Dish	A*	A*	536
Mining			
Mining	-	-	
Recycling			
Large Collection	-	P	
Small Collection	P	P	
Processing Center	-	S	



DISCLAIMER NOTICE
The map is provided "as is" without warranty or any representation of accuracy, timeliness or completeness. The burden for determining accuracy, completeness, timeliness, merchantability and fitness for or the appropriateness for use rests solely on the requester. The City of Lawrence makes no warranties, express or implied, as to the use of the map. There are no implied warranties of merchantability or fitness for a particular purpose. The requester acknowledges and accepts the limitations of the map, including the fact that the map is dynamic and is in a constant state of maintenance, correction and update.

Date: 12/17/2018





Z-18-00495: Rezone Approximately 2.7 acres
from IBP (Industrial/Business Park)
District to IL (Limited Industrial) District
for the property located at 1900 WAKARUSA DRIVE.



PLANNING COMMISSION REPORT
Regular Agenda –Public Hearing Item

PC Staff Report
12/19/2018

**ITEM NO. 6 CONDITIONAL USE PERMIT FOR Westwick Rugby/Soccer
Complex; Northwest of the intersection of County Route 458 & E
1150 Rd. (KEW)**

CUP-18-00501: Consider a Conditional Use Permit for a rugby/soccer complex, located northwest of the intersection of N. 1200 Rd (County Route 458) & E. 1150 Rd. Submitted by Paul Werner Architects, for Westwick LC, property owner of record.

STAFF RECOMMENDATION: Staff recommends approval of the Conditional Use Permit for a Rugby/Soccer Complex with the following conditions:

- Local and State floodplain permits will be required for all phases of development.
- The east entrance for the proposed Phase III parking lot will need to be upgraded to County Standards. A Douglas County entrance permit will be required for work in the Route 458 right-of-way.
- Existing conditions and restrictions for the previous CUP will remain or be revised by the County Commission if alternatives are proposed.
 1. Septic System was to be installed one year after construction of the playing fields.
 - a. Original deadline was July 1, 1996.
 - b. County Commission approved (in October of 1996) the extension of this deadline to the summer of 1998.
 - c. A septic system has not yet been installed according to the Lawrence Douglas County Health Department.
 - d. A septic system is proposed to be installed with Phase III of the improvements.
 - e. Staff recommends a deadline of 1 year from the date of resolution publication for the installation of an approved septic system with the approval of this Conditional Use Permit.
 2. Field use only between the hours of 8 AM and 9 PM.
 3. Ancillary activities proposed only between the hours of 8 AM and 10 PM.
 4. The use of the fields is restricted to rugby, soccer, football, lacrosse, softball, baseball and team sports involving the use of a Frisbee. Any other activity must be approved as a Temporary Business Permit granted by the Board of County Commissioners.
 5. On-site structures can only be used for ancillary uses associated with the allowed restricted activities and only during the approved hours for ancillary use.
 6. Consumption of alcohol and cereal malt beverages is limited to the confines of a club house or within a 25 foot X 25 foot designated an area adjacent to the club house building. The area shall be delineated with a wooden fence.
 7. The goal post for each playing field shall be no closer than 175 feet to any adjoining property line.
 8. The applicant shall provide locking gates with a sign stating the restrictions of the facility and who to contact regarding questions about the use of the facility.
 9. No sales or vending is allowed on the premises.
 10. No public address system or loud speakers are permitted.
 11. Lighting is to be shielded from view off the property.

Reason for Request:

Applicant's Response: *"The property owner would like to improve the onsite amenities. In order to do this, they need and updated CUP."*

ATTACHMENTS

- A. Conditional Use Permit Site Plan

KEY POINTS

- The subject property is zoned VC (Valley Channel) and A (Agricultural) District. The proposed use, *Recreation Facility*, is permitted in the VC District and the A District with approval of a Conditional Use Permit.
- Section 12-319.4.11 of the County Zoning Regulations for the Unincorporated Territory of Douglas County lists *Recreation Facility* as uses which may be approved as a Conditional Use.
- A CUP currently exists for this property and use (CUP-7-5-94.)
- Douglas County Zoning & Codes currently considers this project out of compliance with the existing CUP.
- The applicant has stated that they are not asking for removal of restrictions.
- A condition of approval regarding the installation of a septic system has not yet been installed and is proposed with Phase III of site improvements.

ASSOCIATED CASES

- **CUP-7-5-94**; A Conditional Use Permit for the establishment of the Westwick Rugby/Soccer Complex was approved by the Board of County Commissioners on December 7, 1994. There was a condition extension (regarding septic installation) approved by the Board of County Commissioners in October of 1996. Septic installation was required to be installed by the summer of 1998.

OTHER ACTION REQUIRED

- Approval of the Conditional Use Permit by the Board of County Commissioners.
- Release of the Conditional Use Permit Plan by the Lawrence-Douglas County Planning Office to the Douglas County Zoning and Codes Office.
- Issuance of permit for the Conditional Use by the Zoning and Codes Department following application and determination that all conditions have been met.
- Submittal of building plans by the property owner for approval and issuance of building permit from the Douglas County Zoning and Codes Department prior to development.
- Approval from the Lawrence Douglas County Health Department for the septic system.

PUBLIC COMMUNICATION

There have been no communications or inquiries.

GENERAL INFORMATION

Current Zoning and Land Use:

VC (Valley Channel) and A (Agricultural) District; Westwick Rugby/Soccer Complex, *Recreation Facility*.

Surrounding land use:

To the north, east and west: VC (Valley Channel) District; agriculture land.

To the south: A (Agriculture) District; rural residential and agricultural uses.

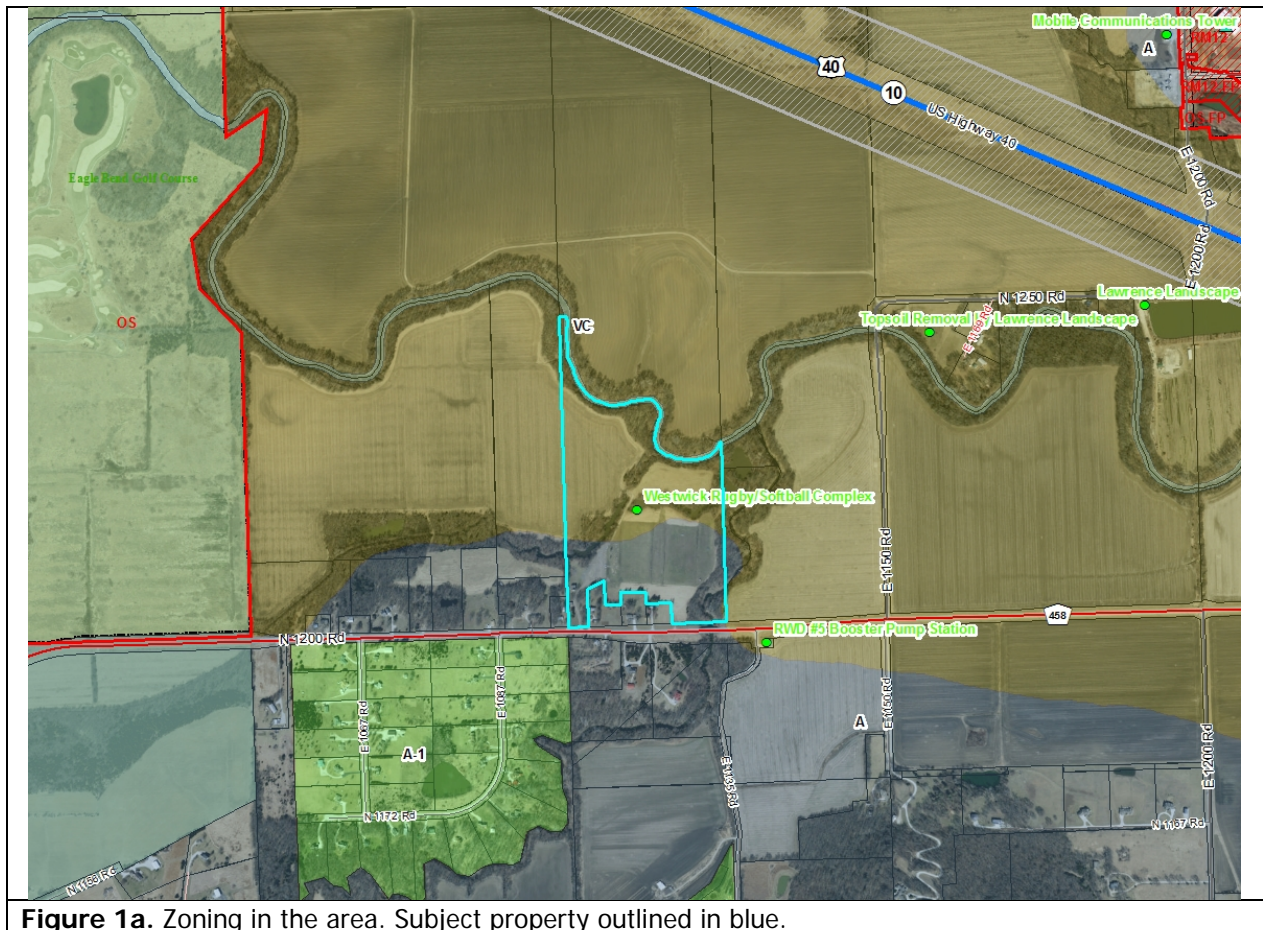


Figure 1a. Zoning in the area. Subject property outlined in blue.

Summary of Request

This request proposes to accommodate improvements to the existing on-site amenities. The use will not change and will remain a rugby/soccer complex. Remodel of the site will include the addition of portable restroom facilities as Phase I, a 900 square foot structure for storage and/or changing rooms as part of Phase II. Locker rooms, restrooms, pavilions for viewing both fields, an additional parking lot, and a septic field is proposed for Phase III.

A Conditional Use Permit (CUP) for the *Recreation Facility* use was previously approved for the subject property. That CUP (CUP-7-5-94) permits the rugby/soccer use, Westwick Rugby and Soccer, on the property. The request is for an updated CUP for the on-site improvements.

I. ZONING AND USES OF PROPERTY NEARBY

The subject property is zoned VC (Valley Channel) and A (Agriculture) and is located southwest of the Lawrence City limits, just north of County Route 458 and east of Clinton Lake. The surrounding properties located outside of the city limits are zoned A District, and VC (Valley Channel) the nearby properties are open agricultural land and rural residential.

Staff Finding –The area is outside the municipal limits of Lawrence and is primarily rural lands and uses. Agricultural land is the predominant land use in this area, with rural dwellings located

primarily to the south and southwest. The proposed request will not alter the base zoning or land use of the area.

II. CHARACTER OF THE AREA

The area contains rural uses. From US Highway 59 west to Clinton Lake, the principal land use is open agricultural land with some rural residences on properties that are outside the city limits.

Staff Finding – The area contains a blend of agricultural land and rural uses. The current use on the property has existed since 1996. The proposed request will not alter the character of the area.

III. SUITABILITY OF SUBJECT PROPERTY FOR THE USES TO WHICH IT HAS BEEN RESTRICTED

Applicant's Response: *"The use is not going to change. The facilities will be updated and the use will remain a rugby complex."*

The subject property is zoned VC (Valley Channel) and A (Agricultural) District.

Section 12-314 of the County Zoning Regulations notes, "...the purpose of this district is to prevent, in those areas subject to periodic or potential flooding, such development as would result in a hazard to health or safety, and to ensure the general public will not be forced to expend exorbitant funds to remedy flood problems."

Section 12-306 of the County Zoning Regulations notes, *"...the purpose of this district is to provide for a full range of agricultural activities, including processing and sale of agricultural products raised on the premises, and at the same time, to offer protection to agricultural land from the depreciating effect of objectionable, hazardous and unsightly uses."* The A District is associated with a majority of the unincorporated portion of Douglas County.

Uses allowed in the VC District include: farms, truck gardens, orchards, or nurseries for the growing or propagation of plants, trees and shrubs in addition other types of open land uses. In addition, uses enumerated in Section 12-319, which are not listed as permitted uses in the A District, may be permitted when approved as Conditional Uses.

Uses allowed in the A District include: farms, truck gardens, orchards, or nurseries for the growing or propagation of plants, trees and shrubs in addition other types of open land uses. It also includes residential detached dwellings, churches, hospitals, clinics for large and small animals, commercial dog kennels, and rural home occupations. In addition, uses enumerated in Section 12-319, which are not listed as permitted uses in the A District, may be permitted when approved as Conditional Uses.

Section 12-319-4.02 *Athletic Field/Baseball field* is an enumerated use permitted by Conditional Use Permit (CUP).

Staff Finding – The property is suitable for the uses which are permitted within the VC (Valley Channel) and A (Agricultural) District. The property is also well suited for rugby and soccer fields, an *Athletic Field/Baseball Field* use.

IV. LENGTH OF TIME SUBJECT PROPERTY HAS REMAINED VACANT AS ZONED

Staff Finding – County Zoning Regulations were adopted in 1966; this property has been zoned “A (Agricultural)” since that adoption. This property is not vacant, but has been used as a rugby/soccer facility since the approval of the Conditional Use Permit in 1994.

V. EXTENT TO WHICH REMOVAL OF RESTRICTIONS WILL DETRIMENTALLY AFFECT NEARBY PROPERTY

Applicant's Response:

“None – Adjacent building within 200 feet is on this property.”

Section 12-319-1.01 of the County Zoning Regulations recognize that *“certain uses may be desirable when located in the community, but that these uses may be incompatible with other uses permitted in a district...when found to be in the interest of the public health, safety, morals and general welfare of the community may be permitted, except as otherwise specified in any district from which they are prohibited.”* The proposed use falls under Section 12-319-4.11 *Athletic Field/Baseball field* of the County Zoning Regulations.

Impacts from this type of use are usually associated with traffic, crowd noise or activity, and lighting. The site is accessible from County Route 458, an improved principle arterial. The street is capable of handling periodic increased traffic volume associated with this use.

The CUP addresses noise concerns through the inclusion of a note stating noise which “unreasonably annoys, disturbs, injures or endangers the comfort, repose, health, peace or safety of a reasonable person will not be a problem.”

Lighting is not being proposed. Any lighting for night games would need to be shielded and conform to lighting standards in the Land Development Code.

Staff Finding – The proposed use is compatible with the surrounding area and will not detrimentally affect nearby properties.

VI. RELATIVE GAIN TO THE PUBLIC HEALTH, SAFETY AND WELFARE BY THE DESTRUCTION OF THE VALUE OF THE PETITIONER'S PROPERTY AS COMPARED TO THE HARDSHIP IMPOSED UPON THE INDIVIDUAL LANDOWNERS

Evaluation of the relative gain weighs the benefits to the community-at-large vs. the benefit of the owners of the subject property.

Approval of this request would allow the renovation of an existing site to be used as an event center, a *Athletic Field/Baseball field* use.

No benefit would be afforded to the public health, safety, or welfare by the denial of the request as no negative impacts are anticipated with the site improvements.

Staff Finding – In staff's opinion, there would be no gain to the public health, safety and welfare by the denial of the request. Approval of the request would allow an existing use to continue and accommodate site improvements.

VII. CONFORMANCE WITH THE COMPREHENSIVE PLAN

An evaluation of the conformance of a conditional use permit request with the comprehensive plan is based on the strategies, goals, policies, and recommendations contained within *Horizon 2020*. A Conditional Use Permit provides additional review for uses which may be desirable in an area, but could have negative impacts. This tool allows development to occur in harmony with the surrounding area and to address specific land use concerns.

Staff Finding – A conditional use permit can be used to allow specific uses that are not permitted in a zoning district with the approval of a site plan. This tool allows development to occur in harmony with the surrounding area and to address specific land use concerns.

STAFF REVIEW (Site Plan)

The proposed request is intended to make improvements to existing facilities, add new accessory structures with septic, parking, and entrance improvements in three phases. The new parking area, locker room, permanent restrooms, viewing area, and septic system are proposed for Phase III. Phase I will include a 600 foot structure to accommodate 4 portable restrooms. Phase II will include a 900 square foot structure to provide additional storage, team changing rooms and/or screening for additional portable restroom.

Parking and Access: The site plan identifies the improvement of a gravel parking area that is located to the west of the existing soccer fields. The parking area accommodates 150 parking spaces. There is not a specific formula to calculate the parking requirement for this activity in the parking regulations of Section 12-316-1 of the County Zoning Regulations. However, similar uses approved by conditional use permit in Douglas County where groups of people congregate for an outdoor field event, such as for soccer, a calculation has been used as follows; 15 team players plus 2 coaches X 2 teams per field, plus referees. This comes to approximately 38-40 persons per field. The rugby use has the same number of team members so this field use could utilize the same calculation and would require at least 120 spaces to support the maximum field use on all three fields. The parking spaces proposed with the site plan exceed this parking requirement. Phase III would include a new parking area taking access off the eastern portion of the site that would accommodate an additional 174 +/- parking spaces. All parking and drive areas will be 4 inch base AB-3 gravel.

Access to the site is accommodated via an existing driveway off of County Route 458 (N. 1200 Road). There is another existing access drive to the east of the site that will be developed as part of Phase III when the additional parking area is installed. The County Engineer indicated:

- The east entrance for the proposed Phase III parking lot will likely need to be upgraded, i.e. widened and possibly flattened to ensure adequate sight distance to the east over the bridge rail. A Douglas County entrance permit will be required for work in the Route 458 right-of-way
- It appears the southernmost one or two parking spaces for the proposed Phase III parking lot are too close to the bottom of the entrance ramp to be readily usable
- Recommend keeping the south edge of the east-west portion of the proposed Phase III parking lot north of the vegetative screening along the north side of property at 1114 N. 1200 Rd.
- While this property lacks the required frontage for two entrances, given the nature of the facility and the need for emergency vehicle access, and the number of entrances in the immediate vicinity, the engineer is fine with the east entrance for access to the proposed Phase III parking lot.

Access to the new parking area will be provided according to county specifications from the existing east access drive at the time of Phase III.

Floodplain: This property does reside in the regulatory floodplain. State and local floodplain permits will be required for all phases of improvements.

Landscape and Screening: The proposed location of the rugby/soccer field complex is an existing use that is located approximately 300 feet from County Route 458 (N. 1200 Road). The complex, as well as the parking and the outdoor areas, will be screened from view from these roadways by stands of mature trees and rural residential parcels that are adjacent to County Route 458.

The site plan notes that the trash receptacle is screened and enclosed.

Limits and Conditions:

The applicant has not proposed to remove or change any of the existing Conditional Use Permit conditions and restrictions that were approved for this use in 1994. (CUP-7-5-94) Those conditions and restrictions are listed as part of this Conditional Use Permit as follows:

HOURS OF OPERATION

The site plan notes that the hours associated with the rugby/soccer fields and ancillary activity will be as follows:

Field hours: 8:00 a.m. to 9:00 p.m.

Ancillary use: 8:00 a.m. to 10:00 p.m.

The use of the fields is restricted to rugby, soccer, football, lacrosse, softball, baseball and team sports involving the use of a Frisbee. Any other activity must be approved as a Temporary Business Permit granted by the Board of County Commissioners.

On-site structures can only be used for ancillary uses associated with the allowed restricted activities and only during the approved hours for ancillary use.

Consumption of alcohol and cereal malt beverages is limited to the confines of a club house or within a 25 foot X 25 foot designated an area adjacent to the club house building. The area shall be identified with a wooden fence.

The goal post for each playing field shall be no closer than 175 feet to any adjoining property line.

The applicant shall provide locking gates with a sign stating the restrictions of the facility and who to contact regarding questions about the use of the facility.

No sales or vending is permitted on the property.

No Public Announcement System or speakers are permitted on the property.

All lighting shall be shielded. No night game lighting is proposed at this time.

KDHE has indicated that a septic system is required for this use/property. A Conditional Use Permit was approved in 1994 for this facility. One of the conditions of approval was the installation of an approved septic system by 1996. The County Commission approved an extension for installation in 1996 to install the system by summer of 1998. A brief history discussing the condition with the applicants follows:

1. In March of 2000, the Planning Staff met with the owners of Westwick Rugby Complex to discuss ongoing compliance with the CUP requirements and restrictions. It is noted that staff met with the applicant to discuss progress toward conditions of approval.
2. In July of 2001, the applicant inquired about making improvements to the site and future plans. Planning staff responded and let the applicant know that additional expansion and improvements would be a revision to the CUP that would need to go before the Board of County Commissioners for approval. Review of the expansion/revision would include compliance with the original conditions of approval.
3. In August of 2004, additional email correspondence with the applicant and planning staff discussed Veritas Junior High School utilizing the fields as their home field. Planning staff determined this would fall under the original CUP as long as all restrictions and conditions were met. Any night games and lighting would need to go back before the Board of County Commissioners for approval. Staff also noted that all conditions, including the installation of the septic system would need to be addressed.
4. In November of 2015, additional improvements to the site were discussed with Planning Staff. Staff noted that any additional structures and improvements would need to obtain floodplain permits. The sanitary system was also discussed. Staff re-capped the history of discussion of this condition and noted that compliance has still not been met. Staff also noted that although there did not seem to be any significant issues with the planned improvements, a revised site plan would need to go before the Board of County Commissioners to address the status of the CUP condition regarding the installation of a septic system.
5. In March of 2016, Planning Staff followed up via email to another inquiry regarding improvements to the site and the installation of a septic system. Planning staff sent a copy of the original CUP permit which listed the original conditions of approval including the installation of the septic system by July 1, 1996 and the extension of this deadline approved by the Board of County Commissioners in October of 1996 to extend the deadline for installation to the summer of 1998. Staff also noted that any improvements would require floodplain permits.

The requirement for an approved septic system is proposed by the applicant to be installed as part of Phase III construction for the improvements associated with this Conditional Use Permit. The applicant has not requested changes to the existing restrictions or conditions and has not provided a timeline for when the phases are expected to be completed.

NOISE

The previous Conditional Use Permit stated there shall be no public announcement system or speakers permitted. This condition will remain.

Conclusion

The property is primarily used a rugby and soccer facility. The scope of this proposal is to permit the *Athletic Field/Baseball field* use as an existing use on the property and accommodate the site improvements. The proposed CUP complies with the County Zoning Regulations and recommendations of *Horizon 2020* with the listed conditions and restrictions as noted.

Staff recommends a deadline of 1 year from the date of resolution publication for the installation of an approved septic system with the approval of this conditional use permit.

SITE DESCRIPTION

A TRACT OF LAND IN THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 13 SOUTH RANGE 14 EAST OF THE 6TH P.M. IN DOUGLAS COUNTY KANSAS MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID SECTION 15; THENCE ALONG SOUTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 15, N 84°25'24" E. 172.30 FEET; THENCE N 0°34'36" W. 347.03 FEET; THENCE N 65°18'44" E. 197.33 FEET; THENCE S 0°34'36" E. 202.60 FEET; THENCE N 84°25'24" E. 197.10 FEET; THENCE N 0°34'36" W. 100.00 FEET; THENCE N 84°25'24" E. 208.10 FEET; THENCE S 0°34'36" E. 100.00 FEET; THENCE N 84°25'24" E. 208.10 FEET; THENCE S 0°34'36" E. 208.10 FEET; TO THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 15; THENCE N 84°25'24" E. 463.02 FEET TO THE SOUTHWEST CORNER OF THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 15; THENCE ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 15, N 0°05'13" W. 370.00 FEET TO THE CENTER OF WASHINGTON CREEK; THENCE ALONG THE CENTER OF WASHINGTON CREEK TO THE INTERSECTION WITH THE SOUTH LINE OF THE NORTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 15; THENCE S 84°25'24" W. 480.00 FEET TO THE NORTHEAST CORNER OF THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 15; THENCE ALONG THE EAST LINE OF THE NORTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 15, N 0°05'13" W. 190.00 FEET TO THE CENTER OF WAKARUSA RIVER; THENCE ALONG THE CENTER OF WAKARUSA RIVER TO THE INTERSECTION WITH THE NORTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 15; THENCE S 84°25'24" E. 50 FEET TO THE WEST QUARTER CORNER OF SAID SECTION 15; THENCE S 0°03'52" E. 2641.01 FEET TO THE POINT OF BEGINNING, CONTAINING 58.5 ACRES, INCLUDING 0.5 ACRES OF EXISTING PUBLIC ROAD RIGHT OF WAY,

PHASE DESCRIPTIONS

- PHASE I - IMPROVEMENTS
- CONSTRUCT UP TO A 600FT STORAGE BUILDING. STRUCTURE TO SATISFY UP TO 4 PORTABLE RESTROOM FACILITIES.
- PHASE II - IMPROVEMENTS
- CONSTRUCT UP TO A 900 SQ FT STRUCTURE TO PROVIDE ADDITIONAL STORAGE, TEAM CHANGING ROOMS, AND/OR SCREENING FOR ADDITIONAL PORTABLE RESTROOM FACILITIES.
- PHASE III - IMPROVEMENTS
- CONSTRUCT LOCKER ROOMS, RESTROOMS AND PAVILIONS FOR VIEWING BOTH FIELDS. OCCUPANCY UP TO 90 PEOPLE, EXCLUDING TEAM ROOMS.
 - AN ADDITIONAL PARKING LOT HOLDING 164 CARS TO BE CONSTRUCTED PRIOR TO OCCUPANCY.
 - A SEPTIC FIELD OR A LAGOON SHALL BE CONSTRUCTED WITH THIS PHASE.

GENERAL NOTES

1. ALL AREAS DESIGNATED AS ROADWAYS AND PARKING AREAS WILL BE CONSTRUCTED WITH A 4" BASE COURSE OF AB-3 GRAVEL.
2. A TOTAL OF 150 REGULAR PARKING SPACES HAVE BEEN PROVIDED IN ADDITION TO 4 ADA SPACES.
3. LIGHTING FOR NIGHT GAMES IS NOT BEING PROPOSED AT THIS TIME.
4. THE WATER SUPPLY IS CURRENTLY DIST. NO. 5. WELLS WILL BE USED FOR IRRIGATION IF THE PLAYING FIELDS. THE WELL SITES HAVE YET TO BE DETERMINED.
5. PORTABLE OUTHOUSES WILL SERVE AS RESTROOM FACILITIES. SEE PHASE I, II AND III DESCRIPTIONS.
6. TRASH COLLECTION WILL BE IN DESIGNATED 10' X 10' AREAS WITH RECEPTACLES HAVING A MECHANISM TO KEEP THE TOP CLOSED.
7. THE EXISTING CONTOUR LINES SHOWN WERE ESTABLISHED FROM THE 1966 15' QUADRANGLE MAP AND THE FLOOD PLAIN AND FLOOD-WAY FROM 2015 FJ.R.M. MAPS.



CONDITIONAL USE PERMIT

SCALE: 1" = 100'-0"

0 50 100 200 300



WESTWICK
CONDITIONAL USE PERMIT
LAURENCE, KANSAS

PROJECT # 211-710

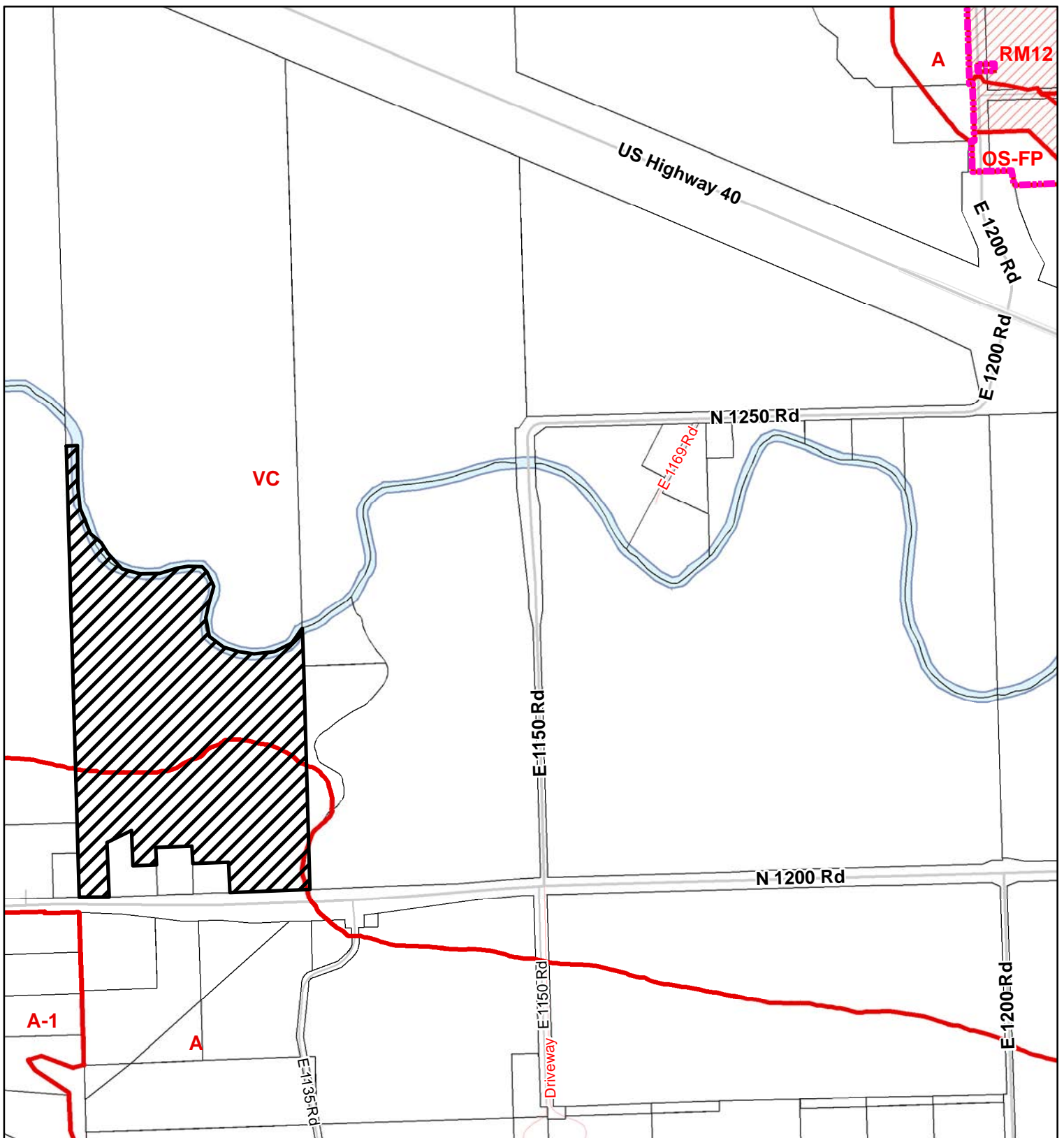
RELEASE: 1.0 DATE: 11.20.17

paulwerner
ARCHITECTS

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CUP-18-00501: Consider a Condition Use Permit for a rugby/soccer complex, located northwest of the intersection of N. 1200 ROAD & E. 1150 ROAD.



PLANNING COMMISSION REPORT
Regular Agenda – Public Hearing Item

PC Staff Report
12/19/18

**ITEM NO. 7 SPECIAL USE PERMIT/INSTITUTIONAL DEVELOPMENT PLAN FOR
DOUGLAS COUNTY TIER II AND TIER III HOUSING 1000 W. 2ND
STREET (SLD)**

SUP-18-00518 Consider a Special Use Permit/Institutional Development Plan for the development of residential housing to include a 12 bedroom *Group Home* and 10 one-bedroom apartments, located at 1000 W. 2nd Street. Submitted by TreanorHL, for Douglas County, property owner of record.

STAFF RECOMMENDATION: Planning Staff recommends approval of a Special Use Permit located at 1000 W. 2nd Street and forwarding the request to the City Commission with a recommendation of approval, subject to the following condition:

1. Prior to recording of the Institutional Development Plan with the Register of Deeds Office the applicant shall provide a photometric plan to the Planning staff for review and approval per Section 20-1103 of the Land Development Code.

Applicant's Reason for Request: *The project proposes to remove the existing structure in favor of a 12-bed assisted living building and ten single-bed units arranged in three attached dwellings, for independent living in order to provide addiction recovery.*

ASSOCIATED CASES/OTHER ACTION REQUIRED

Associated Cases

PF-14-00208

Other Action Required

- City Commission approval of special use permit and adoption of ordinance.
- Publication of special use permit ordinance.
- Submission and approval of local floodplain development permit.
- Submission of an erosion control plan.
- Submission and approval of building permit.

PLANS AND STUDIES REQUIRED

- *Traffic Study* – Pending provision of additional documentation requested by city staff.
- *Downstream Sanitary Sewer Analysis* – Pending provision of additional documentation requested by city staff.
- *Drainage Study* – Pending provision of additional documentation requested by city staff.
- *Retail Market Study* – Not applicable to this development application.
- *Historic or Community Design Guidelines Review Standards* – Not applicable to this development application.
- *Alternative Compliance/Waivers* –
 - Waiver from implementation of specific stormwater best management practices to accommodate excess parking spaces.
 - Waiver from 40 foot building setback from W. 2nd Street.
 - Waiver from Type 3 Bufferyard along W. 2nd Street.

- Waiver to defer the construction of public sidewalk along Maine Street until construction of *Community Mental Health Facility*.

KEY POINTS

- This zoning district is subject to a requirement to provide an *Institutional Development Plan* per Section 20-1307 of the Land Development Code for all property contained in the district.
- The residential component requires a special use permit in this district.
- The east portion of the lot is encumbered by the regulatory floodplain. Proposed improvements are outside of the floodplain boundary.

ATTACHMENTS

- A. Site Plan
- B. Building Elevations
- C. Conceptual Site Design

PUBLIC COMMENT RECEIVED PRIOR TO PRINTING

None to date

GENERAL INFORMATION

Current Zoning and Land Use: GPI (General Public and Institutional) District. Former *Lodge* building use, now vacant.

Surrounding Zoning and Land Use: To the north: GPI (General Public and Institutional) District. Sandra Shaw Community Health Park.

To the east: GPI (General Public and Institutional) District IG (General Industrial) District. Existing field staff parking and lime residual pump station¹.

To the south: GPI (General Public and Institutional) District and RM12 (Multi-Dwelling Residential) District.

- 200 Maine Street – County Health Department, Visiting Nurses, and Bert Nash offices
- 720 W. 3rd Street – Lawrence Kaw Water Treatment Plant.
- 200 block of Illinois and Alabama Streets include *Detached, Duplex* and *Multi-Dwelling* residential uses.

To the west: IG (General Industrial) District and OS (Open Space) District to the southwest. Existing *Manufactured Home Park* – Mobile Village and Woody Park.

¹ Lime is used in the water treatment process and the leftover lime sludge is sent to the Kansas River Waste Water Treatment Plant, located at 1400 E. 8th Street, via the lime residual pump station.

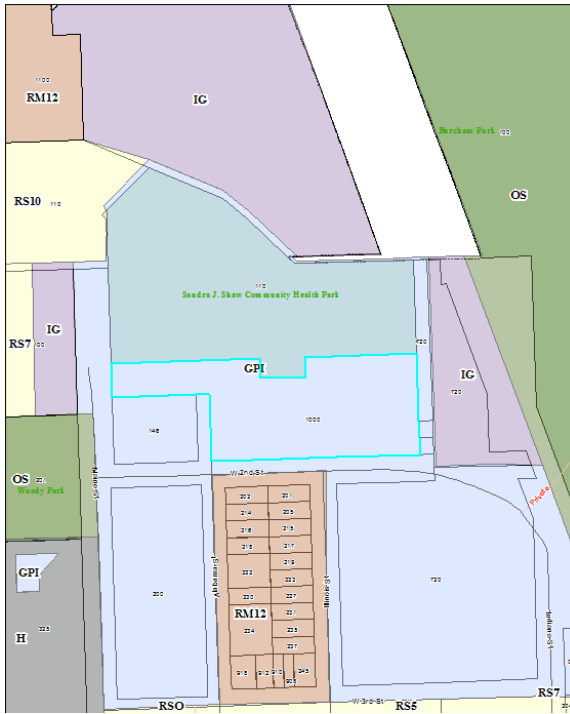


Figure 1: Surrounding Zoning

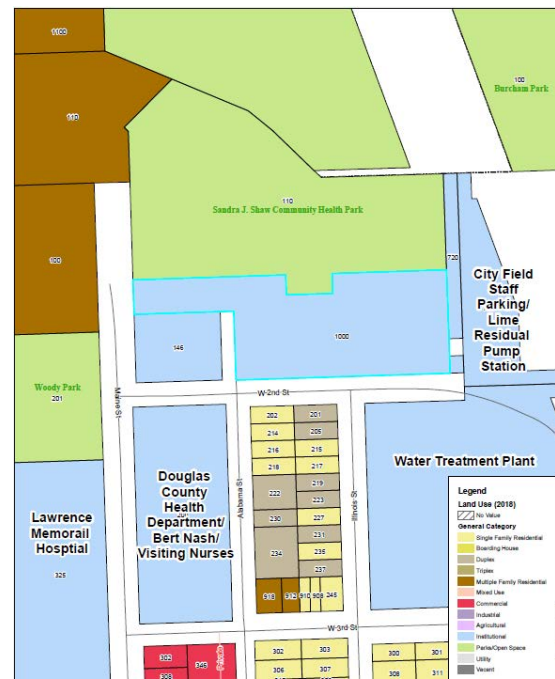


Figure 2: Existing Land Use

PROCESS SUMMARY

An institutional development plan is required for properties zoned GPI (General Public and Institutional) District. Generally, developments less than 10 acres may complete planning requirements as a site plan. However, *Multi-Dwelling Residential* uses and *Group Home*, *General* uses in the GPI District require a special use permit. The plan provides a vision for the long-term use and development of public institutional space and lands so that they are designed to be compatible with surrounding land uses and contribute to the neighborhood and character of the area. These development are processed in the same manner with the same notice and documentation requirements as either a site plan, when the area is less than 10 acres or as a special use permit when more than 10 acres is included in the plan boundary or when a use in the GPI District requires a special use permit.

- The total land area included in this request is less than 10 acres.
- *Group Home* and *Multi-Dwelling Residential* uses are subject to Special Use Permit review approval when located in the GPI District.
- The property included in the application addresses only the residential component of the development at this time.

The site plan submitted with a special use permit application is required to meet the site plan requirements in Section 20-1305(f) of the Development Code. Once approved, Institutional Development Plans are recorded with the Douglas County Register of Deeds Office.

PROJECT SUMMARY

The property is located on the north side of W. 2nd Street. The property is located north of the Bert Nash Center/Lawrence Douglas County Health Department and east of the Lawrence Memorial Hospital. Municipal operations are located to the east and southeast of the proposed development.

The development project includes an initial phase for residential use as part of a larger planned improvement providing community-wide mental health services. This phase is initiated on behalf of

the Lawrence-Douglas County Housing Authority and Douglas County. The project includes a 12-bedroom *Group Home* and 10 *Multi-Dwelling Residential* units in multiple buildings on a single parcel. The project is not designed or intended to be divided for separate ownership of the units or buildings.

A future phase is planned for the property to the west that will include a *Community Mental Health Facility* and an expanded parking lot. The future phase would require the demolition of existing structures (USD 497 Maintenance facility) located at 146 Maine Street. Douglas County acquired the property with the intent to construct the "Crisis Center" as part of the overall campus development that includes this residential phase.

The *Community Mental Health Facility* is a permitted use in the GPI District and subject only to a site plan. At this time, the school district property is not included in development application and is a separate parcel. A future minor subdivision application is expected that will combine the school district property with the proposed development and include the vacation of the remaining Alabama Street right-of-way.

SITE PLAN REVIEW

Review and Decision-Making Criteria (Land Development Code Section 20-1306(i))

1. WHETHER THE PROPOSED USE COMPLIES WITH ALL APPLICABLE PROVISIONS OF THIS DEVELOPMENT CODE

Applicant's Response: *The proposed use complies with all applicable provisions of the Development Code.*

This section of the staff report evaluates the site plan component of the application for compliance with density and dimensional standards, access, off-street parking, and landscape design standards.

A. Site Summary

This property is a single platted lot with an existing building that will be removed as part of the redevelopment of the site.

Table 1: Site Summary

Site Summary: Proposed Lot 1, Research Park Addition			
Total area:	Existing	Proposed	The property is platted as a single large lot with a substantial portion on the northeast portion located within the regulatory floodplain. No development improvements are proposed in the encumbered area of the lot.
Lot Size: Square Feet	207,607	207,607	
Acres	4.76	4.76	
Building: Square Feet	6,237	9,525	The maximum allowed impervious surface cover by code is 75%. The proposed development complies with the design criteria.
Impervious Cover: Square Feet	12,851	24,508	
Percent	6%	12%	
Pervious Cover: Square Feet	194,756	183,101	2.3 units per acre
Percent	94%	88%	
Residential Density	0	11 units	

The GPI District does not specify a maximum residential density. As a project subject to an Institutional Development Plan, the density may be determined for the proposed development. The nearest residential zoning is RM12 located to the south, RM12. This district permits a maximum development of 12 dwelling units per acre. The 200 block of Alabaman/Illinois Street is developed at 11 dwelling units per acre with a mix of *Detached Dwellings*, *Duplex*, and *Multi-Dwelling* residential

structures. The proposed development is substantially less dense than the surrounding residential area.

The project retains a substantial portion of open space and is adjacent to a public park.

The project complies with these density and dimensional standards per section 20-601.

B. Access and Parking

Access: Access to this site is provided by a driveway to N. 2nd Street. This segment of N. 2nd Street is a local street.

Off-site Parking: The proposed development includes a surface parking lot.

The project includes two uses arranged to create a supportive residential campus. Uses include:

- Multi-Dwelling Residential with parking required at 1 space per bedroom plus 1 space per 10 units; and
- Group Home, General with parking required at 1 space + 1 space per employee (at maximum shift).

The *Multi-Dwelling* units will require a total of 14 off-street parking spaces to accommodate the proposed 10 units. The development includes a total of 12 bedrooms. The plan shows 24 parking spaces.

Table 2: Parking Summary

Use	Vehicle Parking Requirements	Spaces Required	Spaces Provided
<i>Multi-Dwelling</i>	1 space per bedroom and 1 space per 10 units	13 spaces	
	10 units 12 total bedrooms		
	Bicycle Parking: 1 space per 4 auto spaces	3.25 spaces	4 spaces
<i>Group Home, General</i>	1 space and 1 space per employee (maximum shift)	5 spaces	
	1 unit 4 employees		
Total Spaces Required	Bicycle Parking: <i>None required for this use</i>	0 spaces	0 spaces
Total Spaces Provided			24 spaces
Accessible Spaces	1 accessible space for lots with 1-25 spaces	1 space	1 space

Parking Lot Design:

The site plan shows excess off-street parking; 18 spaces required and 24 proposed. The site exceeds the minimum required off-street parking by 6 spaces (33% of the required parking). Section 20-901 (c) requires the mitigation of additional impervious surface that results from excess parking through the implementation of stormwater best management practices. The applicant is requesting a waiver from this requirement. The applicant believes that the excess parking is necessary to meet demand and therefore not subject to mitigation required per Section 20-901 (c).

Applicant Justification: The additional parking spaces will provide parking on-site to accommodate some overflow parking associated with support service providers that are also located in the immediate area.

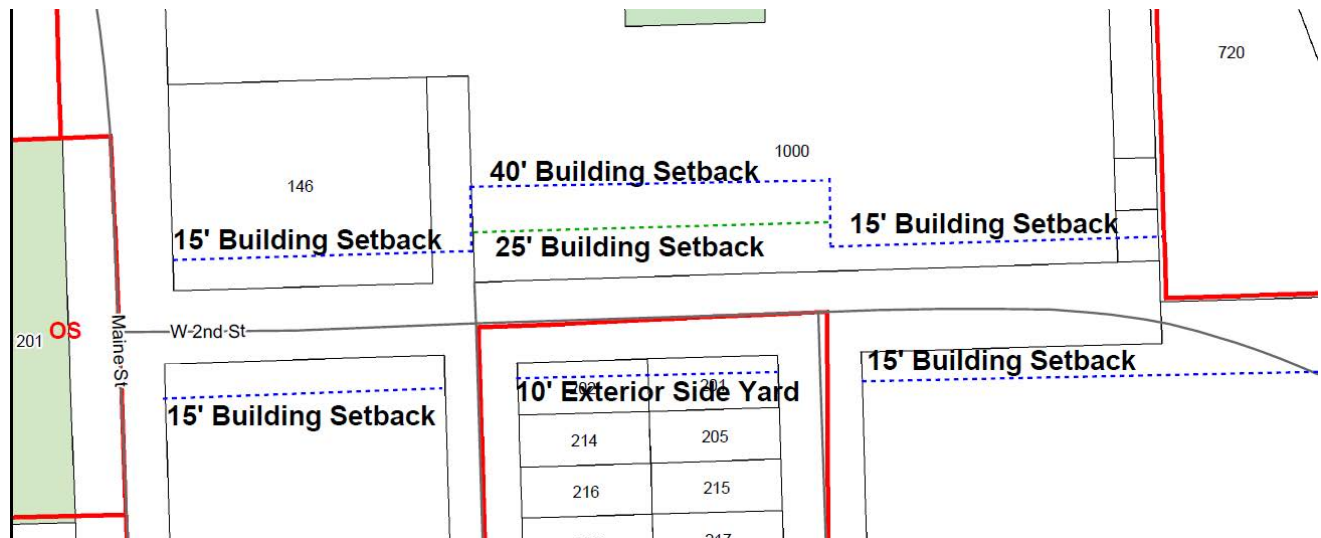
Staff Response: Staff acknowledges the applicant's request for additional parking. There is an acknowledged lack of sufficient parking for the medical office uses located in this immediate area. The additional parking will reduce the need for on-street parking that overflows onto the adjacent

residential streets. This application represents one phase of a larger redevelopment project that includes a multi-bed mental health facility that will have additional parking demand. The proposed parking lot would be expanded to provide the additional parking for the future development phase.

C. Design Standards

Site Design:

This phase is proposed for residential development with *Multi-Dwelling* units clustered around a common courtyard. The *Group Home* use is located to the west of the multi-dwelling units with interior pedestrian connections between the building and to W. 2nd Street. The site plan shows a 25 foot setback from W. 2nd Street. The GPI district requires a 40 foot setback when across from another residential district. As noted above the zoning district along the south side of W. 2nd Street includes an RM zoning. This setback applies to approximately 300 foot of the frontage along W. 2nd street where the project abuts the residential development between Illinois and Alabama Street. The remaining frontage requires only a 15 foot building setback. The corresponding building setback on the south side of W. 2nd Street along this same block face is 10 feet, as an exterior side yard setback for the RM12 District. Otherwise the building setback is 15 feet. The graphic below shows the required building setbacks along W. 2nd Street.



The applicant has submitted a revised site plan showing a reduced setback for that segment of the property abutting the RM zoning. The proposed 25 foot setback provides a comparable residential building setback for the development in the context of the residential uses to the south. The existing residences to the south include detached, duplex, and multi-dwelling uses that face Illinois or Alabama Street. W. 2nd Street is the exterior side yard for the two lots at the north end of the block.

Section 20-1305 (b) (3) (v) provides the Planning Director authority to waive compliance with certain standards "when good cause is shown", when it can be demonstrated that the intent of the code is fulfilled, and if the project meets sound site planning principles.

The purpose of the larger building setback in the GPI District when located adjacent to a residential district is to establish an appropriate separation of uses with the ability to provide an adequate bufferyard. In this application the proposed use is *multi-dwelling residential*. The residential use is appropriately integrated into the surrounding neighborhood fabric and should not be isolated from it by artificial constructs. The reduced setback allows the project to be shifted to the south, preserving the mature vegetation on the north side of the property adjacent to the Sandra Shaw Park.

The proposed *multi-dwelling* units are provided with front porches and rear patios as private space and a common courtyard for joint open space within the development. The *group home* use is setback 145 feet from W. 2nd Street. The back yard of the building looks out over the adjacent park space to the north. The reason for the deep setback for the *group home* use is to provide convenient off-street parking.

The parking lot for the group home use and the *Multi-Dwelling* use is located to serve both uses and will be expanded to the west as the *Community Mental Health Facility* phase is developed. The parking lot complies with the minimum required parking lot setback for the GPI District.

The site design reflects the preservation of the floodplain and stand of mature trees located along the north and east sides of the property.

Building Elevations:

The proposed development is residential. The Community Design Manual does not currently include residential design standards. The proposed building elevations include strong residential components intended to reflect residential characteristics of the neighborhood.



Figure 3: Front Facade - Group Home



Figure 4: Front Facade - Multi-Dwelling

Pedestrian accessibility:

The site plan shows interior pedestrian connections between the residential units and from W. 2nd Street. A portion of the lot abuts Maine Street and a public sidewalk is not shown in this phase along Maine Street. The future development phase will include the public sidewalk and extend interior connections.

Maine Street sidewalk

Typically, institutional development plans less than 10 acres are required to be developed in one phase. Sidewalks required in a multi-phase development must be provided in phase 1. This application is unique in that it is less than 10 acres but is processed the same as if it were larger because the residential component is subject to a special use permit.

Sidewalks are required per Section 20-1105 of the Land Development Code for any Major Development Project; however, the City Engineer may vary width and sidewalk construction standards when impractical because of topography or site conditions. In this instance all of the development is located on the east side of the property. There are no planned internal pedestrian connections between this phase and Maine Street at this time. A future application is expected that will include development on the west portion of the property for the future *Community Mental Health Facility* that will be required to extend the public sidewalk along W. 2nd Street and Maine Street. At this time, only 100 feet of the west property line abuts Maine Street. Construction of the Maine Street sidewalk is postponed until the construction of the *Community Mental Health Facility* in the near future.

D. Landscaping and Screening

Minimum Residential Open Space: Section 20-601 requires a minimum of 50 square feet of open space per dwelling unit when located in an RM (Multi-Dwelling) District. The GPI District does not specify a minimum open space area when residential uses are included. The site plan shows a 54 foot by 62 foot courtyard area (3,348 square feet) that creates a common open space for the *Multi-Dwelling* units that front the space. A conventional multi-dwelling development would require only 500 square feet. The proposed development far exceeds this design standard.

Street Trees: This site abuts W. 2nd Street and Maine Street. This phase of the development is located east of Alabama Street and does not include site changes along Maine Street at this time. The site requires a minimum of 19 shade trees per Section 20-1002; 16 trees along W. 2nd Street and 3 trees along Maine Street. The site plan shows existing vegetation along the west property line that will remain until the commencement of Phase 1. Most of the vegetation is located within the existing Maine Street right-of-way. New trees are not proposed at this time.



Figure 5: West portion Lot 2, Bert Nash Addition

The plan shows 15 trees along the street frontage. Trees are adjusted to accommodate the access drive and retain adequate sight distance at the entrance to the development.

Interior Parking Lot Landscaping: This project includes interior green space that exceeds the minimum required area.

Table 3: Interior Landscape Area Summary

Interior Landscape	Required	Provided
Total Area	Landscaping Interior Parking Lot Area: # Spaces x 40 square feet	
	24 spaces x 40 = 960 square feet	2,469 square feet
Total Interior Landscape Plants	Proposed 24 stalls/10 = 2.4 x 1 = 2.4 (3) Trees	5 Trees
	Proposed 24 stalls/10 = 2.4 x 3 = 7.2 (8) Shrubs	15 Shrubs

Perimeter Parking Lot Landscaping:

Parking is shown located 15 feet setback from the property line, consistent with the district requirement. Parking spaces are appropriately screened.

Bufferyard Requirements:

The block between Illinois Street and Alabama Street on the south side of W. 2nd Street is zoned RM12. As such a Type 3 bufferyard is required along the segment of the development. A Type 3 bufferyard can range from 15 to 25 feet wide. A 15 foot wide bufferyard would include a fence, wall, or berm. A berm in this location would not be suitable given the relationship of the proposed use (residential) to the uses to the south, also residential. The site plan does not include a fence or wall. The buildings are setback 25 feet with the off-street parking setback 15 feet. Appropriate perimeter landscaping is provided.

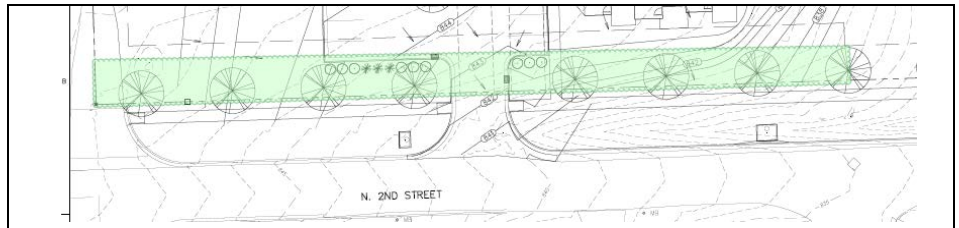


Figure 6: Type 3 Bufferyard Area

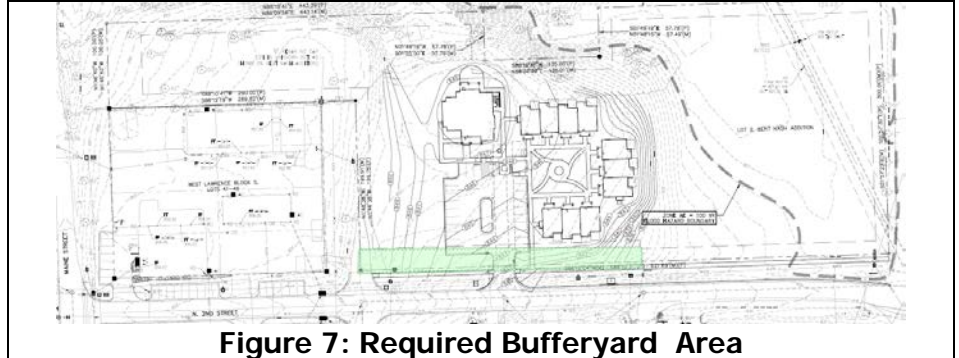


Figure 7: Required Bufferyard Area

The intent of the development is to provide suitable housing including support services with an integrated relationship to the existing residential uses in a neighborhood context. This standard applies only to 330 feet of the project. The total project including phase 1 has 911 feet of frontage along W. 2nd Street. Only the middle third of the overall site would be required to meet this standard.

By adding a fence or wall along this segment the development becomes isolated from the surrounding neighborhood. The intent of the standard is to buffer incompatible uses and provide screening and transition between differing uses.

Mechanical and Equipment Screening

The site plan includes a note indicating the project is subject to screening requirements per section 20-1006 of the Land Development Code. This applies to roof and building mounted equipment as well as trash storage areas. The mechanical systems have not yet been designed for this project. The applicant is advised of the requirement. Staff will continue to review this element to ensure compliance with Section 20-1103 of the Land Development Code as noted on the face of the drawing.

A common trash enclosure is provided in the northeast corner of the parking lot and will serve the residential units. The enclosure includes gates that will provide screening of the dumpster from W. 2nd Street.

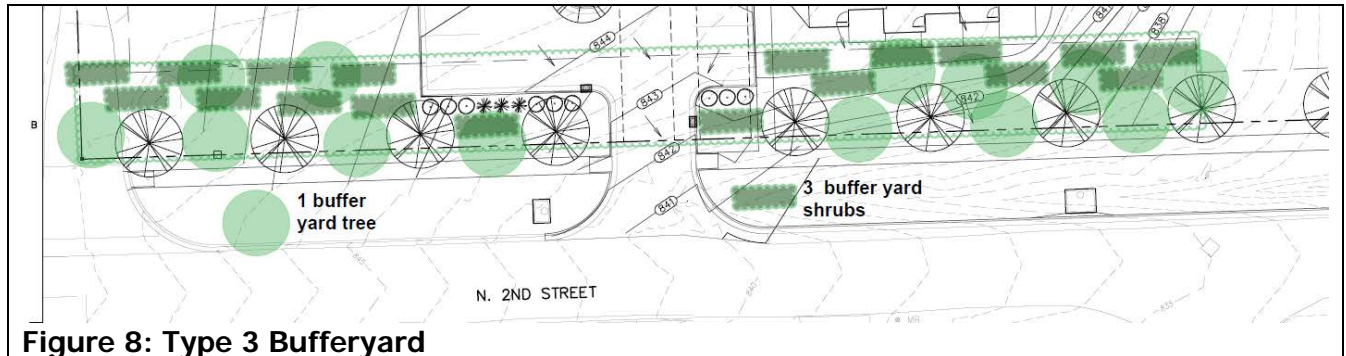
Alternative Compliance:

The proposed development complies with the required landscape standards with the following two exceptions.

1. Street trees along W. 2nd Street: The landscape plan shows 15 street trees rather than the required 18.
2. Bufferyard that is partially 15 feet wide (along the parking lot segment) that does not include a wall, fence, or berm, and does not include additional trees and shrubs. To meet the

minimum landscape standard for a bufferyard the site would require 13 additional trees and between 50 and 66 additional shrubs along this portion of the site.

It is possible to add additional street trees to meet the requirement. The additional screening is excessive and unreasonable if the development is intended to have a connection to the neighborhood.



Per Section 20-1305 (b) (3) (v), the Planning Director waives the required type 3 bufferyard for this project as it applies to the residential component of the proposed development.

E. Lighting

A photometric plan is required. Lighting details are not available since fixtures have not been selected. Additional review of the lighting plan will be required prior to the release of the site plan for issuance of building permits.

F. Sensitive Lands / Floodplain

This property is partially encumbered by the regulatory floodplain. Proposed improvements are located outside of the floodplain. The site also includes a mature stand of trees located along the north side of the property that abuts the public park. Only minor encroachment into the mature trees is proposed to accommodate the residential units along the north side of the development. The existing green space provides a connection to the public open space to the north and provides some privacy for the residents from park users.

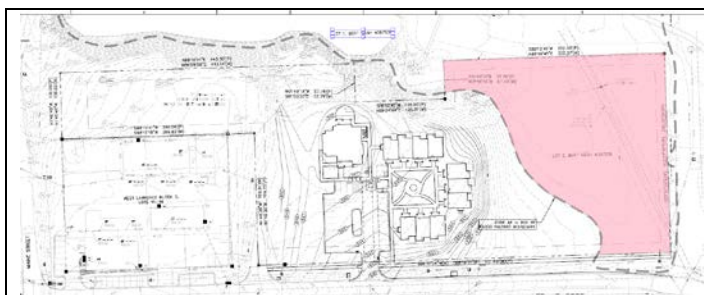


Figure 9: Regulatory Floodplain

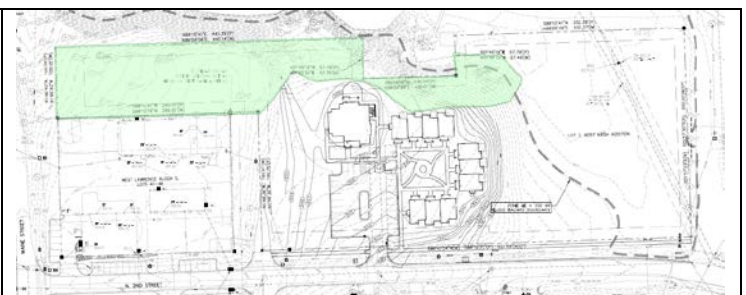


Figure 10: Existing Vegetation Buffer

Staff Finding – This use, as conditioned, complies with the applicable provisions of the Land Development Code as an allowed use in the GPI District subject to a special use permit.

2. WHETHER THE PROPOSED USE IS COMPATIBLE WITH ADJACENT USES IN TERMS OF SCALE, SITE DESIGN, AND OPERATING CHARACTERISTICS, INCLUDING

HOURS OF OPERATION, TRAFFIC GENERATION, LIGHTING, NOISE, ODOR, DUST AND OTHER EXTERNAL IMPACTS

Applicant's Response: *The proposed use is compatible with adjacent uses. The area is generally residential and health care oriented. The proposed use will be architecturally residential, and the use is for addiction recovery, which is a health oriented service.*

The property is located adjacent to a public park on the north and multiple public facilities to the east, west, and south. Residential uses are generally one and two story buildings. Public facilities include the recently vacated building that provided support services for USD 497, Bert Nash, Lawrence-Douglas County Health Department, and the City water treatment facility. The Lawrence Memorial Hospital is located southwest of the proposed development. Building size and massing for the public facilities is substantially different from the residential uses along the south side of W. 2nd Street.

This phase of the development continues the residential aspect of the neighborhood to the north side of W. 2nd Street. The proposed use is compatible with the surrounding area.

Staff Finding – The proposed use is compatible with the adjacent uses in terms of size, massing, orientation, as a residential development. The hours of operation and traffic will not be substantively different than the existing residential uses to the south.

3. WHETHER THE PROPOSED USE WILL CAUSE SUBSTANTIAL DIMINUTION IN VALUE OF OTHER PROPERTY IN THE NEIGHBORHOOD IN WHICH IT IS TO BE LOCATED

Applicant's Response: *The proposed use removes an older unused structure in favor of new residential looking buildings. The improvements will likely increase property values as the property will be improved to Code requirements.*

The proposed development represents a reinvestment in the neighborhood and in the larger community.

Staff Finding – Substantial diminution of other property values in the area is not anticipated.

4. WHETHER PUBLIC SAFETY, TRANSPORTATION AND UTILITY FACILITIES AND SERVICES WILL BE AVAILABLE TO SERVE THE SUBJECT PROPERTY WHILE MAINTAINING SUFFICIENT LEVELS OF SERVICE FOR EXISTING DEVELOPMENT

This site is currently served by public utilities. The proposed parking lot is designed to accommodate the *multi-dwelling residential* and *group home* use in an efficient manner. The future phase of the development will expand the parking available in this area.

Improvements to infrastructure are required as the property is redeveloped including the addition of a public sidewalk along W. 2nd street as associated with the current phase for development.

Staff Finding – Adequate public facilities and transportation access is accommodated for this development. Development will include public improvement plans for necessary improvements related to the redevelopment of the site.

5. WHETHER ADEQUATE ASSURANCES OF CONTINUING MAINTENANCE HAVE BEEN PROVIDED

The proposed request provides an enforceable tool to address the use and continued maintenance of the property with regard to landscaping, exterior activity, and off-street parking. As an

Institutional Development Plan, this application provides a master plan that answers the community expectation for redevelopment of the property. Alternative uses would require a new plan.

Staff Finding – Adequate assurances of continued maintenance are inherent in the use and the special use permit approval process.

6. WHETHER THE USE WILL CAUSE SIGNIFICANT ADVERSE IMPACTS ON THE NATURAL ENVIRONMENT

Applicant's Response: *The proposed use will have no adverse impact on the natural environment. There is nothing proposed in this development that could be deemed detrimental to the environment.*

A portion of the property is encumbered by the regulatory floodplain. The proposed improvements are located outside of regulatory floodplain area. A mature vegetative buffer is located along the north property line. The site plan shows minimal disturbance of the existing tree line.

Staff Finding – The proposed development includes regulatory controls to protect the significant natural features of this site.

7. WHETHER IT IS APPROPRIATE TO PLACE A TIME LIMIT ON THE PERIOD OF TIME THE PROPOSED USE IS TO BE ALLOWED BY SPECIAL USE PERMIT AND, IF SO, WHAT THAT TIME PERIOD SHOULD BE

This special use permit is required to accommodate this specific use and related improvements in the GPI district. The intention of the development and purpose of the base zoning district is to accommodate public and institutional uses to ensure compatibility and integration with the surrounding neighborhood.

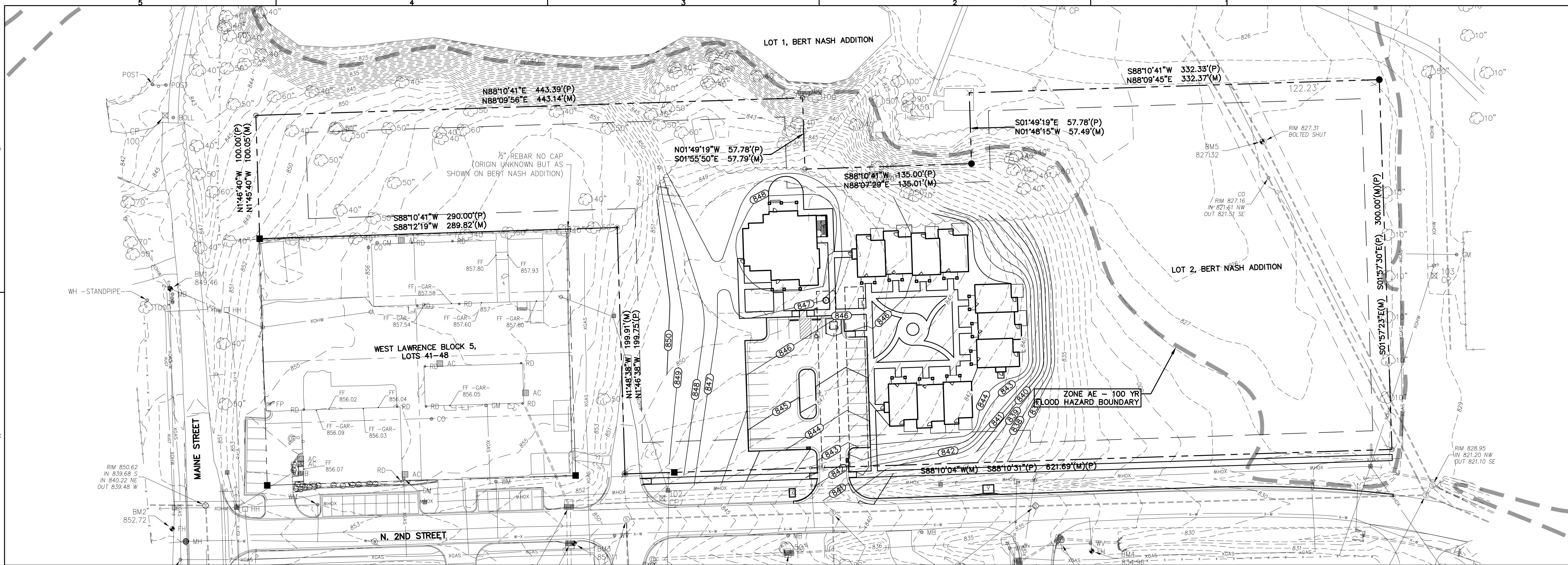
The proposed development represents a significant community investment. As an Institutional Development Master Plan a time limit is not appropriate or required for this project.

Staff Finding – Staff does not recommend a time limit on the special use permit.

CONCLUSION

Overall, the project complies with the Land Development Code and the development pattern of the surrounding neighborhood.

FILE PATH: M:\JS16.001.00B Douglas County Crisis Center\3_Drawings\1_AutoCAD\Sheets\SITE PLAN\SITE PLAN.dwg
DATE PRINTED: Monday, December 03, 2018
LAST SAVED: Monday, December 03, 2018
PRINTED BY: Brian Kemp



LEGAL DESCRIPTION
LOT 2 IN BERT NASH ADDITION, IN THE CITY OF LAWRENCE, DOUGLAS COUNTY, KANSAS.

BENCHMARK
BM #1: RAILROAD SPIKE IN EAST FACE POWER POLE, ±150 NORTH OF INTERSECTION 2ND STREET & MAINE STREET
ELEV. = 849.46

PROJECT SUMMARY
CURRENT ZONING: GPI
PROPOSED ZONING: GPI
CURRENT USE: COMMUNITY FACILITIES: LODGE, FRATERNAL & CIVIC ASSEMBLY.
PROPOSED USE: HOUSEHOLD LIVING: MULTI-DWELLING STRUCTURE
GROUP LIVING: GROUP HOME, GENERAL
LAND AREA: 207,607 S.F. (4.766 ACRES)
PROJECT PROPOSED: A 12 UNIT GROUP LIVING HOME AND 10 PRIVATE LIVING UNITS FOR MENTAL HEALTH AND ADDICTION RECOVERY.

EXISTING CONDITIONS	PROPOSED CONDITIONS
TOTAL BUILDING: 6,237 SF	TOTAL BUILDING 9,525 SF
TOTAL PAVEMENT: 6,614 SF	TOTAL PAVEMENT 14,981 SF
TOTAL IMPERVIOUS: 12,851 SF	TOTAL IMPERVIOUS 24,508 SF
TOTAL PERVIOUS: 194,756 SF	TOTAL PERVIOUS 183,101 SF
TOTAL PROPERTY AREA: 207,607 SF	TOTAL PROPERTY AREA 207,607 SF

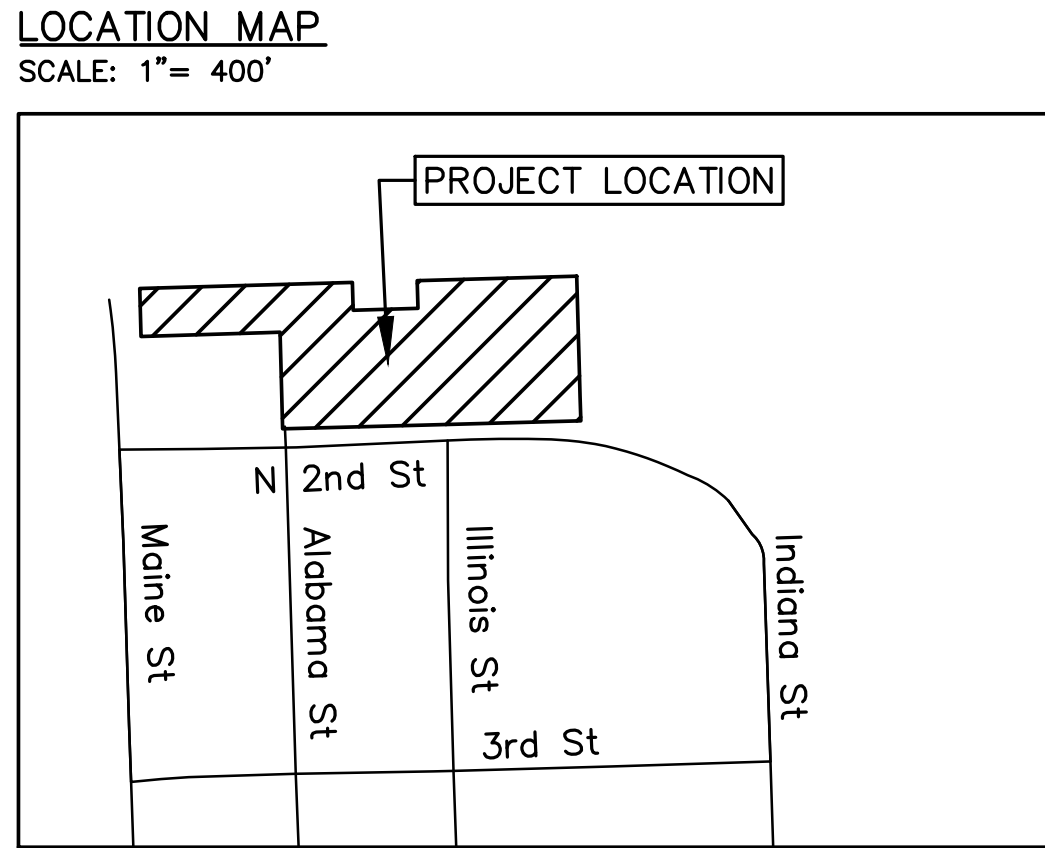
PARKING REQUIREMENTS
MULTI-DWELLING STRUCTURE: 1 STALL PER BEDROOM + 1 PER 10 UNITS.
12 BEDROOMS x 1 STALL + 1 PER 10 UNITS = 14 STALLS.
GROUP HOME, GENERAL: 1 STALL + 1 PER EMPLOYEE (AT MAX. SHIFT).
1 STALL + 1 PER EMPLOYEE AT MAX. SHIFT (4) = 5 STALLS.
BICYCLE PARKING REQUIRED: 1 PER 4 AUTO SPACES (MULTI-DWELLING USE).
14 SPACES / 4 = 4 BIKE PARKING SPACES.

TOTAL PARKING STALLS PROVIDED:
24 STALLS, INCLUDING 1 ACCESSIBLE STALL, AND 4 BIKE PARKING SPACES.

LANDSCAPING SCHEDULE AND NOTES					
SYM.	DESCRIPTION	QTY.	APPROVED TYPES	SIZE	COND.
	EXISTING TREES	SEE PLAN			
	SHADE TREES	2	ELM, LACEBARK, HORNBEAM, COLUMNAR MAPLE, AUTUMN BLAZE NORWAY MAPLE, THORNLESS HONEY LOCUST	2"-2 1/2" CAL	B & B
	ORNAMENTAL TREES	4	KOUSSA DOGWOOD, CORAL BURST CRABAPPLE, EASTERN REDBUD, CRAB, SARGENTINA	1 1/2"-2" CAL	B & B
	DECIDUOUS SHRUBS	12	BARBERRY, CRIMSON PIGMY BURNING BUSH, DWARF SPIREA, MAGIC CARPET NINE BARK, PURPLE LILAC, DWARF KOREAN VIBURNUM, ALLEGANY	18"-24" HT.	CONT
	EVERGREEN SHRUBS	12	DWARF SPREADING YEW, JUNIPER, SEA GREEN JUNIPER, CALLERY CARPET HOLLY, BLUE	24"-36" HT.	CONT

- REQUIRED INTERIOR LANDSCAPING AREA = 40 SURFACE SPACES*40 SQ. FT. PER SPACE = 960 SQ. FT.; 2,469 SQ. FT. PROVIDED.
- REQUIRED INTERIOR PARKING LOT LANDSCAPING = 1 TREE AND 3 SHRUBS PER 10 SPACES; 3 TREES, 8 SHRUBS; PROVIDED = 5 TREES, 15 SHRUBS.
- THERE MUST BE A MIN. OF (2) SPECIES USED IN EACH CATEGORY.
- ALL TURF AREAS TO BE SEED WITH K-31 FESCUE, SOD, OR AS ALLOWED BY CITY CODE.
- PARKING LOT ISLANDS WILL BE LANDSCAPED WITH A MINIMUM OF 2 OF THE FOLLOWING MATERIALS: TREES, SHRUBS OR GROUND COVER. GROUND COVER SHALL BE DROUGHT TOLERANT WITH VARIETIES SUCH AS DAYLILIES, ASTERS, HARDY ICE PLANT, ALPINE CARPET JUNIPER, HUGHES JUNIPER, AND ETC.

- GENERAL NOTES**
- BOUNDARY SURVEY PERFORMED BY CFS ENGINEERS, MARCH 2018.
 - TYPICAL SOIL TYPE: EUDORA-KIMO ASSOCIATION.
 - EXISTING UTILITY LOCATIONS, ELEVATIONS, AND SIZES ARE BASED ON INFORMATION AVAILABLE AT THE TIME THE SITE PLAN WAS PREPARED.
 - A PORTION OF THE PROPERTY IS LOCATED WITHIN A DESIGNATED "SPECIAL FLOOD HAZARD AREA" PER FEMA MAP NUMBER 20045C0176E, AS REVISED SEPTEMBER 2, 2015.
 - THIS SITE PLAN HAS BEEN DESIGNED TO COMPLY WITH THE PROVISIONS OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) FOR BUILDINGS AND FACILITIES, APPENDIX A TO 28 CFR, PART 36.
 - EXISTING CONTOURS GENERALLY INDICATE STORMWATER FLOW AT THE SITE.
 - SIGHT LIGHTING IS SUBJECT TO COMPLIANCE WITH SECTION 20-1103 "OUTDOOR LIGHTS".
 - TRASH WILL BE COLLECTED AT THE ENCLOSURE LOCATION SHOWN ON THE PLAN. THE CITY OF LAWRENCE WILL NOT BE RESPONSIBLE FOR PAVEMENT DAMAGE DUE TO REFUSE COLLECTION. ENCLOSURE TO COMPLY WITH CITY DESIGN CRITERIA.
 - MECHANICAL EQUIPMENT SHALL BE SCREENED PER CITY CODE 20-1006(B).
 - OUTDOOR AREA PROVIDED: 105 S.F. PATIO AND 66 S.F. PORCH AT EACH MULTI-DWELLING UNIT; 165 S.F. DECK, 186 S.F. PORCH, AND 495 S.F. PATIO AT GROUP HOME STRUCTURE.



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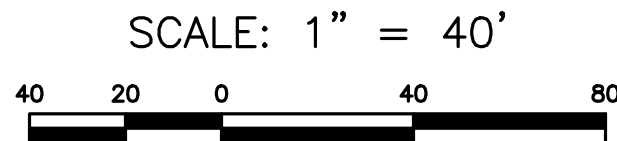
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LEGEND	
○	IRON PIN FOUND (AS NOTED)
●	IRON PIN SET (1/2" x 24" REBAR W/CAP OF&S CLS80)
XC	CONTROL POINT
U/E	UTILITY EASEMENT
(M)	MEASURED
(P)	PLATTED
RD	EXISTING ROOF DRAIN
AC	EXISTING AIR CONDITIONING UNIT
GM	EXISTING GAS METER
○	EXISTING STORMWATER MANHOLE
□	EXISTING STORMWATER INLET
○	EXISTING SANITARY SEWER MANHOLE
○	EXISTING SEWER CLEANOUT
○	EXISTING POWER POLE
○	EXISTING LIGHT POLE
EPED	EXISTING ELECTRICAL PEDESTAL
TE	EXISTING TELEPHONE BOX
TRAN	EXISTING TELEPHONE PEDESTAL
BM	EXISTING TRANSFORMER
WM	EXISTING WATER METER
WV	EXISTING WATER VALVE
FH	EXISTING FIRE HYDRANT
○	EXISTING GAS LINE
X-W	EXISTING WATER LINE
X-GE	EXISTING UNDERGROUND ELECTRICAL LINE
X-UT	EXISTING UNDERGROUND TELEPHONE LINE
X-OHW	EXISTING OVERHEAD ELECTRICAL LINE
X-OHP	EXISTING OVERHEAD TELEVISION LINE (CABLE)
X-FO	EXISTING FIBER OPTIC CABLE
FW	PROPOSED FIRE LINE
W	PROPOSED WATERLINE
SAN	PROPOSED SANITARY SEWER
STM	PROPOSED STORM SEWER
FH	PROPOSED WATER VALVE
○	PROPOSED FIRE HYDRANT
○	DIRECTION OF STORMWATER FLOW
840	EXISTING ELEVATION CONTOUR
840	PROPOSED ELEVATION CONTOUR
---	PROPERTY LINE OF SUBJECT PROPERTY
---	PROPERTY LINE OF SURROUNDING PROPERTY
▨	PROPOSED 7" CONCRETE PAVEMENT
▨	PROPOSED 5" ASPHALT DEPTH
▨	PROPOSED 4" CONCRETE PAVEMENT

Approved and Released

 Case No. _____
Approval Date: _____
Release Date: _____
Planner: _____
_____ of _____ Sheets
Asst./Director: _____



JS16.001.00B

Special Use Site Plan For

Douglas County Tier II and Tier III Housing

1000 W. 2nd Street, Lawrence, Kansas 66044

TREANORHL

1040 Vermont Street
Lawrence, Kansas 66044-2920
Office: 785.842.4858
Fax: 785.842.7536
www.TreanorHL.com

THIS SITE PLAN IS FOR
PLANNING PURPOSES ONLY
NOT FOR CONSTRUCTION

REVISIONS
12.3.18 CITY COMMENTS

Rel. Date: 10.15.2018

SPECIAL USE
PERMIT
SITE PLAN

SP-1

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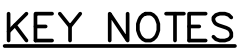
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2.3.18 CITY COMMENTS

SITE LAYOUT PLAN

SP-2

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- 1 CITY STANDARD CURB & GUTTER
- 2 ACCESSIBLE SIDEWALK RAMP
- 3 4" DEPTH CONCRETE SIDEWALK
- 4 ACCESSIBLE PARKING STALLS
- 5 7" DEPTH REINFORCED CONCRETE PAVEMENT
- 6 5" DEPTH ASPHALT PAVEMENT
- 7 4" WIDE WHITE PAINTED PARKING STALL MARKINGS
- 8 6' WOOD DUMPSTER ENCLOSURE (14' WIDE X 12' DEEP)
- 9 BICYCLE RACK
- 10 LANDSCAPED AREA - SEE SHEET 4
- 11 EXISTING ASPHALT TO REMAIN

Approved and Released

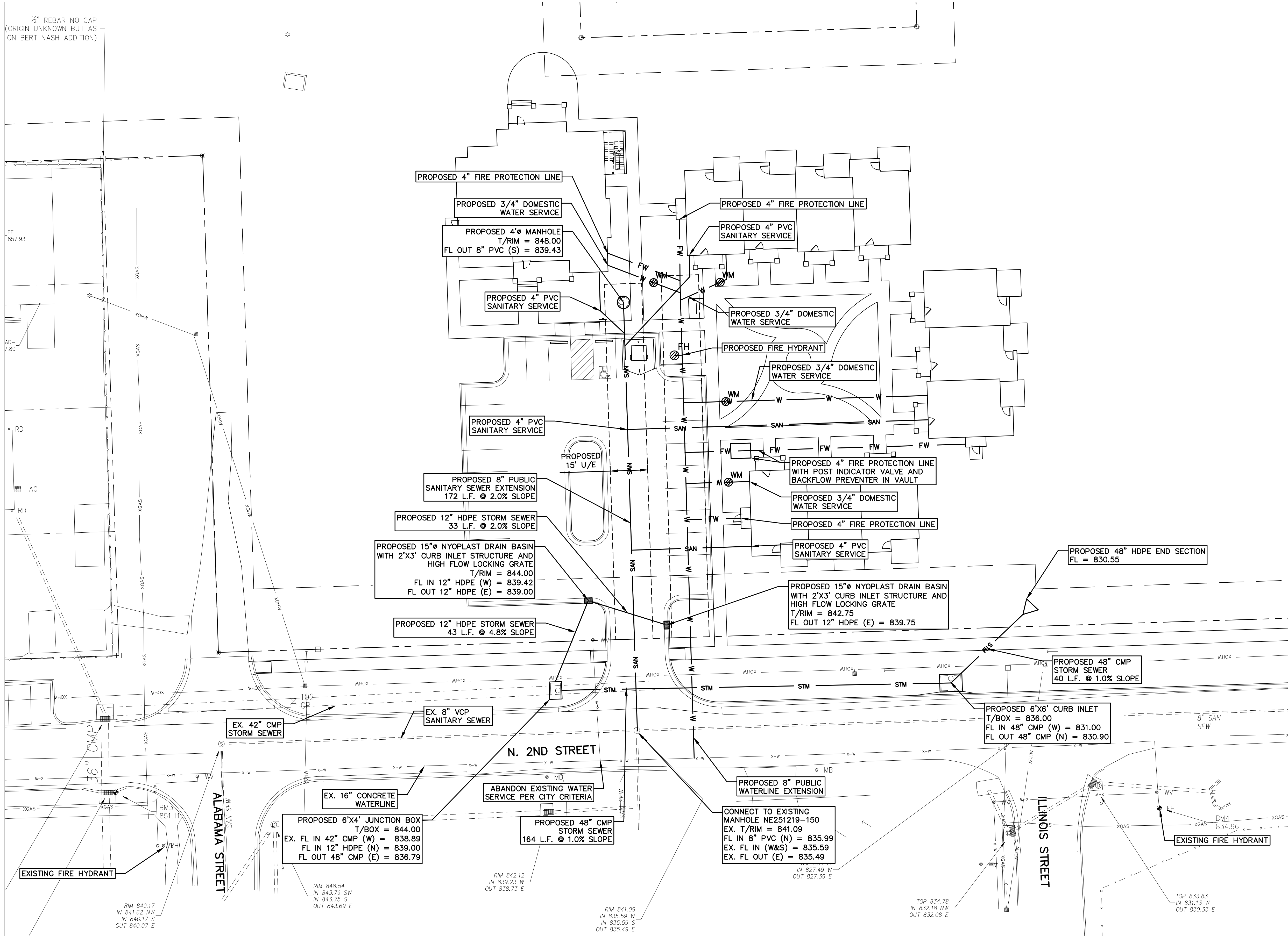
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Douglas County
PLANNING & DEVELOPMENT SERVICES

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SCALE: 1" = 20'

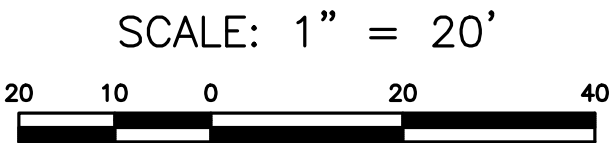


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Special Use Site Plan For
Douglas County Tier II and Tier III Housing
1000 W. 2nd Street, Lawrence, Kansas 66044

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12.3.18 CITY COMMENTS

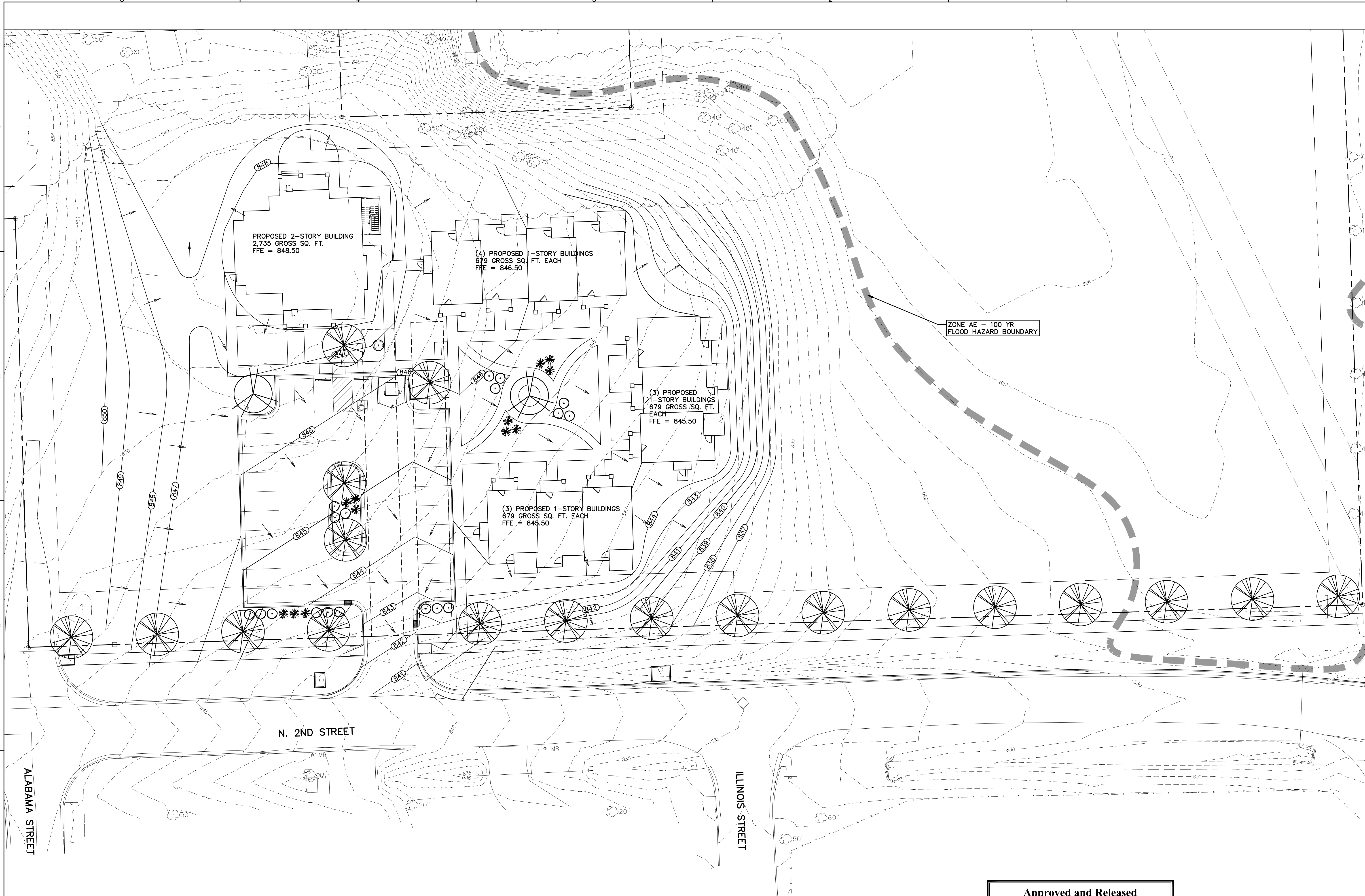
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UTILITY PLAN

SP-3

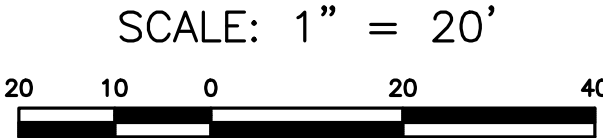
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Special Use Site Plan For
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1000 W. 2nd Street, Lawrence, Kansas 66044

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12.3.18 CITY COMMENTS

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LANDSCAPE AND
GRADING PLAN

SP-4

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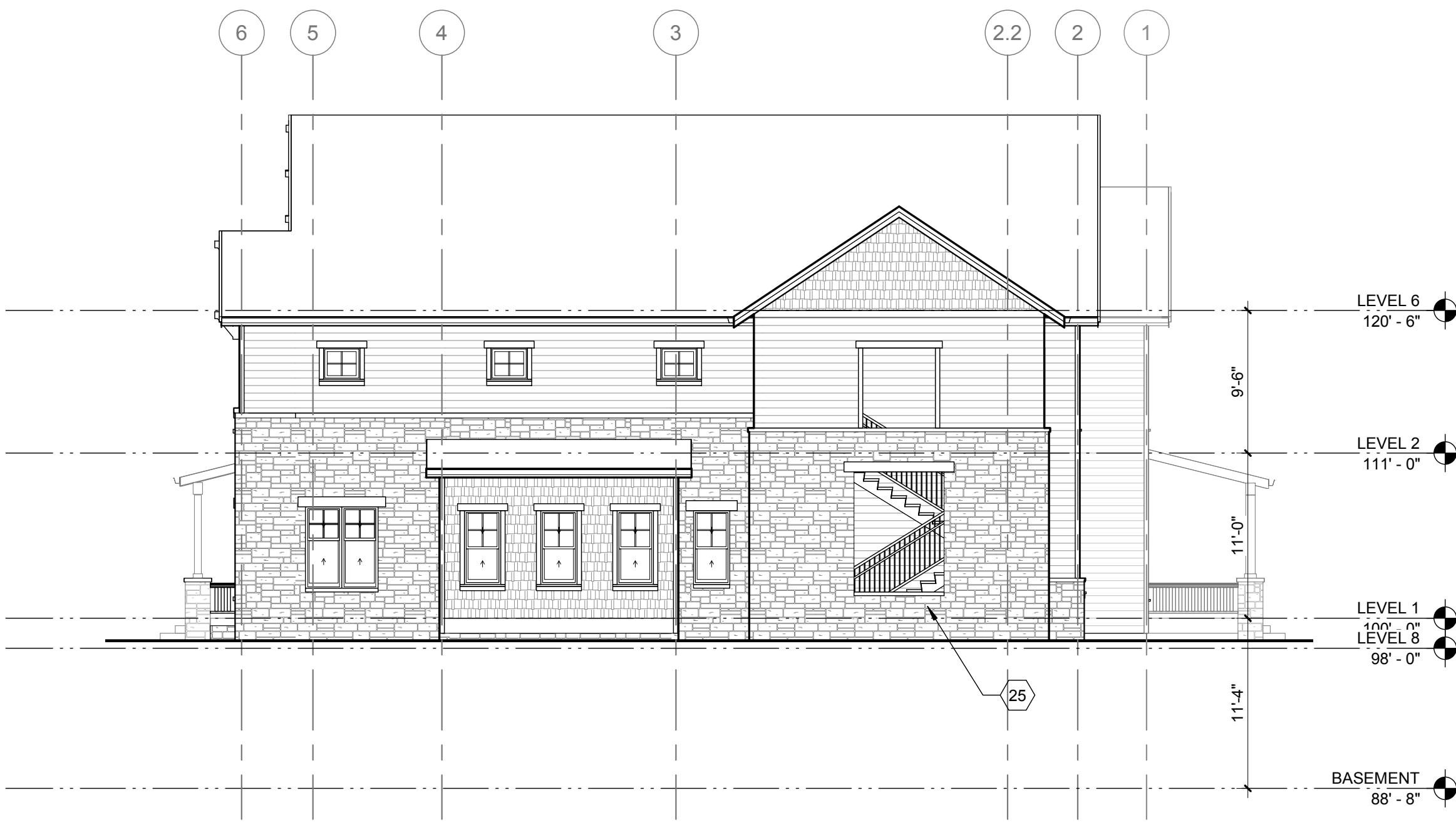
3D View 2 D3



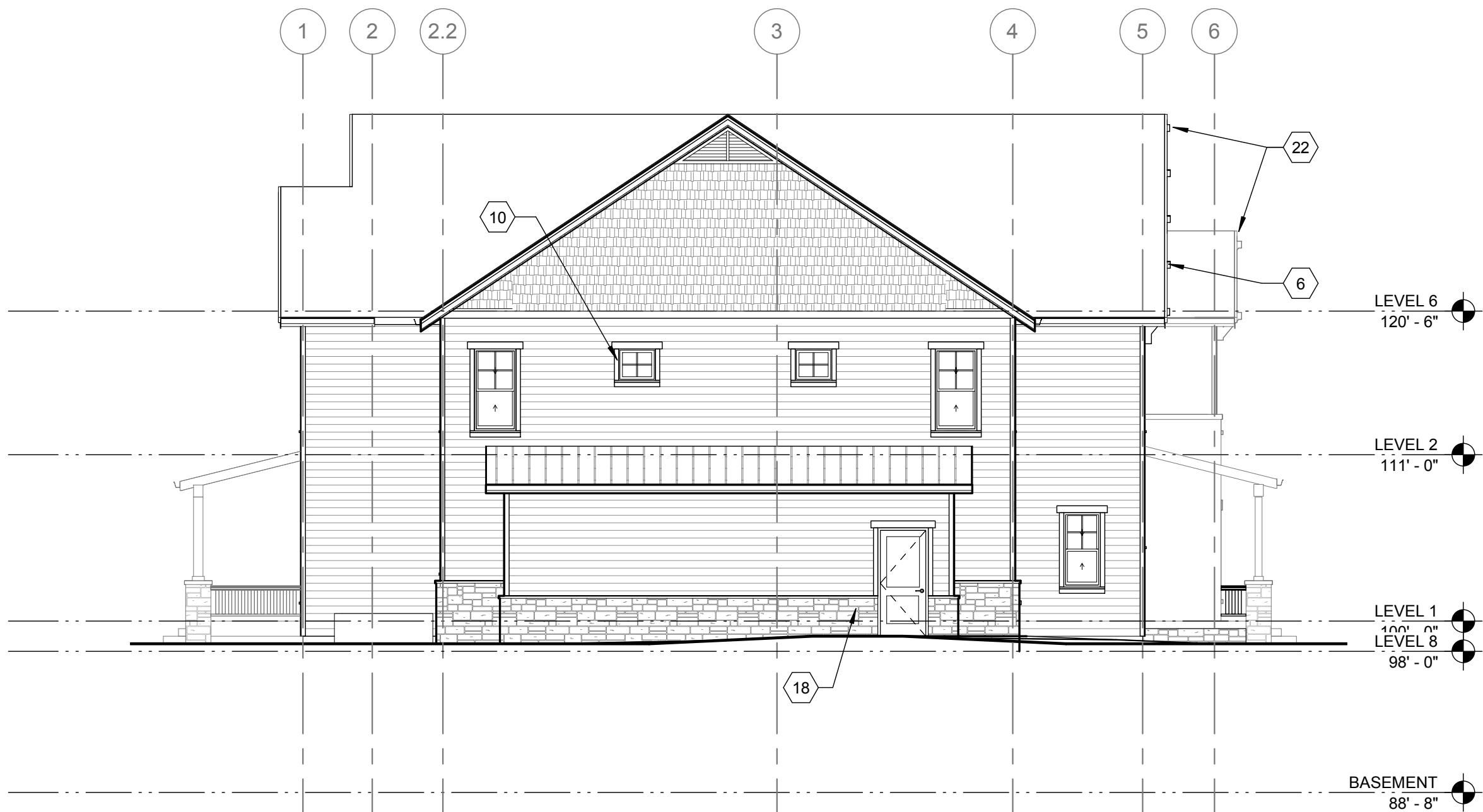
3D View 1 D1



NORTH B3
1/8" = 1'-0"



EAST C1
1/8" = 1'-0"



WEST A3
1/8" = 1'-0"



SOUTH A1
1/8" = 1'-0"

- ELEVATION NOTES**
- 1 FULL BED LIMESTONE VENEER
 - 2 PAINTED LAP SIDING
 - 3 STAINED CEDAR TIMBER FRAMING
 - 4 STANDING SEAM METAL ROOF
 - 5 COMPOSITE ASPHALT SHINGLES ATOP ICE & WATER SHIELD
 - 6 STAINED CEDAR OUTLOOKS
 - 7 CEDAR FASCIA
 - 8 STONE WINDOW HEADER
 - 9 STONE WINDOW SILL
 - 10 FIBERGLASS AWNING WINDOW BY PELLA
 - 11 FIBERGLASS DOUBLE-HUNG WINDOW BY PELLA
 - 12 FIBER CEMENT WINDOW TRIM, PAINTED
 - 13 PAINTED BOARD AND BATT SIDING
 - 14 FIBER CEMENT TRIM, PAINTED
 - 15 STONE WAINSCOT CAP
 - 16 CEMENT FIBER FASCIA BOARD
 - 17 SOLID FIBERGLASS DOOR WITH 2-PANEL SQUARE TOP
 - 18 STONE COLUMN WRAP WITH STONE CAP
 - 19 PAINTED GABLE VENT
 - 20 FIBERGLASS DOOR WITH FULL LIGHT
 - 21 STAINED CEDAR BRACKET
 - 22 FIBER CEMENT SHAKE SHINGLE SIDING, PAINTED
 - 23 PAINTED WOOD PORCH RAILING
 - 24 PAINTED METAL EXTERIOR EGRESS STAIR
 - 25 PREFINISHED GUTTER AND DOWNSPOUTS

Note:

TREANORHL

719 SW Van Buren St., Ste. 200
Topeka, Kansas 66603
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PROGRESS PRINTS
11/26/2018 2:32:58 PM

TIER II HOUSING

1000 W. 2nd Street
Lawrence, KS 66044

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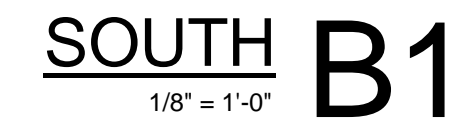
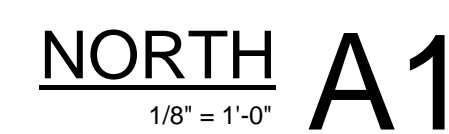
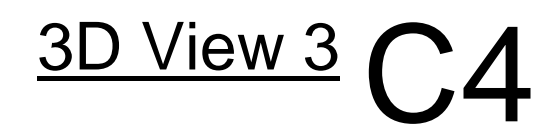
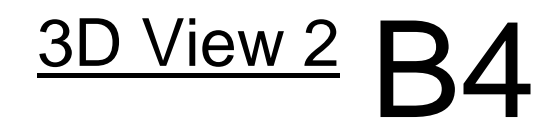
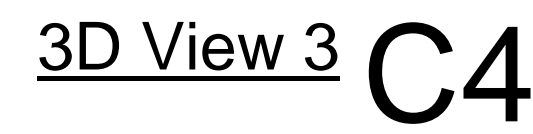
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NO	DESCRIPTION	DATE

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ELEVATIONS

TreanorHL NO. JS16.001.00B



- ### ELEVATION NOTES

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ELEVATIONS

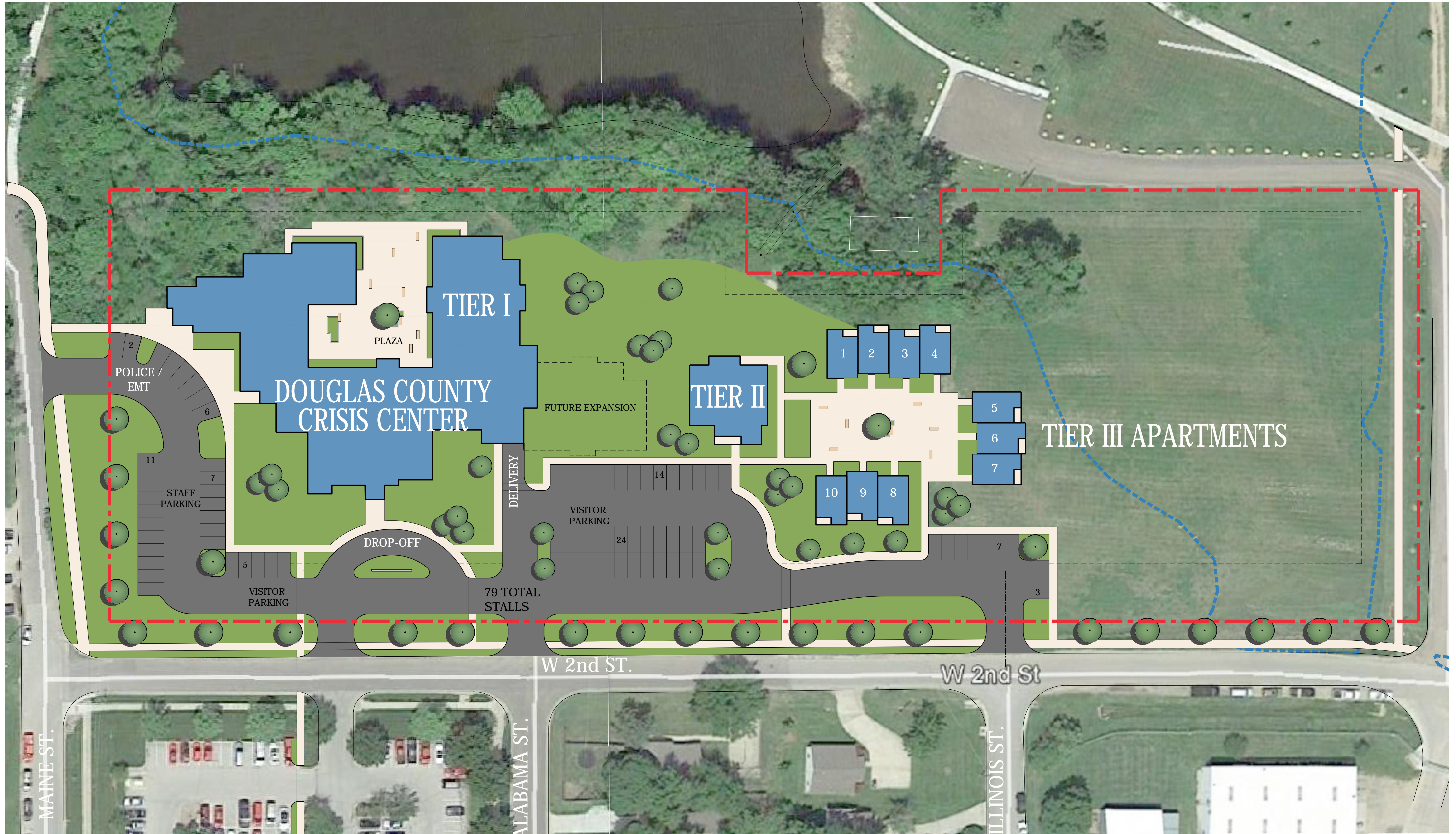
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DOUGLAS COUNTY - MENTAL HEALTH CAMPUS

SITE PLAN

1'-0" = 30'-0"



12/11/17

TREANORHL



SUP-18-00518: Special Use Permit for Douglas County
Crisis Center located at 1000 W. 2nd STREET.

